

Village of Carol Stream

AGENDA

REGULAR MEETING-PLAN COMMISSION/ZONING BOARD OF APPEALS

MONDAY, SEPTEMBER 11, 2023 AT 6:00 P.M.

ALL MATTERS ON THE AGENDA MAY BE DISCUSSED, AMENDED AND ACTED UPON

- I. Roll Call: Present:
Absent:

- II. Approval of Minutes: July 24, 2023

- III. Public Hearing

23-0033 – North Avenue TH LLC / Mainfreight – 27W364 North Avenue
Zoning Map Amendment Upon Annexation
A Special Use Permit for a Motor Freight Terminal
Landscaping Variation
Loading Dock Variation
Fence Variation
North Avenue Corridor Review
Plat of Consolidation

- IV. Presentation:

- V. Old Business:

- VI. New Business:

- VII. Report of Officers:

- VIII. Adjournment:

**Regular Meeting – Plan Commission/Zoning Board of Appeals
Gregory J. Bielawski Municipal Center, DuPage County, Carol Stream, Illinois**

***All Matters on the Agenda may be Discussed, Amended and Acted Upon
July 24, 2023.***

Mr. Farace stated since Chairman Parisi was absent an Acting Chairman would need to be nominated. Commissioner Petella made a motion to nominate Commissioner Tucek and Commissioner Battisto seconded the motion.

Acting Chairman Tucek called the Regular Meeting of the Combined Plan Commission/Zoning Board of Appeals to order at 6:00 p.m. and directed Patty Battaglia, Planning and Permitting Assistant, to call the roll.

The results of the roll call vote were:

Present: 5 Commissioners Battisto, Meneghini, Morris, Petella, and Acting Chairman Tucek

Absent: 2 Commissioner Christopher and Chairman Parisi

Also Present: Tom Farace, Planning and Economic Development Manager; Bravo Berisha, Assistant Planner; Patty Battaglia, Planning and Permitting Assistant and a representative from County Court Reporters

MINUTES:

Commissioner Petella moved and Commissioner Morris seconded the motion to approve the minutes of the Regular Meeting held on May 22, 2023 and June 26, 2023.

The results of the roll call vote were:

Ayes: 5 Commissioners Battisto, Meneghini, Morris, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 2 Commissioner Christopher and Chairman Parisi

The motion passed by a unanimous vote.

PUBLIC HEARING:

Acting Chairman Tucek asked for a motion to open the Public Hearing. Commissioner Petella moved and Commissioner Morris seconded the motion.

The results of the roll call vote were:

Ayes: 5 Commissioners Battisto, Meneghini, Morris, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 2 Commissioner Christopher and Chairman Parisi

The motion passed by unanimous vote.

Case #23-0030 – E.P. Doyle & Sons, LLC/Bartel’s Auto Clinic – 160 S. Gary Avenue

Special Use Permit for Auto Repair

Zoning Variation

Gary Avenue Corridor Review

Acting Chairman Tucek swore in Mr. Tim Doyle, 1100 Wheaton Oaks Ct, Wheaton, Illinois, and asked him to provide his presentation.

Mr. Doyle stated we are missing some key members, but Ann is representing Partners and Design so if there are any questions relative to the architecture she can speak about that. We are here as the design builder and leading the effort for the Bartel family to mimic what they currently have right now at the corner of St. Charles and Gary. Their business has really taken off not only at this location but in other communities that they serve. They were the original owners of that property and developed it all except for that one acre out lot to the south. They are starting to have a parking problem because they are just too busy. They looked at a couple different options of adding on and it made the most sense, with the existing utilities and existing right-of-ways, to build another facility. The new facility will be very similar and we’ve made some improvements like a wider, deeper work base and more parking. This building will be less public driven, meaning the public won’t come into as much as the current building, and more mechanic and building service driven. We have been working with Staff very well over the last several months and we have prepared a solid proposal and have met most of the requirements they are looking for.

Mr. Berisha started the Staff Report by stating Bartels is currently at 190 S. Gary Avenue and they own the one acre parcel at 160 S. Gary Avenue where they are proposing the development of the new repair facility. It will be 8,481 square feet and the primary purpose is to accommodate the growing needs of the business, alleviate some parking restraints and better serve customers.

Staff did conduct a parking study for this facility and, based on the requirements in our UDO, this facility will have 6,656 square feet of net floor area (per the code, the requirement states one parking space for every 250 square feet of net floor area) so they will require 13 spaces and one space per service bay. There will be ten service bays in the new facility for a total of 23 required spaces. They will have 50 spaces in the new development which exceeds the maximum permitted number. This was reviewed by Staff and believe it to be necessary in order for this facility to properly function and alleviate those parking constraints.

The minimum setback for a rear yard is 20 feet in the Industrial District. Bartels is proposing to reduce that to 16 feet 6 inches in order to maximum the working space inside the building, parking spaces and accommodate the service bays correctly. Bartels has contacted the property owner directly to the west, Artlow Systems, and they have confirmed there is no opposition to the variation being granted.

Staff has reviewed and is also supportive of the variation. Access to the site will be from a driveway off Gary Avenue and a sidewalk for customers to travel between the two facilities.

The trash enclosure will be located on the northwest corner of the building and the rooftop units will not be visible due to the height of the roofline. The exterior of the building will be insulated, precast concrete panels along with color bands on the sides of the building. The style and design will mimic the existing Bartel’s Auto Clinic and Staff believes it will compliment the existing building and enhance the subject property which is currently vacant.

The landscaping proposed will consist of tree and shrub groupings on every parking lot island as well as along the Gary Avenue frontage for parking lot screening. There will also be foundation landscaping along the north and east sides of the building which will make it a more aesthetically pleasing site. Staff has reviewed the requests and recommend approval.

Acting Chairman Tucek asked for questions from the audience and there were none.

Acting Chairman Tucek asked for questions from the Commission.

Commissioners Morris and Pelella had no questions.

Commissioner Meneghini asked if they will be able to comply with the eight conditions.

Mr. Doyle stated yes we reviewed them all and we have no problem complying.

Commissioner Battisto asked about the current overflow of cars.

Mr. Doyle stated on average they are short now by about ten spaces. On the new building they will be adding 10 more stalls which is ten more spaces with 30 extra and I think that is going to come out almost perfect.

Acting Chairman Tucek stated it almost looks as if there is landscaping between the bike path and Gary Avenue. Is the petitioner responsible for that landscaping. But they are not there now though so the petitioner is not responsible for it.

Mr. Farace replied correct.

Commissioner Meneghini redirected and apologized for asking a question and not calling Mr. Doyle back up to the podium in order to hear his response.

Commissioner Petella moved and Commissioner Meneghini seconded the motion with no further discussion.

The results of the roll call vote were:

Ayes: 5 Commissioners Battisto, Meneghini, Morris, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 2 Commissioner Christopher and Chairman Parisi

The motion passed by unanimous vote.

This case will go before the Village Board on Monday, August 7, 2023, at 6:00 PM for review.

NEW BUSINESS:

PRESENTATION:

OLD BUSINESS:

OTHER BUSINESS:

Mr. Farace stated the Commission can vote on canceling the August 14, 2023, meeting due to the lack of agenda items.

Commissioner Battisto moved and Commissioner Morris seconded the motion with no further discussion.

The results of the roll call vote were:

Ayes: 5 Commissioners Battisto, Meneghini, Morris, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 2 Commissioner Christopher and Chairman Parisi

The motion passed by unanimous vote.

Commission Morris asked Mr. Farace if the project at the corner of Fair Oaks and Lies Road stopped. I heard a rumor that it had fallen through because the farmer has planted his crop and I think it is soybean. But I heard that it had stopped.

Mr. Farace stated no and they should be resubmitting very soon.

Then there was a brief discussion regarding approval of minutes.

Acting Chairman Tucek stated between the bar on North Avenue, which will be the cannabis dispensary, and the church, there is a new piece of construction. He then asked if that was Carol Stream.

Mr. Farace stated yes. You would have reviewed it last year. It is Dynamic Investments which is a truck repair facility.

Acting Chairman Tucek then asked about the corner of North Avenue and Morton because it's been cleared out. They are going to be constructing a new building in there aren't they.

Mr. Farace stated the corner of North Avenue and Morton is unincorporated for now.

Commissioner Morris stated but that's not ours. That is West Chicago unincorporated.

Mr. Farace stated for now it is unincorporated and you might see a proposal coming before the Commission.

Commissioner Morris asked if it is for annexation.

Mr. Farace stated right.

Commissioner Morris asked if there was a proposal on the table right now to build like a 8,500 square foot building and house 90 vehicles in there. He said that was in the paper.

Mr. Farace stated that was from the county. Yes, a similar proposal.

PUBLIC HEARING:

Acting Chairman Tucek asked for a motion to close the Public Hearing. Commissioner Battisto moved and Commissioner Morris seconded the motion.

The results of the roll call vote were:

Ayes: 5 Commissioners Battisto, Meneghini, Morris, Petella and Acting Chairman Tucek
 Nays: 0
 Abstain: 0
 Absent: 2 Commissioner Christopher and Chairman Parisi

The motion passed by unanimous vote.

ADJOURNMENT:

At 6:18 p.m. Commissioner Battisto moved and Commissioner Morris seconded the motion to adjourn the meeting.

The results of the roll call vote were:

Ayes: 5 Commissioners Battisto, Meneghini, Morris, Petella, and Acting Chairman Tucek
 Nays: 0
 Abstain: 0
 Absent: 2 Commissioner Christopher and Chairman Parisi

The motion passed by unanimous vote.

FOR THE COMBINED BOARD

Recorded and transcribed by,

 Patty Battaglia
 Planning and Permitting Assistant

Minutes approved by Plan Commission on this ____ day of _____, 20____.

 Acting Chairman

Village of Carol Stream Plan Commission/Zoning Board of Appeals

STAFF REPORT

September 11, 2023

TO:
Chairperson and Plan Commissioners

FROM:
Community Development Department

CASE MANAGER:
Tom Farace, Planning & Economic
Development Manager

ACTION REQUESTED:
The applicant is requesting approval of the
following:

- A Zoning Map Amendment Upon Annexation, in accordance with Section 16-8-4 (N) of the UDO
- A Special Use Permit for a Motor Freight Terminal, in accordance with Section 16-3-11 (E) of the UDO
- A Variation to allow loading docks along the south side of the building, in accordance with Section 16-5-16 (C)(1) of the UDO
- A Variation to eliminate foundation landscaping along the south side of the building, in accordance with Section 16-5-6 (B)(3)(a) of the UDO
- A Variation to allow a fence to be located in the front yard, in accordance with Section 16-5-8 (F)(2) of the UDO
- North Avenue Corridor Review, in accordance with Section 16-8-4 (J) of the UDO
- Plat of Consolidation, in accordance with Section 16-8-4 (O) of the UDO

APPLICANT/ CONTACT:

Mr. Connor Harmon
Timber Hill Group LLC
8770 W. Bryn Mawr, Suite 1350
Chicago, IL 60631



CASE #: 23-0033

LOCATION: 27W364 North Avenue

PROJECT NAME: North Avenue TH LLC / Mainfreight



LOCATION	ZONING DISTRICT	LAND USE	COMPREHENSIVE PLAN DESIGNATION
Subject Property	Unincorporated DuPage County R-2 Residence District	Commercial (Vacant)	Industrial (Village of Carol Stream)
North	Unincorporated DuPage County R-2 Residence District	Single-Family Residential	Single-Family Residential
South	B-3 General Business District	Commercial (North Ave Pub & Grill, Affinity Auto Sales)	Corridor Commercial
East	Unincorporated DuPage County B-2 General Business District	Commercial (DuPage Water Conditioning)	Commercial
West	Unincorporated DuPage County R-3 Residence District	Institutional (Wheaton Bible Church)	Institutional

The 15.76 property highlighted above is located at the northeast corner of North Avenue and Morton Road.

Site Assessment

COMPREHENSIVE PLAN DESIGNATION:

The subject property is designated for industrial uses if annexed into Carol Stream according to the Village's 2016 Comprehensive Plan. The proposed motor freight terminal potentially fits within this designation.

AERIAL PHOTOGRAPH:



Project Summary

ATTACHMENTS:

Attached for review is the General Application, General Variation Application, Fence Variation Application, Special Use Application, Map Amendment Application, Cover Letter from Timber Hill received August 21, 2023, Public Notice, Site Location Map (Exhibit A), Existing Conditions and

Demolition Plan (Exhibit B), Overall Site Plan and Detailed Site Plan (Exhibits C-1 and C-2), Site Grading Plan, Soil Erosion and Sediment Control Plan, and Site Utility Plan (Exhibits D-1, D-2, and D-3), Turning Exhibits (Exhibits E-1 and E-2), Landscape Plans and Details (Exhibits F-1 through F-6), Floor Plans (Exhibits G-1 through G-4), Elevations (Exhibits H-1 and H-2), Plat of Consolidation (Exhibits I-1 and I-2), Fence Detail (Exhibits J-1 and J-2), and Traffic Study (Exhibit K).

BACKGROUND:

Connor Harmon, Associate Director of Development with the Timber Hill Group, requests approval of the following items associated with the development of a Mainfreight motor freight terminal at the northeast corner of North Avenue and Morton Road:

- A Zoning Map Amendment to rezone the property to the I Industrial District upon annexation
- A Special Use Permit for a Motor Freight Terminal
- A Variation to allow loading docks along the south side of the building
- A Variation to eliminate foundation landscaping along the south side of the building
- A Variation to allow a fence to be located in the front yard
- North Avenue Corridor (NAC) Review
- Plat of Consolidation

It should be noted that requests for subdivision-related variations pertaining to Morton Road upgrades and a special use permit for outdoor vehicle storage were noticed in the paper after the original public notice for the project, requiring a second public hearing before the PC/ZBA on September 25, 2023. Staff recommends that the PC/ZBA open the public hearing, review, and provide questions and comments on the above requests, but then continue the public hearing to the September 25 PC/ZBA meeting and provide a recommendation on the project and all associated requests at that time.

As stated in the submitted cover letter, North Ave TH LLC represents the property entity under the umbrella company Timber Hill Group. Timber Hill Group specializes in the acquisition and development of logistics-related industrial real estate throughout the U.S. The subject property consists of four parcels; one parcel which previously stored a variety of trucks, trailers, and equipment, and three parcels which are undeveloped. The four parcels will be consolidated for a 57,100 square-foot motor freight terminal for Mainfreight, Inc. Mainfreight is a New Zealand-based freight carrier that focuses on transport and warehousing of goods throughout the world. Mainfreight proposes to relocate its North American corporate headquarters from Los Angeles to the subject property, along with the creation of up to 60 jobs for the office component of the building.

Earlier this year, the applicant received approval from the DuPage County Board for the motor freight terminal within the County. However, the applicant prefers to annex into and develop in Carol Stream, and connect to existing municipal utilities along North Avenue.

Staff Analysis

ZONING MAP AMENDMENT UPON ANNEXATION

For cases involving requests for annexation, the PC/ZBA must conduct a public hearing and make a recommendation regarding the zoning classification of the property upon annexation. In this case, the applicant is seeking to zone the property to I Industrial District upon annexation to the Village. Factors to be considered in determining the appropriate zoning classification for the property include the recommendation of the Comprehensive Plan's Future Land Use Map, the current and future use of the property, the zoning and use of properties in the surrounding area, and the Village's development objectives for the area.

With respect to the Comprehensive Plan Future Land Use Map, the Map recommends Industrial use for the subject property upon annexation. The Comprehensive Plan states that Industrial uses "*constitute one quarter of the community's total land,*" and that "*the Village should support its stable industrial sector, recognizing its enormous contribution to the local and regional economy in terms of generating revenue and creating jobs that pay at least middle-class wages.*" The use of the property as a motor freight terminal fits within the Industrial designation and objectives of the Comprehensive Plan, and annexation of this parcel would allow for a global company to relocate its headquarters to Carol Stream.

As stated in the UDO, the I Industrial District is established "*to provide for industrial and manufacturing uses which may be intensive in nature. This district is intended to accommodate uses that require extensive exterior movement of trucks, vehicles, goods, or other exterior operations.*" Industrial zoning for the subject property would be consistent with the future land use recommendation in the Comprehensive Plan, and provided the subject property is developed according to the NAC regulations, the Industrial District zoning would be compatible with the zoning classification of properties in the area. Therefore, staff supports the subject property being zoned I Industrial District upon annexation to the Village. It should be noted that the Village is currently reviewing an annexation agreement and plat of annexation for the development, which will be reviewed at a future public hearing before the Village Board.

SPECIAL USE PERMIT FOR A MOTOR FREIGHT TERMINAL

As previously stated, a special use permit for a motor freight terminal is requested. The overall design of the development from a site planning, landscaping, and architectural perspective will be discussed in the NAC section of the staff report. According to the UDO, a motor freight terminal is defined as "*a building or area in which freight brought by motor truck is assembled or stored for routing in intrastate or interstate shipment by motor truck.*"

- The proposed development consists of a 57,100 square-foot building, which will include 81 loading docks (41 docks on the north side of the building and 40 docks on the south side of the building), along with a two-story office component on the west side of the building.

- Additional trailer and truck storage is proposed on the northern half of the development, with automobile parking on the west side of the development that is separated from the truck/trailer parking and dock areas.
- According to the submitted cover letter, the proposed development will operate from 7:00am to 5:00pm, with limited activity on weekends. In addition, that applicant states that the proposed development will have approximately 24 trucks entering and exiting the site daily.
- Staff questioned the limited amount of truck traffic given the number of proposed truck docks and truck/trailer parking spaces. The applicant explained that while Mainfreight trucks will have some local product delivery and pickup, there are also trucks making longer hauls across the country and will be away from the site making deliveries for several days at a time.
- The applicant submitted a traffic study, prepared by KLOA and dated March 15, 2023. Traffic projections in the study indicate there will be approximately 10 trucks entering and exiting the site during weekday morning and evening peak hours for Mainfreight, which is lower than previous truck traffic to and from the property. It should be noted that other logistics and construction companies previously leased storage space on the property, but those leases ended earlier this summer and the site is currently vacant.



Staff supports the Special Use request, and believes the motor freight terminal is appropriate within the Industrial District and should not have negative impacts on surrounding properties. For consistency, staff also recommends that conditions of approval for the project include standard conditions that have been established for truck/trailer storage and/or repair businesses. These conditions include:

- Trucks shall not be allowed to back into the property from Morton Road
- Trucks shall be prohibited from exiting the property and travelling north on Morton Road
- The applicant to further design modifications to the north driveway on Morton Road to restrict trucks from turning north

- Trucks and trailers shall only park in designated parking spaces or within loading docks, and not block access nor be parked or stored in drive aisles or the automobile parking lot along Morton Road
- Sleeping in trailers or cabs shall not be allowed
- Only trucks and trailers owned and operated by Mainfreight shall be allowed to be stored on the property, and the storage of recreational vehicles, materials, and other equipment not associated with Mainfreight shall not be allowed on the property

SPECIAL USE FINDINGS OF FACT

The Plan Commission's recommendation regarding the requested Special Use requests must be based on the evaluation criteria set forth in the UDO. As stated in §16-8-4 (L) of the UDO, no Special Use shall be recommended by the Plan Commission nor approved by the Village Board unless the Special Use:

1. Is deemed necessary for the public convenience at the location.

The redevelopment of the property with a modern motor freight terminal will be considered a public convenience at the subject location.

2. Will not be unreasonably detrimental to or endanger the public health, safety, morals, comfort or general welfare.

The proposed motor freight terminal is designed and will operate in a safe manner, and should not be detrimental to or endanger the public health, safety, morals, comfort or general welfare.

3. Will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.

The surrounding area has a mix of industrial, commercial, institutional, and residential uses. While the proposed use will generate truck traffic, there will be less truck traffic than what was generated with the previous use, and the development has been designed so that there is improved traffic flow, landscaping, storm water management, and air quality for the immediate area. Therefore, the business should not be injurious to the use and enjoyment of other properties in the immediate vicinity for the purposes already permitted, nor diminish or impair property values within the area.

4. Will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

Surrounding properties are already developed. As such, there should be no impact on the normal and orderly development and improvement of surrounding properties.

5. Will provide adequate utilities, access roads, drainage and other important and necessary community facilities.

Utilities, access roads, drainage and other public improvements will be constructed with the proposed development.

6. Will conform to the applicable regulations of the district in which it is located, except as the Village Board may in each instance modify such regulations.

The project is expected to conform to all applicable codes and requirements.

NORTH AVENUE CORRIDOR REVIEW

NAC regulations are applicable to properties abutting North Avenue, and Plan Commission review of the proposed development is required to ensure that the proposal is in conformance with the NAC regulations. The Plan Commission has the authority to make the final determination of conformance with the NAC regulations and Village Board consideration is not required. The sections of the NAC regulations that apply to this project include site, landscape and architectural design, and signage.

Site Design

- As proposed, access into the development is proposed from two driveways along Morton Road. A full access driveway is proposed further north on Morton Road, and a right-in only driveway is proposed near the North/Morton intersection. Staff initially recommended that the applicant eliminate the driveway closest to the North/Morton intersection, and construct a right-in/right-out driveway on North Avenue (near the southeast corner of the development). North Avenue is under the jurisdiction of the Illinois Department of Transportation (IDOT). The applicant has had initial discussions with IDOT regarding access for the development, and IDOT was not in favor of allowing a driveway along North Avenue. It should be noted that IDOT has not fully reviewed access for the proposed development, and may request that the right-in only driveway be shifted further north along Morton Road. The applicant is aware of this potential request, which will also require additional site plan changes to be reviewed again by the Plan Commission and Village Board. Staff is recommending that a condition of approval for the project be included which stipulates that future site plan modifications related to IDOT access requirements will require review of an amendment to the special use request.
- Staff supports the full access driveway further north on Morton Road, but recommends that the driveway be modified to prevent trucks from traveling north on Morton Road. The applicant proposes to install signage that states “No Right Turn” onto Morton Road, but additional measures such as installing a taller curb or placing items such as large bollards or boulders on the north side of the proposed driveway to preclude trucks from travelling northbound on Morton Road. The applicant describes in the submitted cover letter that only Mainfreight trucks will be entering and exiting the site, so that Mainfreight can monitor and control trucks entering and exiting from both driveways along Morton Road.

- As described in the submitted cover letter, trucks will utilize both proposed driveways along Morton Road (entering and exiting from the north driveway, and entering only from the south driveway), and will be positioned on either side of the terminal building for distribution of products. Additional trailer and tractor parking spaces are proposed north of the building.
- Automobile parking spaces are proposed west of the terminal building, and adjacent to the office portion of the building. Parking for development will be met on-site, and is calculated as follows:

Business	Area of Use*	Code Requirement	Spaces Required
Mainfreight (57,000 sq ft GFA)			
10,000 sq ft of net office space	Office Space	1 space per 250 sq ft of NFA	40 spaces
37,100 sq ft of net terminal space	Terminal Space	1 space per 1,500 sq ft of NFA	25 spaces
Parking Spaces Required:			65 spaces
Parking Spaces Provided:			92 spaces

- A storm water management system is proposed along the North Avenue frontage of the property, with additional detention capacity proposed in an underground vault system in the paved area north of the storm water basin. Large greenspace areas are also proposed on the north, east, and west sides of the development for screening and buffering purposes (described in more detail in the landscaping section of the report).
- Additional site details need to be revised on the site plan. For example, the applicant has indicated that gates with security controls will be provided across both driveways. However, the plans do not show the gates, and staff has not reviewed a design for said gate system. Likewise, a dumpster enclosure is not illustrated on the plans, and Mainfreight may install a trash compactor instead of providing a separate enclosure for dumpsters. Either the compactor or enclosure needs to be illustrated on the plans, or a note should be provided on the plans stating that a future enclosure or compactor shall comply with all design and location requirements. Staff recommends revised plans be submitted prior to the September 25 PC/ZBA meeting.

Landscape Design

- The applicant has submitted a landscape plan that meets the requirements for properties along the North Avenue Corridor. Landscaping in the form of trees and shrubs are proposed for screening purposes along North Avenue and around the storm water basin. Native plantings are also proposed within the basin. Likewise, extensive landscape buffering is proposed on the north and east sides of the property, and includes several existing trees and

new plant material. Landscape screening is also proposed along Morton Road, with a combination of trees and shrubs, to screen the parking lot. Finally, foundation landscaping is proposed along the east and west sides of the building, and landscaped islands are proposed in the automobile parking lot on the west side of the development.

- Similar to the site plan, revisions are required on the landscape plan. While the UDO requires that automobile parking lots contain landscaped parking lot islands, parking lots that contain truck and trailer parking spaces are not required to have islands. The site plan illustrates some striped areas at the ends of truck and trailer parking rows, but the landscape still illustrates landscaped islands. Likewise, an outdoor patio at the northwest corner of the terminal building is shown on the site plan, but is not shown on the landscape plan. Staff recommends revised plans be submitted prior to the September 25th PC/ZBA meeting illustrating these modifications.

Building Design

- The majority of the proposed building will contain single-story space for products to be moved between trailers, with docks proposed on both the north and south sides of the building.
- The office portion of the building, located on the west side, will be two stories. The first floor will contain a training room, break room, fitness area, locker room, and drivers lounge. The second floor will contain office space, meeting rooms, and storage areas.
- The building architecture will be modern in nature, with the use of articulated gray precast concrete panels. Blue color bands are proposed on all four sides of the building, as well as the installation of clerestory windows on the upper level of the terminal portion of the building, which will bring in natural light. The two-story office portion of the building will contain a substantial amount of glazing and an aluminum composite material (ACM) panel entry feature facing Morton. The use of precast concrete and ACM panels, along with glazing, will provide for a dramatic building presence along the NAC.



It should be noted that monument signs are not proposed along either North Avenue or Morton Road. Wall signage is proposed on the ACM panel entry on the west side of the building. In addition, a large sign on top of the terminal portion of the building is proposed. The UDO does not provide specific regulations for building-mounted signage (other than wall signage meeting size requirements within the UDO), and the sign proposed on top of the terminal portion of the building will only be visible from the sky. The submitted cover letter indicates that solar panels are proposed on the roof of the building, and staff requests that the applicant explain how solar panels and the signage on top of the building will coexist.

In addition, the applicant continues to work with Village Engineering staff on detention and utility matters. Engineering has the several comments regarding the submitted plans, which will need to be addressed during final civil engineering review. Comments have been provided in the attached Exhibit at the end of the staff report.

Overall, staff is supportive of the proposed building design, site layout, and landscaping, and will provide for an attractive development along the NAC.

VARIATIONS – LOADING DOCK LOCATION, LANDSCAPING AND FENCING

The applicant is requesting variations from the following provisions in the UDO:

Loading Dock Location Variation

The first variation request is to allow loading docks on the south side of the building. According to Section 16-5-16 (C)(1) of the UDO (North Avenue Corridor Regulations), service areas are required to be out of sight from North Avenue. Service areas include components such as loading docks and dumpster enclosures. As proposed, the building contains loading docks on both the north and south sides of the building, which will allow products to be loaded and unloaded on both sides of the building. Docks on the south side of the building will be visible from North Avenue, which necessitates the variation request.

Staff can support the variation request for a few reasons. The terminal portion of the building is not extremely wide, at approximately 70 feet in width. Based on this building design, and the configuration of the site that is wider on the south end and narrower on the north end, the building placement cannot be rotated so that the building runs north to south. Truck circulation would also be awkward with a north/south building placement. The placement of the proposed building with an east/west configuration allows for more efficient traffic flow. In addition, the landscape plan illustrates a large amount of landscaping along the North Avenue property frontage, which will aid in screening the loading docks from view.

Foundation Landscaping Variation

Section 16-5-6 (B)(3)(a) of the UDO states that plantings are required along building foundations facing roadways. The submitted landscape plan illustrates landscaping along the west side of the building facing Morton Road, but there are no plantings proposed along south side of the building

facing North Avenue. Due to the configuration of the proposed building and location of loading docks on the south side of the truck terminal portion of the building, it would be impractical to install plantings along the south building foundation. Along with the plantings proposed along the west foundation, there are also plantings proposed on the north and south ends of the office portion of the building, and along the east building foundation. The east foundation plantings are not required per the UDO, but provide additional plant material to soften the building wall and anchor the building to the site. As such, staff supports the foundation landscaping variation request along the south side of the building.

Fencing Variation

The applicant proposes to install a six-foot tall decorative fence along the north and east sides of the property. Along the east side of the property, the fence is proposed to extend to the east side of the storm water basin, but will still be at least 40 feet away from North Avenue. Section 16-5-8 (F)(2) of the UDO states that fences are not allowed to be located in the front yard of a property. The extension of the proposed fence beyond the southeast corner of the building requires approval of a variation. Staff can support the variation request, as the proposed fence will be decorative in nature, will not extend in such a manner as to inhibit motorist visibility along North Avenue, and will provide an additional open screening mechanism for the property. Staff does recommend that the fence be extended along the west property line facing Morton Road, north of the full access driveway and up to the north property line. The extension will provide additional screening of the north truck/trailer parking area in conjunction with the proposed landscaping, and provide a more consistent appearance for the fencing around the property.

VARIATION FINDINGS OF FACT

With regard to any variation, the Zoning Board of Appeals shall not recommend a variation unless it shall make findings based upon the evidence presented to it in the following case, as per Section 16-8-4 (K) of the Carol Stream Unified Development Ordinance:

1. The requested variations arise from conditions that are unique to the subject property, that are not ordinarily found in the same zoning district and that are not a result of the owner's intentional action.

The property is oriented in a way that restricts the proposed building from functioning and operating with a north/south orientation. Likewise, the proposed east/west building orientation, with loading docks on the south side of the building, precludes the applicant from installing landscaping along the building's south foundation. Finally, the installation of the proposed fence past the front of the building allows for additional screening and security for the development.

2. The variations to be granted will not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property.

The essential character of the surrounding neighborhood is highly varied, with commercial, institutional, industrial, and residential uses in the immediate vicinity. The proposed development will have a greatly improved design with additional landscaping as compared to the previous use of the property. While loading docks will face North Avenue, they will be located over 200 feet away from the roadway, with landscaping and a storm water basin between the docks and the roadway to minimize their visual impact. In addition, the proposed fencing on the east side of the property will provide additional screening and security measures for the development.

3. The strict application of the applicable standards will constitute an unnecessary physical hardship (not economic hardship) because the property cannot be used for an otherwise allowed use without coming into conflict with applicable site development standards.

The configuration of the subject property compels an east/west building placement rather than north/south. As such, loading docks will face North Avenue and there will be a lack of foundation landscaping on the south side of the building, but the applicant proposes additional foundation plantings around the building to make up for the lack of plantings on the south side of the building. Likewise, the loading docks will be over 200 feet away from North Avenue, with perimeter landscaping and a storm water basin proposed between the loading docks and the roadway. Finally, installing decorative fencing past the front of the building will not impede visibility or be in conflict with applicable site development standards.

4. The variations are the minimum action necessary to alleviate the hardship and observes the spirit of this UDO.

The requested loading dock and foundation landscaping variations are necessary to create a functional site for Mainfreight, and the proposed decorative fencing located past the front of the building will not have negative impacts on surrounding properties or impact visibility of motorists and will observe the spirit of the UDO.

5. The variations desired will not adversely affect the public health, safety, or general welfare or impair the purposes or intent of this UDO or the comprehensive plan.

It is not believed that the requested variations will have an adverse effect on public health, safety, or general welfare, and meets the intent of the UDO and Village Comprehensive Plan by bringing a high quality and attractively designed development into the community.

PLAT OF CONSOLIDATION

A plat of consolidation has been submitted for review, which consolidates the four existing lots into one lot. In total, the newly established lot will measure approximately 15.76 acres. Staff from Community Development, Engineering Services, and the Village Clerk's office have reviewed the submitted plat, and deem it generally acceptable. However, the following revisions to the plat are requested prior to the review of the project at the September 25, 2023 PC/ZBA meeting:

- Please revise the plat title to read “Consolidation” instead of “Subdivision.”
- The DuPage County Recorder’s Office requests that the upper right corner of a plat be left blank so the recording number can be placed on the plat in that area. Please relocate the site map and other items to another place on the plat (potentially near the bottom left corner of the plat).
- Provide a list of applicable taxing bodies on the plat. In addition, school district information can be placed in this list rather than in the owner’s certificate.
- Along the western edge of existing Lot 1, a dedication of 7’ for Right of Way for Morton Road needs to be illustrated which will keep a straight right of way line along the east edge of the Morton Road right of way.
- The signature page has the following statement at the top: “A Plat of Subdivision shall contain all of the certificates listed above for a Plat of Easement along with the following additional signature certificates.” This statement can be removed.
- The mortgagee and notary certificates are only required if the land will be mortgaged at the time that the plat is recorded.
- Since Morton Road is currently under Wayne Township’s jurisdiction, a certificate block for Wayne Township Highway Commissioner will need to be provided.

It should be noted that storm water and utility easements would need to be dedicated. Staff recommends submitting a separate plat of easement after final civil/storm water approval so the exact location of all required utility easements are known.

RECOMMENDATION

Staff has reviewed the proposed requests, and is supportive of the project. However, there are additional zoning requests associated with the project that need to be reviewed at the September 25, 2023 PC/ZBA meeting. Therefore, staff recommends that the PC/ZBA make comments and ask questions about the proposal and then continue the case to the September 25, 2023 meeting. In addition, staff recommends that the following revisions be completed and resubmitted plans be provided in advance of the September 25th meeting:

1. The full access driveway on Morton Road shall be modified to prohibit trucks from traveling north on Morton to the greatest extent possible. Modifications shall include the installation of a taller curb and gutter (B9.12) in the vicinity of the north radius of the driveway, reduce the north curb radius of the north driveway, and potentially installation of bollards and boulders or other means of deterring trucks from traveling north on Morton Road;
2. Additional site details or notes shall be provided on the plans. These include proposed gates with security controls at driveway entrances, and location of dumpster enclosure or trash compactor on the property. Details and/or specifications shall also be provided, and no dumpster enclosures or compactors shall be installed visible from North Avenue or Morton Road;

3. Plans shall be consistent with parking lot islands and striping in the rear truck/trailer storage areas on the north side of the property, the outdoor patio location near the northwest corner of the building, and installation of either solar panels or signage on the top of the terminal portion of the building;
4. The proposed six-foot tall decorative fence shall be extended along the west property line facing Morton Road, from the north property line to the north full access driveway; and
5. The plat of consolidation shall be revised to including the following:
 - a. Please revise the plat title to read "Consolidation" instead of "Subdivision." In addition, it is recommended that "TH" be spelled out to "Timber Hill" for ease of searching recorded records in the future.
 - b. The DuPage County Recorder's Office requests that the upper right corner of a plat be left blank so the recording number can be placed on the plat in that area. Please relocate the site map and other items to another place on the plat (potentially near the bottom left corner of the plat).
 - c. Provide a list of applicable taxing bodies on the plat. In addition, school district information can be placed in this list rather than in the owner's certificate.
 - d. Along the western edge of existing Lot 1, a dedication of 7' for Right of Way for Morton Road needs to be illustrated which will keep a straight right of way line along the east edge of the Morton Road right of way.
 - e. The signature page has the following statement at the top: "A Plat of Subdivision shall contain all of the certificates listed above for a Plat of Easement along with the following additional signature certificates." This statement can be removed.
 - f. The mortgagee and notary certificates are only required if the land will be mortgaged at the time that the plat is recorded.
 - g. Since Morton Road is currently under Wayne Township's jurisdiction, a certificate block for Wayne Township Highway Commissioner will need to be provided.

For reference, staff also recommends the following conditions of approval for the project, once all requests are reviewed at the September 25, 2023 PC/ZBA meeting:

1. That should IDOT either allow a driveway on North Avenue or require that the proposed right-in driveway on Morton Road be shifted further north, a special use amendment will need to be reviewed by the Plan Commission and Village Board;
2. That all improvements, including but limited to the parking lot, fencing and landscaping, must be built, installed, and maintained in accordance with the attached plans and exhibits;
3. That the landscape materials must be installed as shown on the attached landscape plan, and that all materials shall be maintained in a healthy condition, with dead or dying materials being replaced in accordance with the approved plan on an annual basis;
4. That trucks shall not be allowed to back into the property from Morton Road;

5. That trucks and trailers shall only park in designated parking spaces or within loading docks, and not block access nor be parked or stored in drive aisles or the automobile parking lot along Morton Road;
6. That sleeping in trailers or cabs shall not be allowed;
7. That only trucks and trailers owned and operated by Mainfreight shall be allowed to be stored on the property, and the storage of recreational vehicles, materials, and other equipment not associated with Mainfreight shall not be allowed on the property;
8. That a Knox padlock or gate switch shall be provided on any security gates, and the Carol Stream Fire Protection District shall be provided keys to said gate for access onto the property;
9. That parking lot lighting shall meet requirements of the UDO;
10. That the applicant must obtain the required sign permit prior to the installation of any new signage;
11. That if any ground-mounted or roof mounted mechanical and utility equipment is installed, said equipment shall be screened per requirements of the UDO; and
12. That the site and business must be maintained and operated in accordance with all State, County and Village codes and regulations.

T:\Planning New\Planning\Plan Commission\Staff Reports\2023 Staff Reports\23-0033 North Ave TH LLC Mainfreight SUP VAR NAC PLAT 27W364 North Ave.docx

Engineering Comments – Exhibit to Staff Report for Case 23-0033

- There is an existing pipe crossing under Morton Road near the proposed north access point that is partially RCP and partially CMP. The CMP will need to be removed and replaced.
- The north curb line radius of the full access driveway on Morton should be reduced to perhaps 10' radius. In addition, B9.12 curb and gutter should be proposed for the north radius.
- According to the truck turning movement plans, the right-in driveway on Morton should be widened to 35'-40'. There is no margin for error for entering trucks.
- The use of Corrugated Metal Pipe is not acceptable for the proposed underground storage. Per the Village's Design Standards & Construction Specifications, "Pipe used in storm water subsurface storage construction shall be limited to reinforced concrete unless used solely for volume control post-construction best management practices, then all material listed is allowed."
- Preliminary Storm Water
 - Site Runoff Conveyance & Storage (Article IV):
 - The calculations for the required volume (7.22 Ac-ft) appears reasonable.
 - The offsite flows from Wheaton Bible Church (OF-1) should take into account the restricted release rate and actual tributary area.
 - Post-Construction Best Management Practices PCBMPs (Article VIII):
 - The current design of the native vegetated wetland bottom (15-64.A.2) does not adhere to the technical guidance provided by Appendix E. Without a larger footprint, the flow paths and surface area will be too small to effectively treat the size of the upstream impervious area.
 - Although the incorporation of a native vegetated wetland bottom is preferred, the current site design should plan to incorporate them as part of a "treatment train" while addressing PCBMPs per 15-64.A.3 instead (i.e. volume & pollutant control provided separately).

Certificate of the Publisher

Examiner Publications, Inc. certifies that it is the publisher of The Examiner of Carol Stream. The Examiner of Carol Stream is a secular newspaper, has been continuously published weekly for more than fifty (50) weeks prior to the first publication of the attached notice, is published in the Village of Carol Stream, township of Bloomingdale, County of DuPage, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 ILCS 5/5.

A notice, a true copy of which is attached, was published 1 times in The Examiner of Carol Stream, namely one time per week for 1 successive weeks. The first publication of the notice was made in the newspaper, dated and published on August 23, 2023 and the last publication of the notice was made in the newspaper dated and published on August 23, 2023. This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

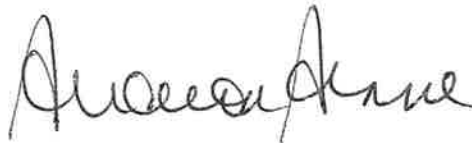
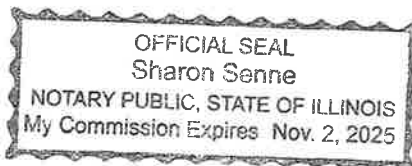
In witness, Examiner Publications, Inc. has signed this certificate by Randall Petrik, its publisher, at The Village of Carol Stream, Illinois, on August 23, 2023.

Examiner Publications, Inc.

By: Publisher 
Randall E. Petrik, Publisher, Examiner Publications, Inc.

Subscribed and sworn to before me this 23 day of August, A.D. 2023.

Notary Public



LEGAL NOTICE PUBLIC NOTICE FILE #23-0033

Notice is hereby given that the Carol Stream Plan Commission/Zoning Board of Appeals will hold a Public Hearing at the Carol Stream Gregory J. Bielawski Municipal Center, 500 N. Gary Avenue, Carol Stream, Illinois, on Monday, September 11, 2023 at 6:00 p.m. to consider an application from North Avenue TH LLC/Mainfreight for the following actions:

A Zoning Map Amendment Upon Annexation, in accordance with Section 16-8-4 (N) of the Carol Stream Unified Development Ordinance;

A Special Use Permit for a Motor Freight Terminal, in accordance with Section 16-3-11 (E) of the Carol Stream Unified Development Ordinance;

A Variation to eliminate foundation landscaping along the south side of the building, in accordance with Section 16-5-6 (B)(3)(a) of the Carol Stream Unified Development Ordinance;

A Variation to allow loading docks along the south side of the building, in accordance with Section 16-5-16 (C)(1) of the Carol Stream Unified Development Ordinance; and

A Variation to allow a fence to be located in the front yard, in accordance with Section 16-5-8 (F)(2) of the Village of Carol Stream Code of Ordinances to allow a fence to be located in the front yard

For the property located at 27W364 North Avenue (PINs 01-36-200-017, 01-36-200-040, 01-36-200-006, and 01-36-200-005)

The hearing will be held in-person and via a web conference meeting. The web conference will allow the public to view the meeting online or listen to the meeting

Join from a PC, Mac, iPad, iPhone or Android device by using the following URL:
[https://us02web.zoom.us/webinar/register/WN_6ysYwZ1RsKqwn9NgW-11Q](https://us02web.zoom.us/join/https://us02web.zoom.us/join/https://us02web.zoom.us/webinar/register/WN_6ysYwZ1RsKqwn9NgW-11Q)

Or join by phone: Dial (for higher quality, dial a number based on your current location):

+1 312 626 6799 or +1 301 715 8592 or +1 646 558 8656

Webinar ID: 842 9020 2664

International numbers available:

<https://zoom.us/j/adsnXEZQRK>

Those members of the public wishing to be heard with respect to this matter must attend in-person, or complete the public comment form at <http://carolstre.am/pc> by 4:00 p.m. the day of the public hearing. The public may also provide written comments prior to the public hearing by submitting them to Tom Farace, AICP, Planning & Economic Development Manager, 500 N. Gary Ave., Carol Stream, IL 60188, or may provide email comments by sending them to tfarace@carolstream.org. Comments must be provided by 4:00 p.m. the day of the public hearing. The application is available for public inspection on the Village's website at www.carolstream.org, and under the "Development Projects" button. Individuals with disabilities who plan to attend the hearing and who require certain accommodations in order to allow them to observe and participate, or who have questions regarding the accessibility of the meeting, are re-

quested to contact the ADA Coordinator at 630-871-6250.

As published in *The Examiner* August 23, 2023. 0823



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Do Not Write in This Space

Date Submitted: 7-24-2023

Fee Submitted: \$5,980.00

File Number: 23-0033

Meeting Date: 9/11/23

Public Hearing Required: Y

Village of Carol Stream

500 N. Gary Avenue ■ Carol Stream, IL 60188

PHONE 630.871.6230 ■ FAX 630.665.1064

www.carolstream.org

FORM A

GENERAL APPLICATION PUBLIC HEARINGS AND DEVELOPMENT APPROVAL REQUESTS

1. Name of Applicant North Ave TH LLC Phone 630-743-3334
 Address 8770 W. Bryn Mawr Ave, Ste 1350, Chicago IL 60631 Fax _____
 E-Mail Address charmon@timberhillgroup.com
 (required)
 Name of Attorney _____ Phone _____
 (if represented)
 Address _____ Fax _____
 Name of Owner _____ Phone _____
 (required if other than applicant)
 Address _____ Fax _____
 Name of Architect _____ Phone _____
 (if applicable)
 Address _____ Fax _____

2. *Common Address/Location of Property 27W364 North Avenue

3. Requested Action: (check all that apply)

<input checked="" type="checkbox"/> Annexation	<input checked="" type="checkbox"/> Subdivision – Minor/Major
<input type="checkbox"/> Courtesy Review	<input type="checkbox"/> Temporary Waiver/Code of Ordinances
<input type="checkbox"/> Development Staff Review	<input type="checkbox"/> Variation – Zoning (requires Form B-1)
<input checked="" type="checkbox"/> Gary/North Avenue Corridor Review	<input type="checkbox"/> Variation – Sign (requires Form B-2)
<input type="checkbox"/> Planned Development – Final	<input type="checkbox"/> Variation – Fence (requires Form B-3)
<input type="checkbox"/> Planned Development – Preliminary	<input checked="" type="checkbox"/> Special Use Permit (requires Form C)
<input type="checkbox"/> Shared Parking Facility	<input checked="" type="checkbox"/> Map Amendment (requires Form D-1)
<input checked="" type="checkbox"/> Site Plan Review	<input type="checkbox"/> Text Amendment (requires Form D2)
	<input type="checkbox"/> Staff Adjustment (requires Form E)

Describe requested action Applicant is seeking annexation into the Village of Carol Stream, a minor plat of subdivision, a map amendment, a Gary/North Avenue corridor approval, and a special use to develop a 57,100 SF freight terminal and industrial office headquarters facility.

4. Fee Schedule: *(Check all that apply)*

Total Application Fee: \$ _____

Annexation approvals:	
New development	\$2,000
Other	\$800
Annexation Agreement Amendment	\$800
Appearance fee for approvals required by the UDO but not listed herein	\$500
Courtesy Review	\$240
Development Staff Review	\$640
Easement Encroachment	\$300
Gary/North Avenue Corridor Review:	
New or replacement monument sign	\$500
One discipline (site design, architecture or landscaping)	\$500
Two disciplines	\$1,000
Three disciplines	\$1,500
New development	\$1,500
Outdoor Dining Permit	\$120
Planned Development:	
Existing Planned Development, minor change	\$500
Existing Planned Development, major change	\$1,000
New Planned Development	\$1,500
Rezoning (Zoning Map Amendment)	\$640
Shared Parking Facility Permit	\$500
Site Plan Review	\$640
Special Use:	
First	\$800
Each additional	\$200
Special Use Amendment	\$800
Staff Adjustment	\$240
Subdivision:	
Major	\$700
Minor	\$400
Temporary Building, Structure or Use	\$120
Temporary Waiver to the Code of Ordinances	\$120
Text Amendment:	
Building Codes	\$240
Unified Development Ordinance	\$640
Variation:	
First	\$640
Each additional	\$200
Zoning verification letter	\$80

5. Applicant Certification

*I authorize the Village of Carol Stream to install a temporary sign or signs on the property having the common address indicated in Item 2 on this form, for notifying the public of the upcoming public hearing, once the hearing has been scheduled.

I have reviewed a copy of the informational handout(s) for the zoning process(es) for which I am making an application. I am familiar with the code requirements that relate to this application and I certify that this submittal is in conformance with such code(s).

I understand that incomplete or substandard submittals may increase the staff review time and delay scheduling of the public hearing. I also understand that, per § 6-13-6 of the Municipal Code, the Village's costs of legal reviews, structural engineering review, and other special reviews determined to be necessary by the Community Development Director, performed by means of consultant services, shall be paid at the billed rate to the Village by the applicant.

Connor Harmon

Print Name

Signature

7/12/23

Date

Revised 11/22



FORM B-1

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Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188
630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org ▪ website: www.carolstream.org

GENERAL VARIATIONS

Please see Section 16-8-4(K) of the Unified Development Ordinance (UDO) to learn more about Variations.

In accordance with the applicable statutes of the State of Illinois, no variation shall be made by the Village Board except after a Public Hearing is held before the Zoning Board of Appeals.

Both the Zoning Board of Appeals and Village Board must decide if the requested variation is in harmony with the general purpose and intent of the Unified Development Ordinance (UDO) and if there is a practical difficulty of hardship in carrying out the strict letter of the regulations of the UDO.

The Zoning Board of Appeals shall make findings based upon evidence presented on the following conditions: (Please respond to each of these standards in writing below as it relates to your request.)

1. The requested Variation arises from conditions that are unique to the subject property, that are not ordinarily found in the same zoning district and that are not a result of the owner's intentional actions.

The property is oriented in a way that prohibits our building and parking lot from fully

functioning and operating at a north/south building design. The site flows significantly better

and trucks are able to maneuver around the site at code standards given an east/west

orientation. As such, we also request to waive the foundation landscape requirements along North Ave

2. That the Variation to be granted will not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property.

The project is a significant improvement to what currently exists on site, and will improve the

character of the existing property and surrounding neighborhood. In the areas we cannot plant

foundation landscaping, we propose to install the landscaping elsewhere on the site, further

beautifying the property.

3. That the conditions of a Variation will constitute of an unnecessary physical hardship (not economic hardship), in which the property cannot be used for an otherwise allowed use without coming into conflict with applicable site development standards.

The shape of the property is not suitable for the type of building Mainfreight that is needed
for their operation. Various landscape screening measures will help block view of the building
from the North Avenue right-of-way.

4. The Variation is the minimum action necessary to alleviate the hardship and observes the spirit of this UDO

Both the service area variation and foundation landscape variation are both required to
create a functioning site for Mainfreight, which is not otherwise possible with the service areas
facing Morton Road. Further, the architectural detail of the building and reduction in trip
generation are both significant improvements to what currently exists.

5. The Variation desired will not adversely affect the public health, safety, or general welfare or impair the purposes or intent of this UDO or the Comprehensive Plan.

Both variation requests will not affect the public health, safety, or general welfare or impair
the purposes or intent of the UDO or comprehensive plan. In fact, our proposal accomplishes
several goals in the comprehensive plan, and improves the property significantly from a
public health, safety, and general welfare perspective.

6. Other pertinent information or reason for the request.



FORM B-3

Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188
630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org • website: www.carolstream.org

FENCE VARIATION

Please see Section 16-8-4(K) of the Unified Development Ordinance (UDO) to learn more about Fence Variations.

In accordance with the applicable statutes of the State of Illinois, no variation shall be made by the Village Board except after a Public Hearing is held before the Zoning Board of Appeals.

Both the Zoning Board of Appeals and Village Board must decide if the requested variation is in harmony with the general purpose and intent of the UDO and if there is a practical difficulty of hardship in carrying out the strict letter of the regulations of the UDO.

The Zoning Board of Appeals may vary the provisions of the Fence Code after making findings based upon the evidence presented with regards to the following conditions: (Please respond to each of the following as it relates to your request.)

1. An exceptional situation related to topography, surroundings or conditions of a specific piece of property, or by reason of exceptional narrowness or shallowness.
Because of the orientation of the building, where the front entrance of the building is off
of Morton Road, a fence is needed to enclose the parking lot for security purposes.
While this is the side yard of the building, from a setback perspective, it is the front yard
by code. Thus, variation is needed to install a fence past the North Ave facing elevation of the
building.
2. Difficulties or particular hardship in the way of carrying out the strict letter of the Fence Standards.
The orientation of the building creates a fence related hardship by requiring to fence in the southern
area of the development from a security perspective. The property does not function from a
north/south building orientation. With an east/west orientation, the parking lot and access
areas operate to Mainfreights standard. Because there are goods being stored on site
(for short durations), site security is a priority.
3. The fence will not impair an adequate supply of light and air to adjacent property.
The fence will be wrought iron and decorative in nature. The fence will be setback far
enough from the building and property line that light and air will not be issues from an
installation and placement perspective.

4. The fence will not endanger the public safety.
The proposed fence will not endanger the public safety, rather it will enhance the safety of
the property, Mainfreight employees, and deter would be criminals from committing theft
related crimes.

5. The fence will not unreasonably diminish or impair established property values within the
surrounding area.
The fence will be a high quality, decorative addition to the property, and further beautify
the aesthetic of the development. The fence will also help screen trailers from view of North
Avenue.

6. The fence will not impair the public health, safety, comfort, morals or welfare of the
inhabitants of the village.
The proposed fence will not impair the public health, safety, comfort, morals or welfare
of Village residents, rather it is meant as a quality addition to the development, and will
only add to the wellbeing of Mainfreight employees, and success of the development.

7. Other pertinent information or reason for the request.



FORM C

Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188
630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org ▪ website: www.carolstream.org

SPECIAL USE APPLICATION

Uses designated under the various zoning districts herein as special uses are so classified because they may have site-specific impacts that require the discretionary review of the Plan Commission and Village Board. Each special use request must be reviewed based on its unique character, with consideration being given to the proposals impact upon neighboring properties. (Please address each of the following standards as it relates to your request.)

No special use shall be recommended by the Plan Commission nor approved by the Village Board, unless the special use:

1. Is deemed necessary for public convenience at the location.
 The use would significantly improve existing conditions and public convenience at _____
 the subject property by reducing the amount of truck traffic. _____

2. Will not be unreasonably detrimental to or endanger the public health, safety, morals, comfort or general welfare.
 The proposal will not be to the detriment or endanger the public health, safety, morals, _____
 comfort, or general welfare of the community or surrounding properties. _____

3. Will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.
 The proposal will improve traffic flow, stormwater management, and air quality of _____
 the immediate area. _____

4. Will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.
 The proposal will not impede or disrupt orderly development surrounding the subject _____
 property, rather it will improve the area overall. _____

5. Will provide adequate utilities, access roads, drainage and other important and necessary community facilities.

The proposal will abide by and meet all requirements and regulations as it pertains to
utilities, access roads, drainage, and all other codes and ordinances.

6. Will conform to the applicable regulations of the district in which it is located, except as such regulations may in each instance be modified by the Village Board.

The Petitioner is committed to meeting all regulations and requirements set forth by the
Village of Carol Stream.

7. Other pertinent information or reason for request.



FORM D-1

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Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188

630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org ▪ website: www.carolstream.org

MAP AMENDMENT

Please see Section 16-8-4(N) of the Unified Development Ordinance (UDO) to learn more about Map Amendments.

In accordance with the applicable statutes of the State of Illinois, an amendment shall be granted or denied by the Village Board only after a public hearing before the Plan Commission and a report of its findings and recommendations has thereafter been submitted to the Village Board.

The decision to amend the Official Zoning Map is a matter of legislative discretion that is not controlled by any single review criterion. In making recommendations and decisions on Map Amendments, the Plan Commission and Village Board must consider all relevant factors, including at minimum the following criteria: (Please respond to each of these standards in writing below as it relates to your request.)

1. The existing uses and zoning of nearby property.
 All adjacent property is in unincorporated DuPage County besides the property to the

 south, which is zoned B-3 General Business District. Surrounding uses include a place of

 worship, single-family residential, an office complex, a restaurant, and several light

 industrial uses.

2. The extent to which property values are diminished by the current zoning classification.
 The existing property is currently in unincorporated DuPage County. It consists of

 no stormwater infrastructure, and is a gravel surface lot which emits dust into the

 air to adjacent properties. The use generates significant truck traffic daily, which

 will be greatly reduced through the proposed development.

3. The extent to which the diminishment of property value of the applicant promotes the health, safety, morals or general welfare of the public.
 Removing the existing use would increase safety by decreasing truck traffic, improve the air

 quality, and improve stormwater management all in the immediate area.

4. The relative gain to the public as opposed to the hardship imposed upon the individual property owner.

The property is in unincorporated DuPage County. We (Petitioner) are requesting an industrial zoning classification to improve the property, tie into Village utilities, and remove an existing non conforming use (by Village standards) currently in the County.

5. The suitability of the subject property for the zoned purposes.

The property will fit into the into the industrial zoning category. If approved for a special use, the property will be suitable and fit within the zoning and land use ordinance set forth by the Village of Carol Stream.

6. The length of time the property has been vacant as zoned considered in the context of land development in the area.

The property has always existed in the County, and has been operated as a truck parking and storage facility for over 30 years.

7. Community need for the use proposed by the applicant.

The economic development benefits by the proposed development are significant. The development would generate 60 high quality jobs, would locate a North American corporate HQ into Carol Stream, would significantly increase the EAV on the property which equals additional tax revenue to the Village, and would clean up a non-conforming use.

8. Other pertinent information or reason for the request.

Mainfreight is interested and will be installing several energy efficient designs/systems into their development, including but not limited to; solar panels, greywater collection, and rainwater harvesting.

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COMMUNITY DEVELOPMENT
DEPT



TIMBERHILL

Village of Carol Stream
Plan Commission, Zoning Board of Appeals, Village Board
500 N. Gary Avenue
Carol Stream, IL 60188

RE: 27W364 North Avenue Cover Letter

Dear PC, ZBA, and Village Board,

North Ave TH LLC represents the property entity under the umbrella company Timber Hill Group. Timber Hill Group is a private equity firm specializing in the acquisition and development of logistics related industrial real estate throughout the U.S. The company is based in Chicago, and we are incredibly excited to propose a high-quality, class A development in our backyard.

We recently purchased the subject property and propose to demo the existing gravel parking lot and associated smaller structures near North Avenue to construct a 57,100 (47,100 SF footprint) square foot office and truck terminal structure with associated auto and truck/trailer parking. The development is a build-to-suit project with Mainfreight Inc, who is a New Zealand based freight carrier which focuses on transport, air & ocean, and warehousing of goods throughout the world. The company has a footprint and operations in over 20 countries and has been operating for over 40 years. Mainfreight intends on moving its North American corporate headquarters from Los Angeles to the subject property, along with the creation of 60 high quality jobs to the office component of the building.

The development would consist of 92 auto stalls, 44 tractor stalls, 86 trailer stalls, and 81 dock positions. The parking lot is proposed to be setback at least 60' along all residential property lines, including a 189' setback directly to the north. The parking lot also consists of a 21.1' setback to the east, and a 40' setback to the west. The office portion of the building consists of a 153' setback from the southern property line, while the terminal portion consists of a 265' setback from the southern property line.

In terms of screening, the property will have a 60' natural/forested landscape buffer along the residential property lines, and a 6' opaque fence. The property will be fully lit, however light levels along all property lines will measure 0.0 footcandles. Mainfreight and Timber Hill will ensure no glare spills over into adjacent properties.

Mainfreight is also committed to reducing its impact on the environment while promoting several sustainable initiatives. As such, all upcoming design build projects for Mainfreight include use of solar panels, greywater collection, and EV charging stations. Mainfreight intends on installing roof mounted solar panels along the entire terminal portion of the building, a greywater system which will consist of a holding tank and be able to reuse water onsite for potable use, and several electric vehicle charging stations for EV automobiles. From a regulation perspective, we will meet all zoning regulations and further requirements by the Village during the permitting and final design stages.

From an operations standpoint, Mainfreight will operate 7AM-5PM. The site will typically see 14 in/out truck movements per day from local pickup and delivery. There are additional network line haul trailers that go cross-country that will add approximately 10 in/outs a day to the operation. When not in use, many of the trailers will be stored onsite. Trucks will utilize both access points and will not travel northbound onto Morton Road. There will also be no refrigerated trailer or fueling on-site.

We engaged KLOA to conduct a traffic study at the site, and findings indicated that North Avenue and other arterial and collector roadways consist of sufficient capacity to handle the proposed development. In fact, the development will significantly improve the circulation and trip generation of the existing use on-site. Our preference is for IDOT to allow us to install a signal at North Avenue and Morton Road, however the traffic generation by the proposal is not large enough to warrant a signal. We are confident the development will improve existing traffic movements and congestion in the immediate area and will be a net positive to the community.

From an access perspective, we had several meetings with Village of Carol Stream staff to determine the best way to accommodate the southern access to the site. Without formal comment from IDOT, we determined with Mainfreight input, that the right-in off Morton Road represented the best and safest circulation pattern for the site. We are confident with Mainfreight as a single tenant user and all truck drivers as Mainfreight employees, that there will be no illegal exits utilizing the right in off Morton Road to North Avenue. Mainfreight is committed to operating the site with safety at the forefront of any operations. We are also confident that with the relatively low usage of the site and significantly less trips than what previously existed, that no queuing will take place from North Avenue onto the southern access on Morton Road. Once IDOT issues guidance on the best path forward toward southern access, we will adjust any plans accordingly. Preliminary indications with IDOT show that they will not approve any access onto North Avenue. Further, we are confident any changes to access will not substantially impact the site plan or civil design, and that we can accommodate such changes during permit review.

In this resubmittal, we have submitted responses to the review letter, updated all plans accordingly, and submitted a variation application. Please let me know if you have any questions or need additional information.

Sincerely,

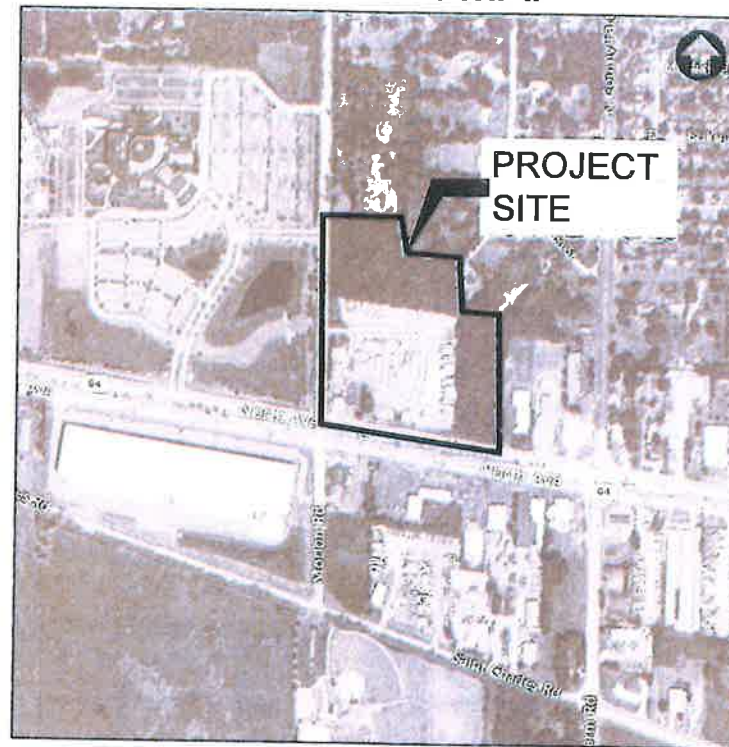
Connor Harmon, Associate Director of Development
Timber Hill Group LLC

PROPOSED IMPROVEMENTS FOR MAINFREIGHT TRUCKING TERMINAL

NORTH AVENUE & MORTON ROAD, CAROL STREAM, ILLINOIS

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LOCATION MAP



SECTION 36, TOWNSHIP 40 N, RANGE 36 E

Sheet List Table	
Sheet Number	Sheet Title
C0.0	Site Location Map & Civil Legend
C0.1	General Notes & Specifications
C1.0	Existing Conditions & Demolition Plan
C2.0	Overall Site Layout Plan
C2.1	Detailed Site Layout Plan
C3.0	Site Grading Plan
C4.0	Soil Erosion & Sediment Control Plan
C4.1	Soil Erosion & Sediment Control Details
C5.0	Site Utility Plan
C6.0	Construction Details
C6.1	Construction Details
C6.2	Construction Details
C6.3	Construction Details
C6.4	Construction Details
C6.5	Construction Details

EXISTING LEGEND	PROPOSED LEGEND
EXISTING TREE	CURB & GUTTER
CURB & GUTTER	REVERSE PITCH CURB & GUTTER
EXISTING BUILDING	DEPRESSED CURB & GUTTER
PCC SIDEWALK	PROPOSED BUILDING
GAS SERVICE	PCC SIDEWALK
ELECTRIC SERVICE	STANDARD DUTY PAVEMENT
STORM SEWER	HEAVY DUTY PAVEMENT
SANITARY SEWER	GAS SERVICE
WATER MAIN	ELECTRIC SERVICE
CABLE LINE	STORM SEWER
OVERHEAD UTILITY LINE	SANITARY SEWER
COMMUNICATION LINE	WATER MAIN
FIBER OPTIC LINE	FENCE
FENCE	STORM STRUCTURE
STORM STRUCTURE	DOWNSPOUT CONNECTION
SANITARY MANHOLE	SANITARY MANHOLE
CLEANOUT	CLEANOUT
WATER METER	WATER METER
VALVE VAULT	VALVE VAULT
VALVE BOX	VALVE BOX
HYDRANT	HYDRANT
GAS METER	GAS METER
ELECTRIC METER	ELECTRIC METER
PARKING LOT LIGHT	PARKING LOT LIGHT
UTILITY POLE	FLOW ARROW
GUY WIRE	OVERLAND FLOOD ROUTE
TRANSFORMER	100.00 TW TOP OF SIDEWALK GRADE
FIBER OPTIC BOX	100.00 TC TOP OF CURB GRADE
FIBER OPTIC PEDESTAL	100.00 P PAVEMENT GRADE
CABLE PEDESTAL	100.00 G GROUND GRADE
PHONE PEDESTAL	100 MAJOR CONTOUR
ELECTRIC PEDESTAL	100 MINOR CONTOUR

SURFACE WATER DRAINAGE STATEMENT

TO THE BEST OF OUR KNOWLEDGE AND BELIEF, THE DRAINAGE OF THE SURFACE WATERS WILL NOT BE CHANGED BY THE CONSTRUCTION OF THIS PROJECT OR ANY PART THEREOF, OR, THAT IF DRAINAGE WILL BE CHANGED, REASONABLE PROVISION HAS BEEN MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS INTO PUBLIC AREAS, OR DRAINS APPROVED FOR USE BY THE DIRECTOR OF PUBLIC WORKS AND ENGINEERING, AND THE SUCH SURFACE WATERS ARE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGE TO ADJOINING PROPERTIES BECAUSE OF THE CONSTRUCTION OF THIS PROJECT.

07/21/2023
DATE

ENGINEER

2200 CARBET DRIVE
SUITE 322
LITTLE ROCK, AR 72202
P: 501.343.0007
WWW.CAGECIVIL.COM



REVISIONS

NO.	DATE	DESCRIPTION

08/19/2023 CITY RESUBMITTAL

TH LOGISTICS FUND LLP
MAINFREIGHT TRUCKING
TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS

PROJ. NO. 220302
ENG. KH/MCK/JGN
DATE 07.28.2023

SHEET TITLE
SITE
LOCATION
MAP & CIVIL
LEGEND









SHEET NUMBER
C0.0
1 OF 15



EXHIBIT A

EXHIBIT B

DEMOLITION LEGEND

-  BUILDING DEMOLITION
-  ASPHALT/GRAVEL PAVEMENT REMOVAL, FULL DEPTH
-  CONCRETE PAVEMENT / SIDEWALK REMOVAL, FULL DEPTH
-  SAWCUT PAVEMENT, FULL DEPTH
-  CURB & GUTTER REMOVAL
-  UTILITY REMOVAL
-  TREE/BUSH REMOVAL
-  UTILITY STRUCTURE REMOVAL (TBR) TO BE REMOVED

SOURCE BENCHMARK:
"TRIMBLE R10 VRS NETWORK"
NAVD83

- SITE BENCHMARK
1. IRON PIPE WITH CAP ALONG THE WEST EDGE OF PAVEMENT OF MORTON ROAD, 3 FEET NORTH OF FIRE HYDRANT. SAID IRON WITH CAP IS OPPOSITE THE DRIVEWAY AT 2N579 MORTON ROAD ALONG THE NORTH LINE OF THE SUBJECT SITE. (ELE = 762.79)
 2. CROSS CUT IN CURBLINE AT THE NORTHWEST CORNER OF NORTH (IL ROUTE 64) AVENUE AND MORTON ROAD, AT THE CORNER OF WHEATON BIBLE CHURCH. SAID CROSS IS 42 FEET SOUTHWEST OF WESTERLY OF 2 MANHOLES IN THE SOUTHBOUND LANE OF MORTON ROAD AND 104 FEET NORTHWEST OF THE CENTERLINE OF NORTH AVENUE AND MORTON ROAD (ELE = 780.87)
 3. CROSS CUT IN CONCRETE ALONG THE NORTH EDGE OF NORTH AVENUE ROADWAY. SAID CROSS IS 1.5 FEET SOUTH OF SAID NORTH EDGE, OPPOSITE EASTERLY OF 3 30 IN FLARED END SECTIONS ALONG NORTH RIGHT-OF-WAY AND 160 FEET WEST OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF LOT 33. (ELE = 757.01)

2200 CABOT DRIVE
SUITE 325
LISLE, IL 60532
P. 630.598.0007
WWW.CAGECIVIL.COM



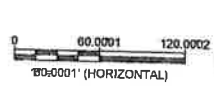
REVISIONS Δ

08/12/2023 CITY SUBMITTAL

TH LOGISTICS FUND I LP
MAINFREIGHT TRUCKING TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS



JULIE
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PROJ NO 220302
ENG. KH/MCK/JGN
DATE: 07.28.2023

SHEET TITLE
EXISTING CONDITIONS & DEMOLITION PLAN

SHEET NUMBER
C1.0
3 OF 15

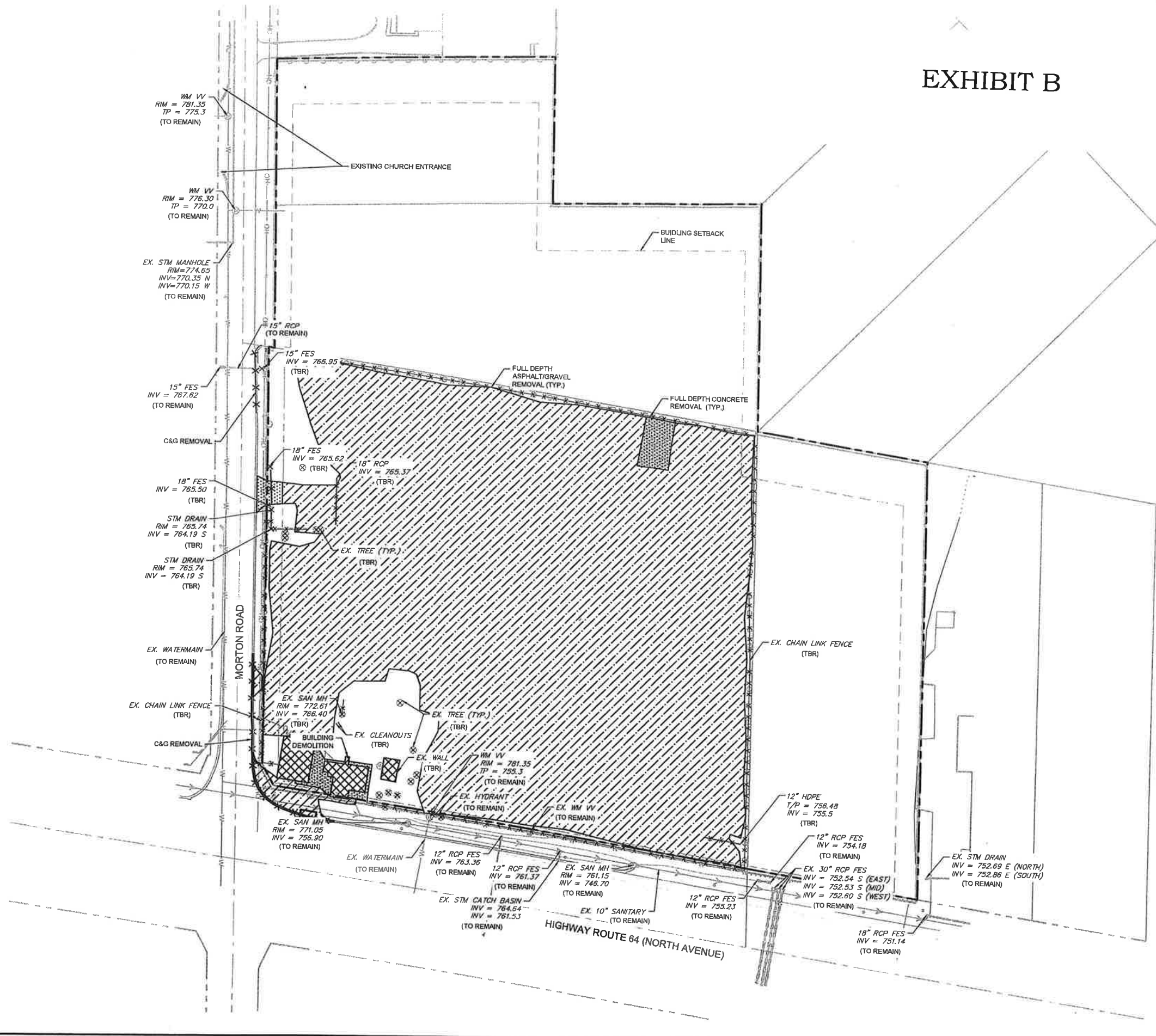
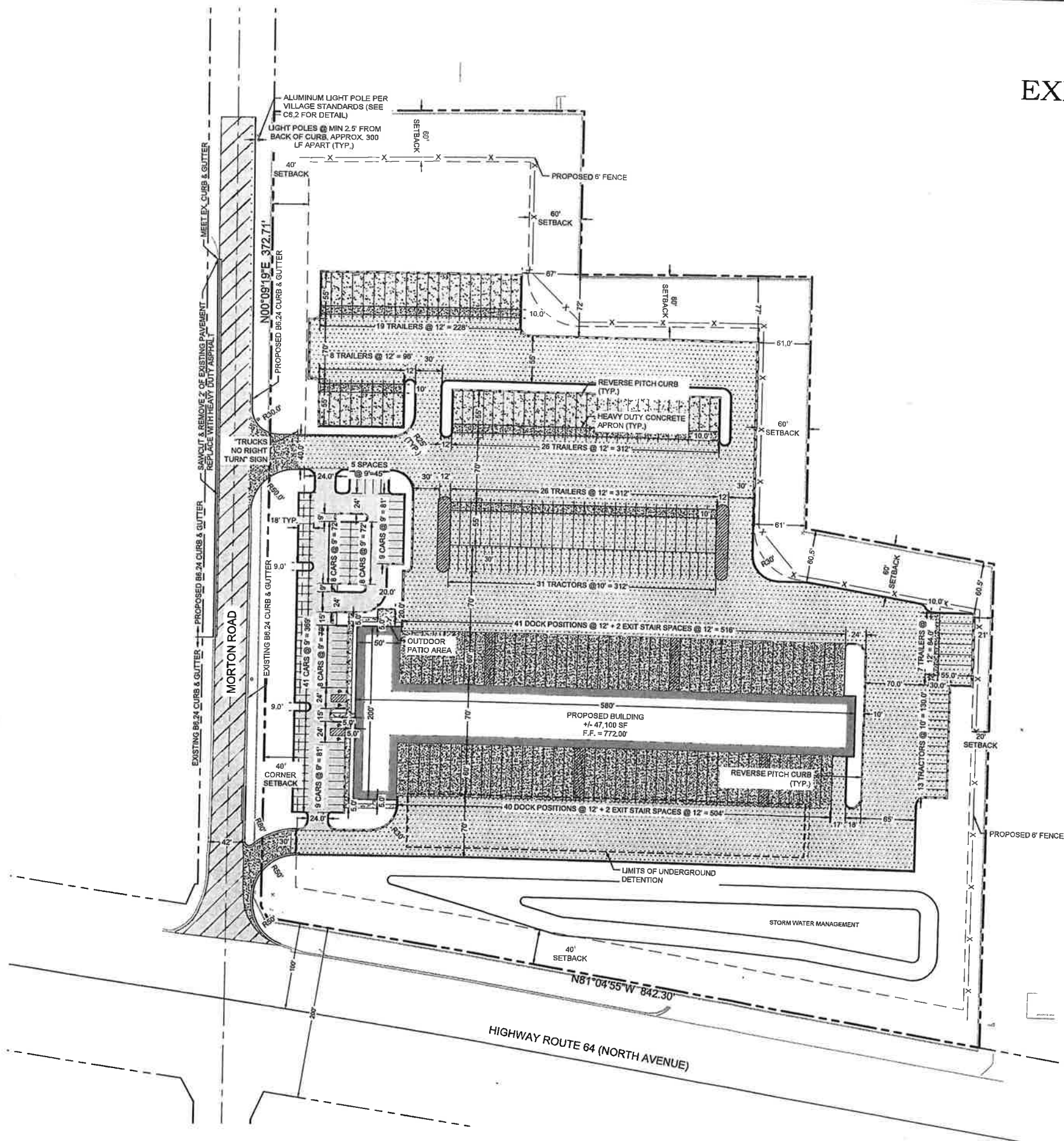


EXHIBIT C-1



PAVEMENT LEGEND

- STANDARD DUTY BITUMINOUS PAVEMENT
- HEAVY DUTY BITUMINOUS PAVEMENT
- 2" MILL & OVERLAY
- HEAVY DUTY CONCRETE PAVEMENT
- PCC SIDEWALK

SITE DATA TABLE:

BUILDING REQUIREMENTS
 EXISTING ZONING CLASSIFICATION: DUPAGE COUNTY R2 - SINGLE FAMILY
 PROPOSED ZONING CLASSIFICATION: CAROL STREAM I - INDUSTRIAL DISTRICT

SITE AREA = 686,580 SF = 15.78 ACRES
 NET OFFICE AREA = 10,000 SF
 NET TERMINAL AREA = 37,100 SF
 TOTAL BUILDING AREA = 47,100 SF

BUILDING SETBACKS

FRONT	60 FT
SIDE	10 FT
REAR	20 FT

PARKING REQUIREMENTS:

I - INDUSTRIAL DISTRICT
 OFFICE PARKING = ONE (1) SPACE FOR EACH 1,500 SPACE FEET OF FREIGHT TERMINAL NET FLOOR AREA (NFA) AND ONE (1) SPACE FOR EACH 250 SQUARE FEET OF OFFICE NET FLOOR AREA (NFA)

REQUIRED PARKING = 90 SPACES
 TOTAL PARKING PROVIDED = 88 SPACES
 ADA PARKING = 4 SPACES

SHEET NOTES:

ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE TO THE VILLAGE OF CAROL STREAM'S REQUIREMENTS. SECTION 18-5-2 (C). ALL PAVEMENT STRIPING SHALL BE FOUR (4) INCH HAIR-PIN LOOPED LINES, 16 INCHES ON CENTER.

SOURCE BENCHMARK:

TRIMBLE R10 VRS NETWORK
 NAVD88

SITE BENCHMARK

- IRON PIPE WITH CAP ALONG THE WEST EDGE OF PAVEMENT OF MORTON ROAD, 3 FEET NORTH OF FIRE HYDRANT. SAID IRON WITH CAP IS OPPOSITE THE DRIVEWAY AT 25579 MORTON ROAD ALONG THE NORTH LINE OF THE SUBJECT SITE. (ELE = 782.75)
- CROSS CUT IN CURBLINE AT THE NORTHWEST CORNER OF NORTH (IL ROUTE 64) AVENUE AND MORTON ROAD, AT THE CORNER OF WHEATON BIBLE CHURCH. SAID CROSS IS 42 FEET SOUTHWEST OF WESTERLY OF 2 MANHOLES IN THE SOUTHWEST CORNER OF MORTON ROAD AND 104 FEET NORTHWEST OF THE CENTERLINE OF NORTH AVENUE AND MORTON ROAD (ELE = 780.87)
- CROSS CUT IN CONCRETE ALONG THE NORTH EDGE OF NORTH AVENUE ROADWAY. SAID CROSS IS 1.5 FEET SOUTH OF SAID NORTH EDGE, OPPOSITE EASTERLY OF 3 30 IN FLARED END SECTIONS ALONG NORTH RIGHT-OF-WAY AND 188 FEET WEST OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF LOT 33. (ELE = 757.01)

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REVISIONS

NO.	DATE	DESCRIPTION

LOGISTICS FUND 1/P
MAINFREIGHT TRUCKING TERMINAL
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, ILLINOIS



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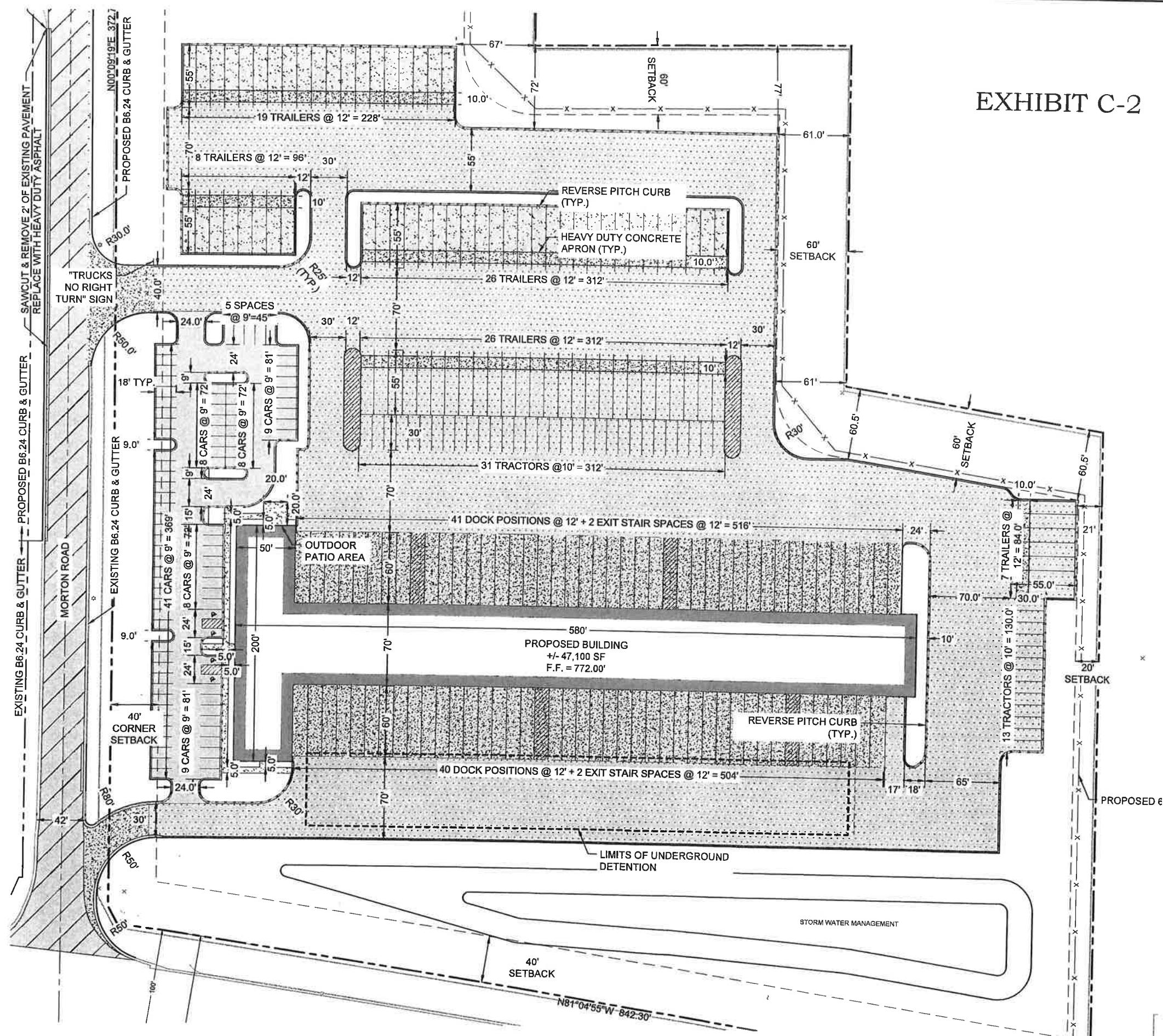


PROJ NO: 220302
 ENG: KHHMCK/JGN
 DATE: 07.28.2023

SHEET TITLE
OVERALL SITE LAYOUT PLAN

SHEET NUMBER
C2.0
 4 OF 15

EXHIBIT C-2



PAVEMENT LEGEND

[Pattern]	STANDARD DUTY BITUMINOUS PAVEMENT
[Pattern]	HEAVY DUTY BITUMINOUS PAVEMENT
[Pattern]	2" MILL & OVERLAY
[Pattern]	HEAVY DUTY CONCRETE PAVEMENT
[Pattern]	PCC SIDEWALK

SITE DATA TABLE:

BUILDING REQUIREMENTS

EXISTING ZONING CLASSIFICATION: DUPAGE COUNTY R2- SINGLE FAMILY

PROPOSED ZONING CLASSIFICATION: CAROL STREAM I- INDUSTRIAL DISTRICT

SITE AREA = 686,580 SF = 15.76 ACRES
 NET OFFICE AREA = 10,000 SF
 NET TERMINAL AREA = 37,100 SF
 TOTAL BUILDING AREA = 47,100 SF

BUILDING SETBACKS

FRONT	60 FT
SIDE	10 FT
REAR	20 FT

PARKING REQUIREMENTS:

I-INDUSTRIAL DISTRICT

OFFICE PARKING = ONE (1) SPACE FOR EACH 1,500 SPACE FEET OF FREIGHT TERMINAL NET FLOOR AREA (NFA) AND ONE (1) SPACE FOR EACH 250 SQUARE FEET OF OFFICE NET FLOOR AREA (NFA)

REQUIRED PARKING = 90 SPACES

TOTAL PARKING PROVIDED = 88 SPACES

ADA PARKING = 4 SPACES

SHEET NOTES:

ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE TO THE VILLAGE OF CAROL STREAM'S REQUIREMENTS. SECTION 16-S-2 (C). ALL PAVEMENT STRIPING SHALL BE FOUR (4) INCH HAIR-PIN, LOOPED LINES, 16 INCHES ON CENTER.

SOURCE BENCHMARK:
 TRIMBLE R10 VRS NETWORK
 NAVD88

SITE BENCHMARK

1. IRON PIPE WITH CAP ALONG THE WEST EDGE OF PAVEMENT OF MORTON ROAD, 3 FEET NORTH OF FIRE HYDRANT. SAID IRON WITH CAP IS OPPOSITE THE DRIVEWAY AT 2N579 MORTON ROAD ALONG THE NORTH LINE OF THE SUBJECT SITE. (ELE = 782.75)

2. CROSS CUT IN CURBLINE AT THE NORTHWEST CORNER OF NORTH (IL ROUTE 64) AVENUE AND MORTON ROAD, AT THE CORNER OF WHEATON BIBLE CHURCH. SAID CROSS IS 42 FEET SOUTHWEST OF WESTERLY OF 2 MANHOLES IN THE SOUTHBOUND LANE OF MORTON ROAD AND 104 FEET NORTHWEST OF THE CENTERLINE OF NORTH AVENUE AND MORTON ROAD (ELE = 780.87)

3. CROSS CUT IN CONCRETE ALONG THE NORTH EDGE OF NORTH AVENUE ROADWAY. SAID CROSS IS 1.5 FEET SOUTH OF SAID NORTH EDGE, OPPOSITE EASTERLY OF 3 3/8 IN FLARED END SECTIONS ALONG NORTH RIGHT-OF-WAY AND 168 FEET WEST OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF LOT 33. (ELE = 757.01)

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REVISIONS

TH LOGISTICS FUND LP
MAINFREIGHT TRUCKING TERMINAL
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, ILLINOIS

811 JULIE CALL BEFORE YOU DIG 811

PROJ. NO: 220302
 ENG: KHM/MCK/JGN
 DATE: 07.28.2023

SHEET TITLE
DETAILED SITE LAYOUT PLAN

SHEET NUMBER
C2.1
 5 OF 15

1" = 40' (HORIZONTAL)

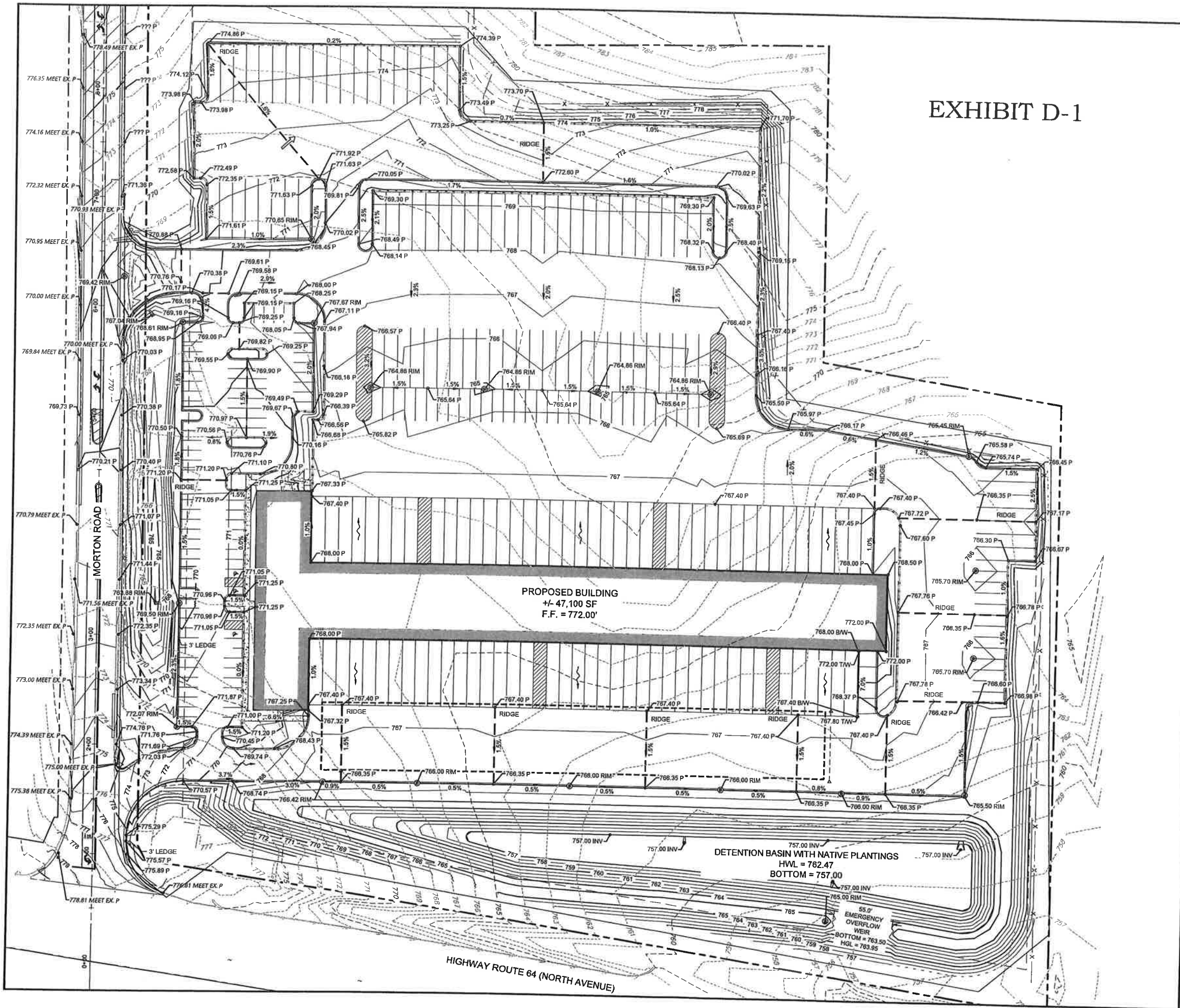


EXHIBIT D-1

GRADING LEGEND

- DRAINAGE ARROW
- OVERLAND FLOOD ROUTE
- SPOT ELEVATION
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR

SHEET NOTES

- STORM SEWER IS DESIGNED FOR THE 100-YEAR EVENT.
- DETENTION BASIN SHALL BE PLANTED WITH NATIVE PLANTINGS IN ACCORDANCE WITH THE DUPAGE COUNTY STORMWATER ORDINANCE.

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REVISIONS

NO.	DESCRIPTION

TH LOGISTICS FUND I LP
MAINFREIGHT TRUCKING TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS

PROJ NO 220302
ENG KH/MCK/JGN
DATE 07.28.2023

SHEET TITLE
SITE GRADING PLAN

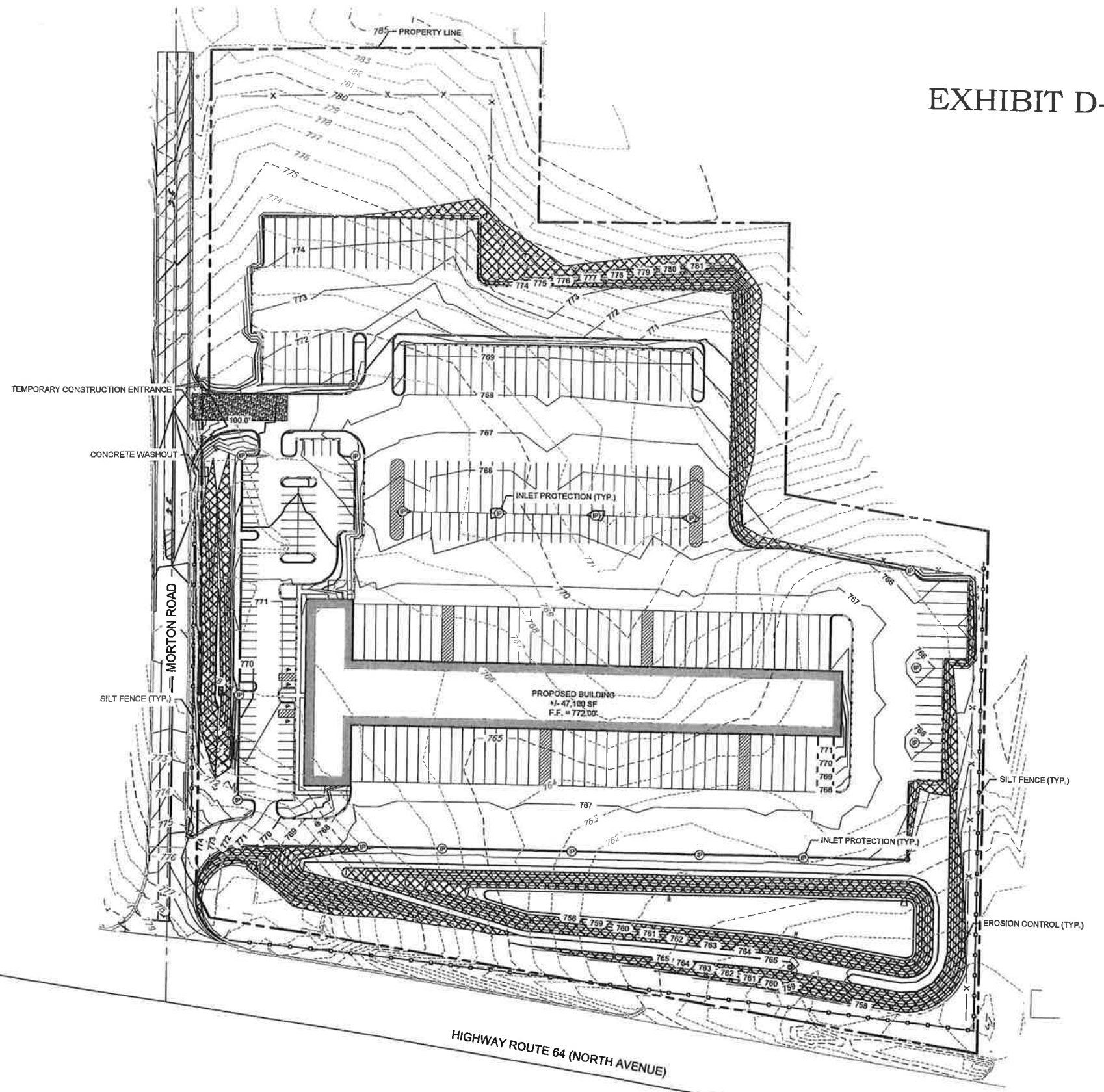
SHEET NUMBER
C3.0
6 OF 15



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EXHIBIT D-2



SESC LEGEND

- SILT FENCE
- TREE PROTECTION FENCE
- EROSION CONTROL BLANKET
- RIPRAP
- TEMPORARY CONSTRUCTION ENTRANCE
- SEDIMENT LOG
- INLET PROTECTION FILTER BASKET
- CONCRETE WASHOUT

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REVISIONS

NO.	DATE	DESCRIPTION

06/18/2023 CITY RESUBMITAL

TH LOGISTICS FUND, LP
MAINFREIGHT TRUCKING TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS

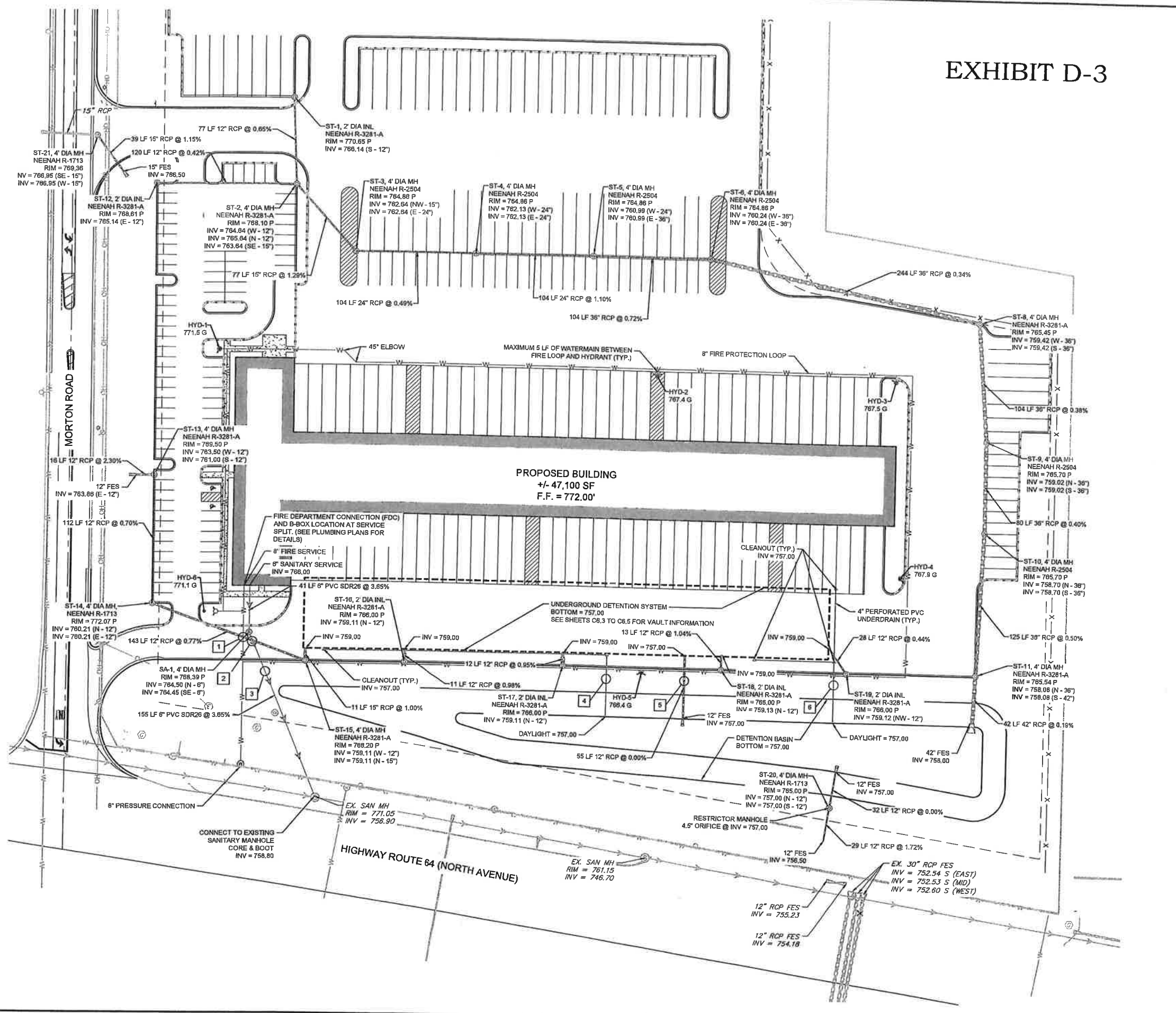


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PROJ NO: 220302
ENG: KHM/MCK/JGN
DATE: 07.28.2023
SHEET TITLE
SOIL EROSION & SEDIMENT CONTROL PLAN
SHEET NUMBER
C4.0
7 OF 15

EXHIBIT D-3



UTILITY LEGEND

- PROPOSED SANITARY SEWER
- PROPOSED SANITARY MANHOLE
- PROPOSED SANITARY CLEANOUT
- PROPOSED WATER LINE
- PROPOSED WATER VALVE BOX
- PROPOSED STORM PIPE
- PROPOSED STORM STRUCTURE
- EXISTING SANITARY SEWER LINE
- EXISTING WATER LINE
- EXISTING STORM PIPE
- EXISTING FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING SANITARY MANHOLE

STORMWATER SUMMARY

REQUIRED VOLUME:	7.22 AC-FT
PROVIDED VOLUME:	7.55 AC-FT
EXISTING RELEASE RATE:	61.73 CFS
PROVIDED RELEASE RATE:	61.21 CFS
HIGH WATER LEVEL:	782.47
BOTTOM OF SYSTEM:	757.00

4.5" ORIFICE AT 757.00 IN THE RESTRICTOR MANHOLE

*SEE CONSTRUCTION DETAILS C6.3 TO C6.5 FOR UNDERGROUND DETENTION DRAWINGS

EMERGENCY OVERFLOW WEIR:

LENGTH:	55 FT
BOTTOM:	763.50
HGL:	763.95
TOP OF EMBANKMENT:	765.00

ALL STORM SEWER IS DESIGNED FOR THE 100-YEAR EVENT.

HYDRANT RING GRADES SHALL BE 0.2' ABOVE THE FG SURFACE LABELED ON THE PLANS.

UTILITY CROSSINGS

1	12" ST B/P = 759.40± 8" WM T/P = 757.90±
2	8" SAN B/P = 764.30± 12" ST T/P = 760.70±
3	6" SAN B/P = 763.10± 8" WM T/P = 760.22±
4	4" ST B/P = 757.00± 8" WM T/P = 755.50±
5	12" ST B/P = 756.83± 8" WM T/P = 755.33±
6	4" ST B/P = 757.00± 8" WM T/P = 755.50±

* - INDICATES WM TO BE DIPPED MIN 1.5' BELOW CROSSING PIPE

2200 CABOT DRIVE
SUITE 325
Lisle, IL 60532
P. 630.588.0007
WWW.CAGECIVIL.COM

CAGE
CIVIL ENGINEERS, INC.

PROFESSIONAL ENGINEER
STATE OF ILLINOIS
NO. 065-035940

REVISIONS

TH LOGISTICS FUND I LP
MAINFREIGHT TRUCKING TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS

PROJ. NO. 220302
ENG. KH/MCK/JGN
DATE: 07.28.2023

SHEET TITLE
SITE UTILITY PLAN

SHEET NUMBER
C5.0
9 OF 15

811
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1" = 40' (HORIZONTAL)

Scale bar and North arrow.

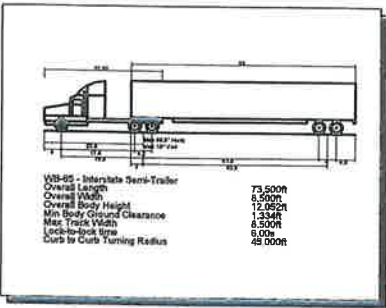
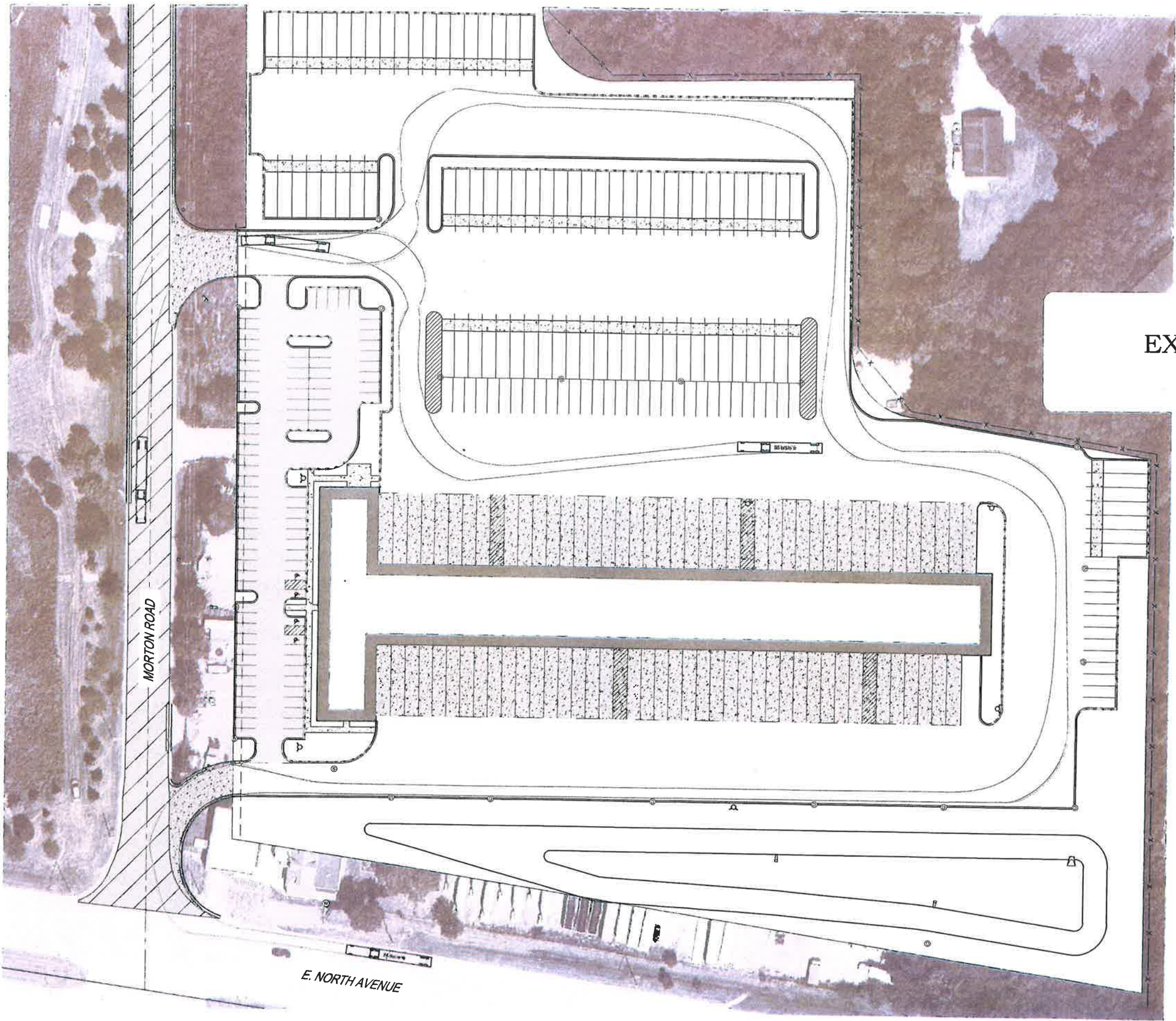
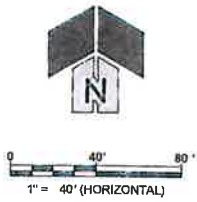


EXHIBIT E-1

RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT



2008 CARBOT DRIVE
SCALE: AS SHOWN
21000 S. 1000 E. 1000 N.
WINNECAGO, IL 60093
WWW.CAGECIVIL.COM



REVISIONS	DATE

TH LOGISTICS FUND I/P
**PROPOSED FREIGHT
TERMINAL**
NORTH AVENUE & MORTON ROAD
CAROL STREAM, IL

PROJ NO: 220302
ENG: KHH
DATE: 08/18/2023

SHEET TITLE
**TURNING
EXHIBIT - WB65**

SHEET NUMBER
EX-1
NA OF NA

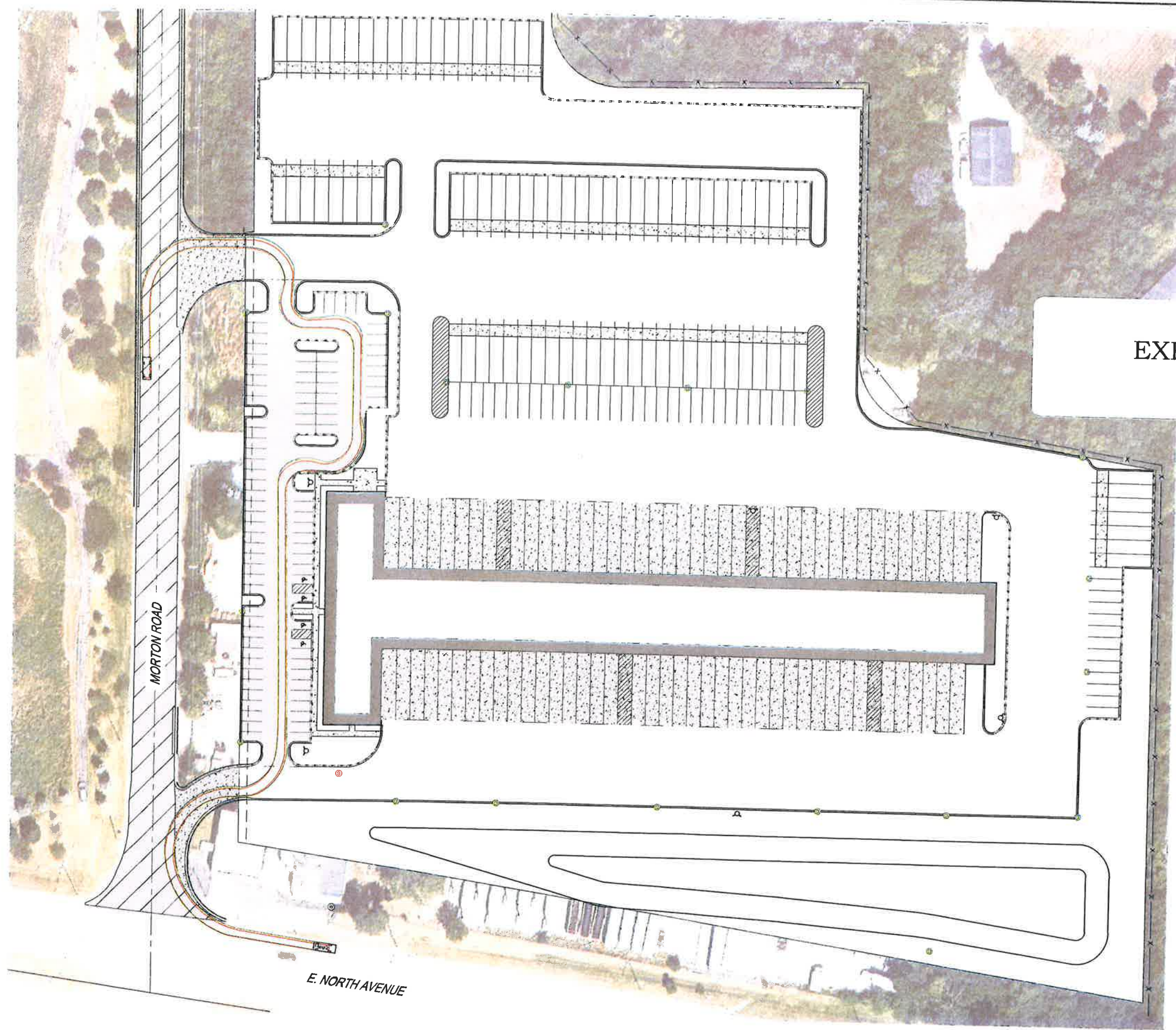


EXHIBIT E-2

	P - Passenger Car	19.000'
	Overall Length	7.000'
	Overall Width	5.100'
	Overall Body Height	7.100'
	Min. Body Ground Clearance	6.000'
	Track Width	4.500'
	Lock-to-lock time	24.000"
	Curb to Curb Turning Radius	

Z206 CABOT DRIVE
SUITE 325
LISLE, IL 60532
P: 639.556.0087
WWW.CAGECIVIL.COM



REVISIONS	Δ

RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT



0 40' 80'
1" = 40' (HORIZONTAL)

TH LOGISTICS FUND/ILP
**PROPOSED FREIGHT
TERMINAL**
NORTH AVENUE & MORTON ROAD
CAROL STREAM, IL

PROJ NO 220302
ENG K/H
DATE 08/18/2023

SHEET TITLE
**TURNING
EXHIBIT - WB65**

SHEET NUMBER
EX-1
NA OF NA

EXHIBIT F-1

LG Workshop, LLC
 Landscape Architecture
 Site Planning
 Illustration
 1955 N. Wilmot Ave.
 Chicago, IL 60647
 ph. 773.697.4388
 www.LGWLA.com
SEAL



PROJECT TEAM

CIVIL ENGINEER:
CAGE CIVIL

RECEIVED
 AUG 21 2023
 COMMUNITY DEVELOPMENT
 DEPT

PROJECT NAME

Mainfreight
 Trucking

North Ave & Morton Rd.
 Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
1.	For Permit	08/04/2023
2.	Per Village Comments	08/18/2023

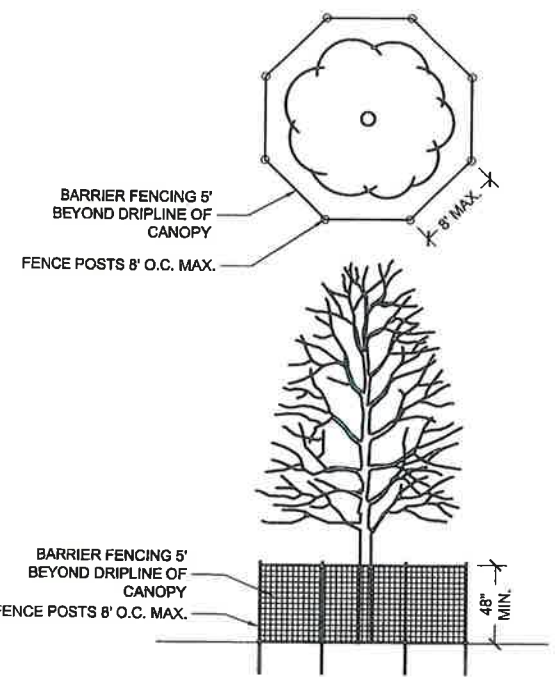
SET TYPE
 LANDSCAPE PLANS

PROJECT NUMBER
 2301004

DATE
 01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG

SHEET TITLE
 PRELIMINARY TREE
 PROTECTION & REMOVAL
 PLAN
SHEET NUMBER

L1.0



5 TREE PRESERVATION FENCING DETAIL NOT TO SCALE

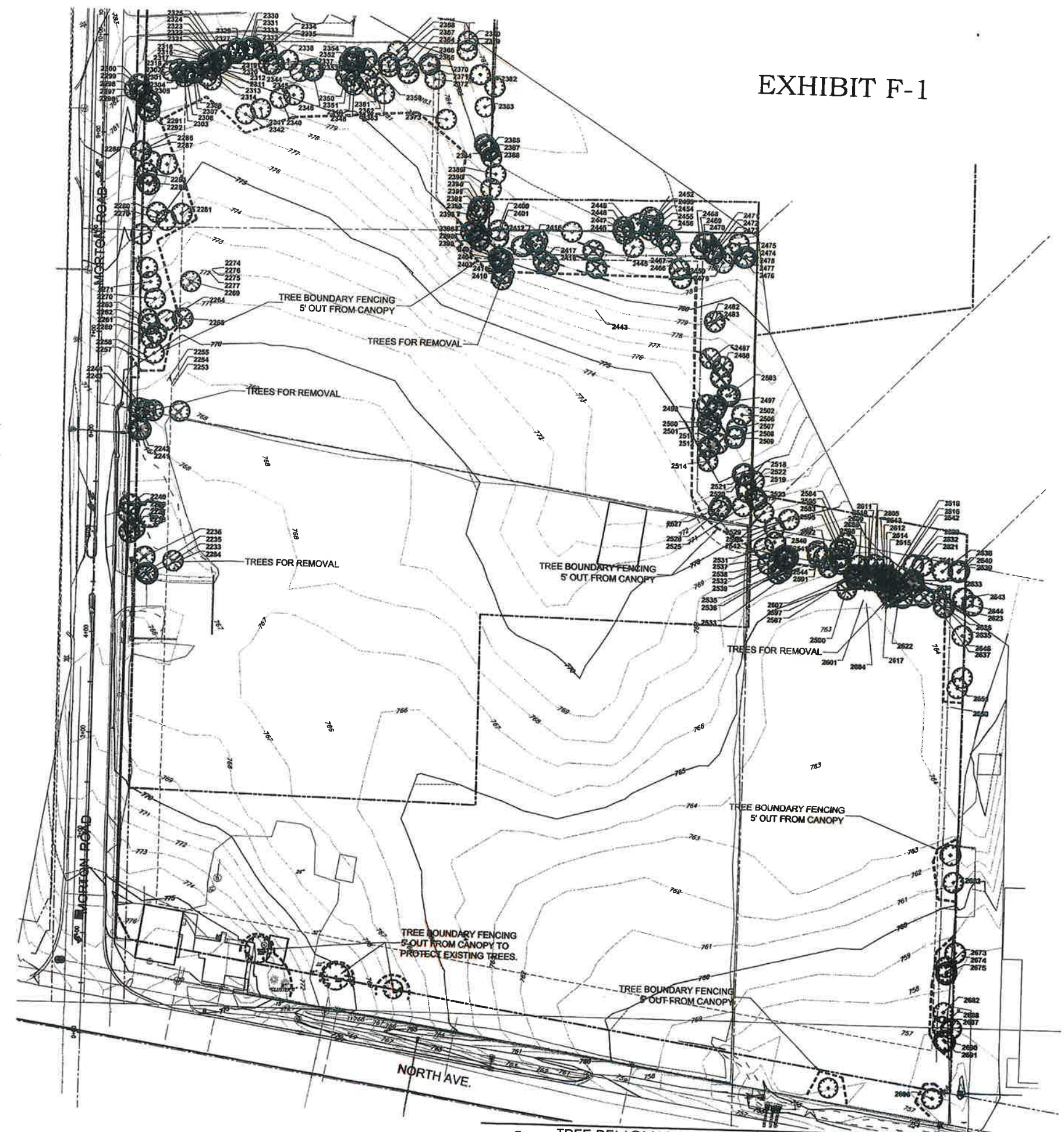
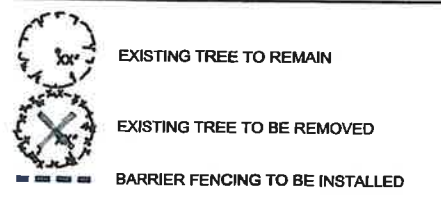
EXISTING VEGETATION DESCRIPTION

THE PROJECT SITE CONSISTS OF AN EXISTING TRUCKING LOT OF 15.73 ACRES. DENSE TREE STANDS COVER APPROXIMATELY 8.25 ACRES OF THE SITE. SITE GRADING AND LAYOUT OF PAVING WILL REQUIRE REMOVING TREES ON SITE WITH APPROXIMATELY 3.04 ACRES OF TREE STAND REMAINING ON-SITE. 40 TREES WILL BE REMOVED AS PART OF THIS CONTRACT.

TREE PROTECTION & REMOVAL NOTES

- CONTRACTOR SHALL OBTAIN ALL NECESSARY STATE AND LOCAL PERMITS AND PERMISSIONS TO PRUNE, REMOVE, AND/OR TRANSPLANT ANY TREES ON SITE.
- DEAD AND DYING MATERIAL ON THE SITE SHALL BE REMOVED OR PRUNED. MATERIALS NOT LABELED ON THE PROTECTION PLAN SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR REMEDIATION.
- DURING CONSTRUCTION EXISTING TREES OVER FOUR INCHES IN CALIPER SHALL BE PROTECTED WITH BARRIER FENCING.
- BARRIER SHALL BE CONSTRUCTED OF A MIN. 3' TALL SNOW FENCE OR SIMILAR AND SUPPORT POSTS MIN. 8' O.C. AND SHALL BE ERECTED FIVE (5') FEET BEYOND THE DRIP LINE OFF ALL EXISTING TREES ON SITE AND ADJACENT SITES TO REMAIN.
- BARRIER FENCING SHOWN ON THE PLAN IS APPROXIMATE. CONTRACTOR SHALL ADJUST LOCATION OF BARRIER TO POSITION OUTLINED IN COMMENT 4.
- NO EXCESS SOIL OR ADDITIONAL FILL, BUILDING MATERIALS OR DEBRIS SHALL BE PLACED WITHIN THE PROTECTIVE BARRIER.
- NO VEHICLES OR HEAVY MACHINERY SHALL BE ALLOWED TO WORK WITHIN THE BARRIER AREA.
- NO ATTACHMENTS OR WIRES, OTHER THAN PROTECTIVE GUY WIRES, SHALL BE ATTACHED TO ANY OF THE TREES WHICH ARE WITHIN PROTECTIVE BARRIER.
- STUMPS OR TREE REMAINS NOT TO BE FULLY EXCAVATED SHALL BE REMOVED. A STUMP GRINDER SHALL BE USED TO REMOVE ALL REMAINING ROOTS AND WOODY MATERIAL WITHIN A 24" RADIUS OF THE TREE TRUNK TO MIN. 6" BELOW GRADE. DISTURBED AREA SHALL BE BACKFILLED WITH COMPACTED TOPSOIL TO MEET SURROUNDING GRADES.

TREE PROTECTION & REMOVAL LEGEND



1 TREE REMOVAL & PROTECTION PLAN
 SCALE: 1"=50'
 0' 25' 50' 100' 150'

PRELIMINARY PLANT LIST

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT	SALT	DROUGHT
DECIDUOUS SHADE TREES							
AFA	2.5' cal.	7	Acer x freemanii 'Autumn Blaze'	Autumn Blaze Freeman Maple	B&B	M	L
ARF	2.5' cal.	11	Acer rubrum 'Frank Jr.'	Redpointe Red Maple	B&B	L	L
AMM	2.5' cal.	8	Acer miyabei 'Morton'	State Street Miyabe's maple	B&B	H	H
CEO	2.5' cal.	14	Celtis occidentalis	Common Hackberry	B&B	H	H
GBP	2.5' cal.	6	Ginkgo biloba 'Princeton Sentry'	Princeton Sentry Maidenhair Tree	B&B	H	H
GDE	2.5' cal.	7	Gymnocladus dioica 'Espresso'	Espresso Kentucky Coffeetree	B&B	H	H
GTS	2.5' cal.	15	Gleditsia triacanthos 'Shademaster'	Shademaster Honeylocust	B&B	H	H
LSM	2.5' cal.	6	Liquidambar styraciflua 'Moraine'	Moraine American Sweetgum	B&B	H	H
QUB	2.5' cal.	9	Quercus bicolor	Swamp White Oak	B&B	M	H
QUR	2.5' cal.	3	Quercus rubra	Northern Red Oak	B&B	L	H
NYS	2.5' cal.	10	Nyssa sylvatica	Black Tupelo	B&B	H	H
TAM	2.5' cal.	16	Tilia americana 'MckSentry'	American Sentry Linden	B&B	L	L
TCR	2.5' cal.	5	Tilia cordata 'Greenspire'	Greenspire Littleleaf Linden	B&B	L	L
ULA	2.5' cal.	10	Ulmus davidiana var. japonica 'Morton'	Accolade Elm	B&B	M	H
ULH	2.5' cal.	8	Ulmus 'Regal'	Regal Elm	B&B	H	H
ORNAMENTAL TREES							
AGF	6' Multi.	6	Acer ginnala 'flame'	Flame Amur Maple	B&B	M	M
AGP	6' multi.	2	Amelanchier x grandiflora.	Shadblow Serviceberry	B&B	H	M
BEN	6' multi.	15	Betula nigra	River Birch	B&B	H	M
EVERGREEN TREES							
ACO	6' ht.	28	Abies concolor	White Fir	B&B	L	H
PIS	6' ht.	40	Pinus strobus	White Pine	B&B	L	L
PGD	6' ht.	38	Picea glauca 'Densata'	Black Hills Spruce	B&B	M	H
DECIDUOUS SHRUBS							
AAB	24" ht.	29	Aronia arbutifolia 'Brilliantissima'	Brilliant Red Chokeberry	B&B	M	M
AMA	24" ht.	40	Aronia melanocarpa 'Elata'	Elata Chokeberry	B&B	M	M
CAM	24" ht.	75	Cornus alba 'Elegantissima'	Variegated Dogwood	B&B	H	L
CSF	24" ht.	19	Cornus stolonifera 'Farrow'	Arctic Fire Redtwig Dogwood	B&B	H	M
FOS	24" ht.	36	Forsythia x intermedia 'Sunrise'	Sunrise Forsythia	B&B	H	M
HYA	24" ht.	20	Hydrangea arbor. 'Haas Halo'	Haas Halo Hydrangea	B&B	M	L
HYQ	24" ht.	55	Hydrangea quercifolia 'Alice'	Alice Oakleaf Hydrangea	B&B	M	M
POB	24" ht.	21	Physocarpus opulifolius 'Monlo'	Diablo Ninebark	B&B	H	H
SBT	18" w.	25	Spiraea betulifolia 'tor'	Tor Birchleaf Spiraea	B&B	H	H
SYM	24" ht.	4	Syringa patula 'Miss Kim'	Miss Kim Korean Lilac	B&B	H	H
VTC	24" ht.	33	Viburnum trilobum 'Alfredo'	Alfredo American Cranberrybush	B&B	M	H
EVERGREEN SHRUBS							
JCS	24" w.	28	Juniperus chinensis 'Sargentii'	Green Sargent Juniper	B&B	M	H
TMT	24" w.	20	Taxus x media 'Taunton'	Taunton's Yew	B&B	L	H
ORNAMENTAL GRASSES							
CAA	#1 cont.	36	Calamagrostis acutifolia 'Strictus'	Strictus Feather Reed Grass		L	H
GROUNDCOVER / PERENNIALS							
ALS	#1 cont.	80	Allium 'summer beauty'	Summer Beauty Wild Onion	18" O.C.	H	H
CVM	#1 cont.	18	Coreopsis verticillata 'Moonbeam'	Moonbeam Coreopsis	18" O.C.	H	H
ECP	#1 cont.	30	Echinacea purpurea 'Magnus'	Magnus Purple Coneflower	24" O.C.	H	H
HEC	#1 cont.	92	Hemerocallis x 'Chicago Apache'	Chicago Apache Daylily	24" O.C.	H	H
LAV	#1 cont.	23	Lavendula 'Munstead strain'	Munstead English Lavender	24" O.C.	H	H
NFW	#1 cont.	8	Nepela fasseni 'Walker's low'	Walkers Low Catmint	24" O.C.	H	H
PHD	#1 cont.	16	Phlox divaricata	Blue Phlox	15" O.C.	H	H
RUD	#1 cont.	60	Rudbeckia fulgida 'Goldsturm'	Goldsturm Black-Eyed Susan	18" O.C.	H	H
SOD	sq. yd.	2822	Sodded Lawn				
SEED	sq. yd.	10686	Seeded Lawn				
DET	sq. yd.	7360	Salt Tolerant Wetland / Detention Seed - See Below				

Salt Tolerant Wetland / Detention Seed mix available through: Genesis Nursery Inc. Tampico, IL - 877-817-5325. Apply at manufacturers recommended rates. All slopes over 4:1 to receive BioNet SC150BN or similar Erosion Blanket.

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT	SALT	DROUGHT
LANDSCAPE LEGEND							
(Circle with 'x')				EXISTING TREE			
(Circle with '+')				PROPOSED SHADE TREE			
(Circle with 'x' and 'o')				PROPOSED EVERGREEN TREE			
(Circle with 'x' and 'o' and '+')				PROPOSED ORNAMENTAL TREE			
(Circle with 'x' and 'o' and '+') (Large)				PROPOSED LARGE SHRUB			
(Circle with 'x' and 'o' and '+') (Medium)				PROPOSED MEDIUM SHRUB			
(Circle with 'x' and 'o' and '+') (Small)				PROPOSED EVERGREEN SHRUB			
(Circle with 'x' and 'o' and '+') (Small)				PROPOSED LOW SHRUB			
(Circle with 'x' and 'o' and '+') (Small)				PROPOSED ORNAMENTAL GRASS			
(Circle with 'x' and 'o' and '+') (Small)				PROPOSED PERENNIAL PLANTING			
(Square with 'x')				SODDED LAWN			
(Square with 'x')				SEEDDED LAWN			
(Square with 'x')				SEEDDED DETENTION			

LANDSCAPE LEGEND

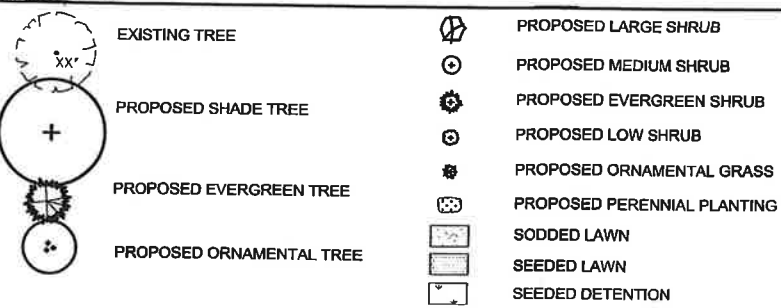
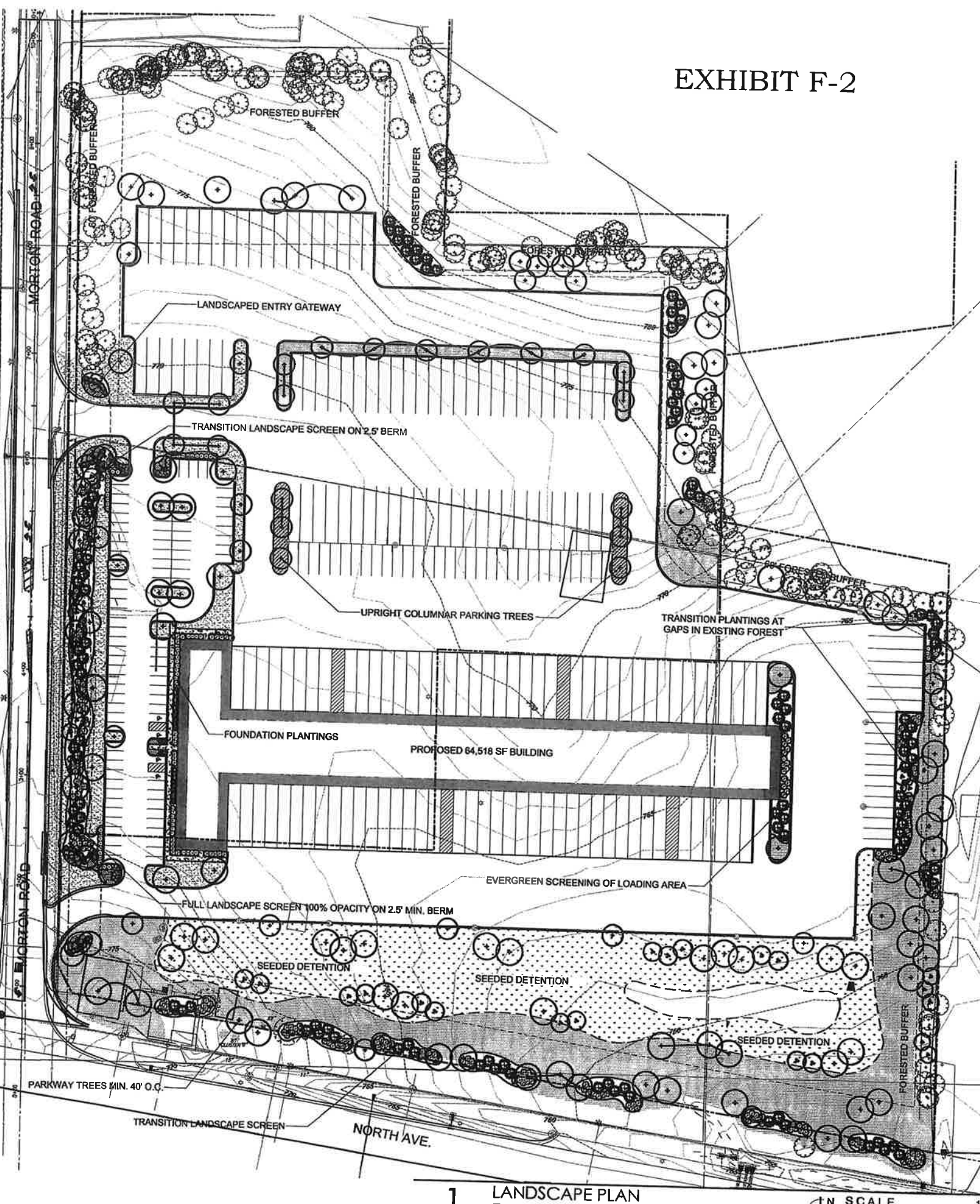


EXHIBIT F-2



LG Workshop LLC
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SEAL
LANDSCAPE ARCHITECT
LARRY G. GLASSCOCK
STATE OF ILLINOIS
EXPIRES 08/2025

PROJECT TEAM
CIVIL ENGINEER:
CAGE CIVIL

PROJECT NAME
Mainfreight
Trucking
North Ave & Morton Rd.
Carol Stream, IL

DRAWING ISSUED
NO. TITLE DATE
1. For Permit 08/04/2023
2. Per Village Comments 08/18/2023

SET TYPE
LANDSCAPE PLANS
PROJECT NUMBER
2301004
DATE
01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG
SHEET TITLE
LANDSCAPE PLAN
ENTIRE SITE
SHEET NUMBER

1 LANDSCAPE PLAN ENTIRE SITE
SCALE 1"=50'
0' 25' 50' 100' 150'



PROJECT TEAM

CIVIL ENGINEER:



PROJECT NAME

Mainfreight
Trucking

North Ave & Morton Rd.
Carol Stream, IL

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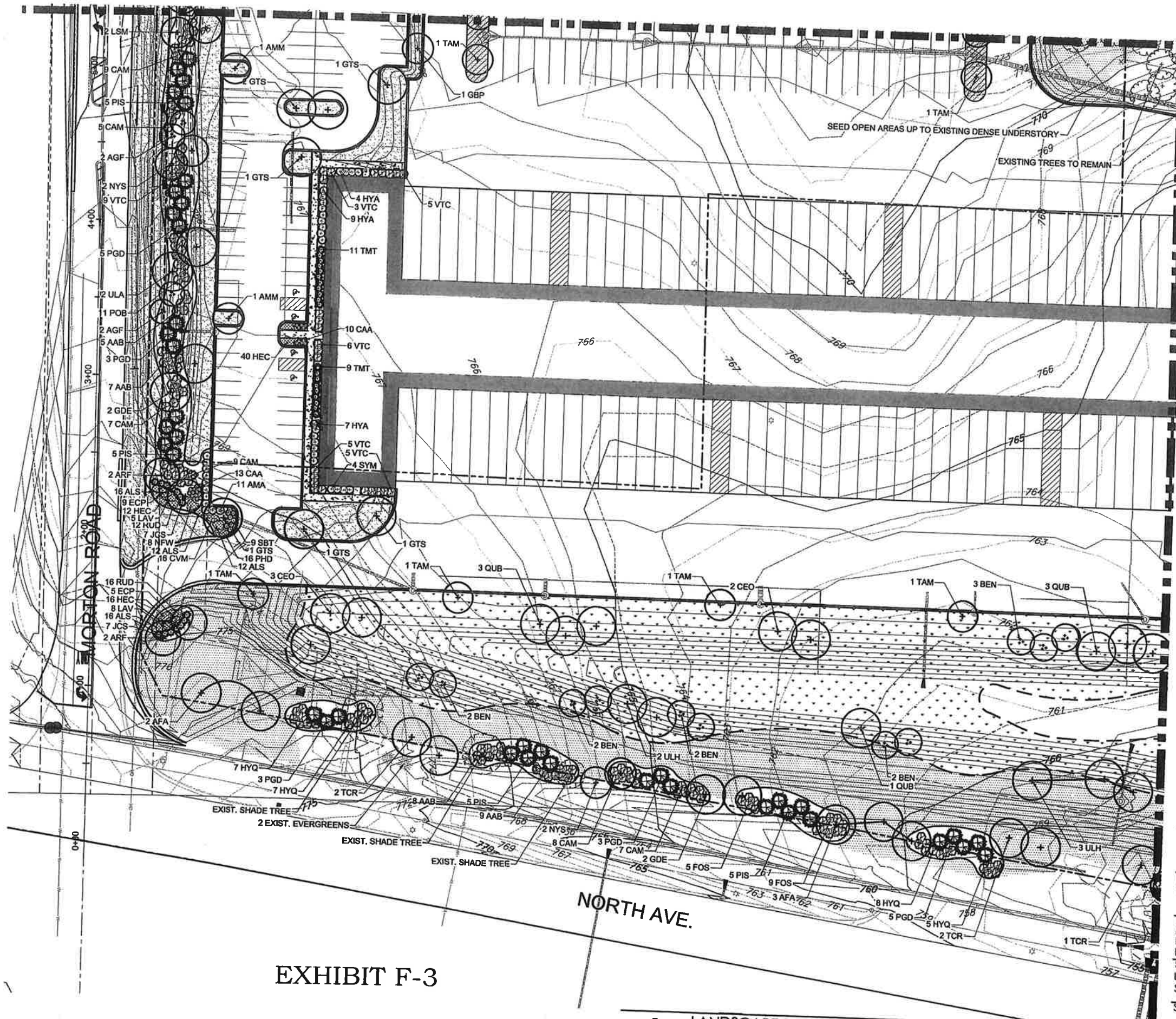
PROJECT NUMBER
 2301004

DATE
 01-06-2022
 DRAWN BY: LCG
 APPROVED BY: LCG

SHEET TITLE
 LANDSCAPE PLAN -
 SOUTHWEST

SHEET NUMBER

L2.1



LANDSCAPE LEGEND

- EXISTING TREE
- PROPOSED SHADE TREE
- PROPOSED EVERGREEN TREE
- PROPOSED ORNAMENTAL TREE
- PROPOSED LARGE SHRUB
- PROPOSED MEDIUM SHRUB
- PROPOSED EVERGREEN SHRUB
- PROPOSED LOW SHRUB
- PROPOSED ORNAMENTAL GRASS
- PROPOSED PERENNIAL PLANTING
- SODDED LAWN
- SEEDED LAWN
- SEEDED DETENTION

EXHIBIT F-3

1 LANDSCAPE PLAN
SOUTHWEST

N SCALE
 0' 15' 30' 60' 1"=30'
 90'



PROJECT TEAM

CIVIL ENGINEER:
CAGE
CIVIL

PROJECT NAME

Mainfreight
Trucking

North Ave & Morton Rd.
Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
1.	For Permit	08/04/2023
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SET TYPE
LANDSCAPE PLANS

PROJECT NUMBER
2301004

DATE
01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG

SHEET TITLE
LANDSCAPE PLAN -
SOUTHEAST

SHEET NUMBER

L2.2

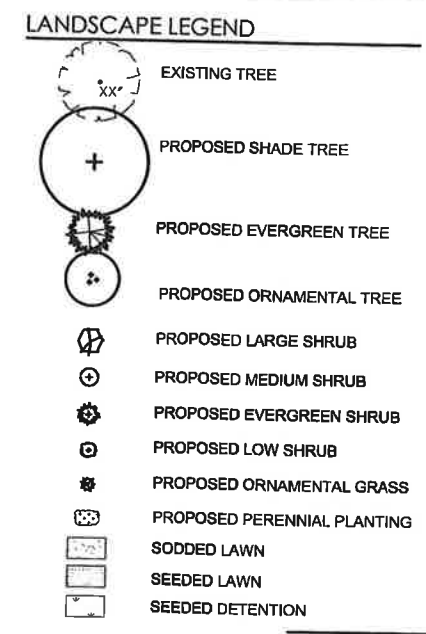
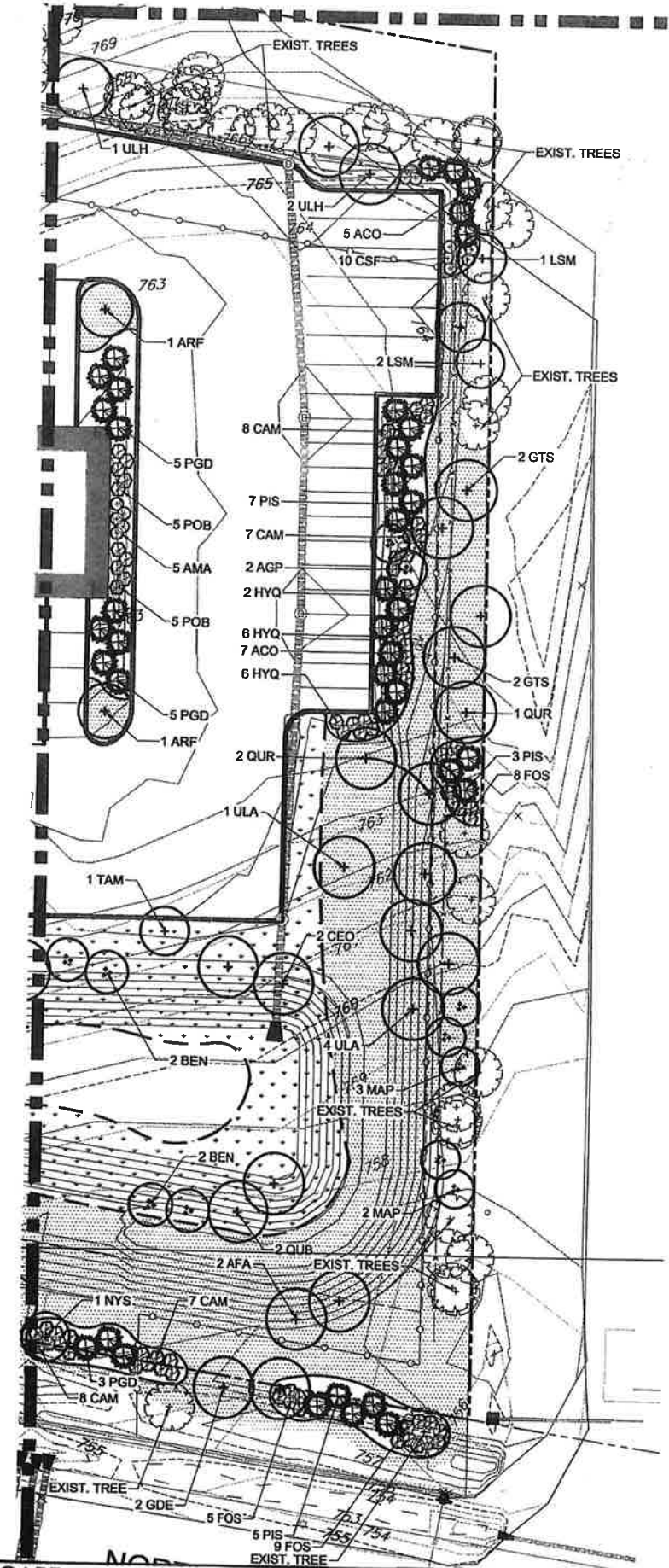
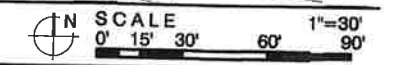


EXHIBIT F-4

1 LANDSCAPE PLAN
SOUTHEAST



SEAL



EXPIRES 08/2025

PROJECT TEAM

CIVIL ENGINEER:



EXHIBIT F-5

PROJECT NAME

Mainfreight
 Trucking

North Ave & Morton Rd.
 Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
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SET TYPE
 LANDSCAPE PLANS

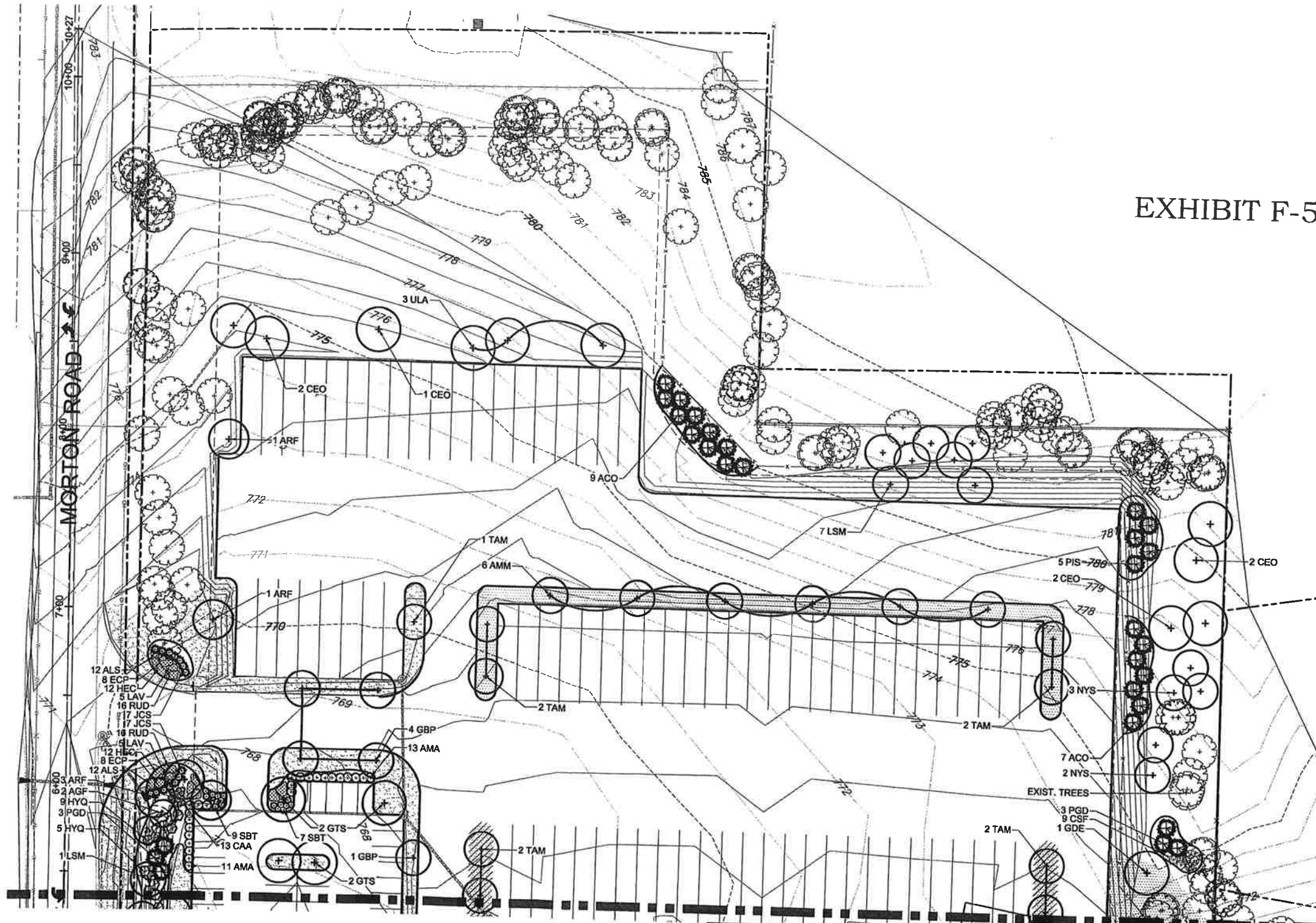
PROJECT NUMBER
 2301004

DATE
 01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG

SHEET TITLE
 LANDSCAPE PLAN -
 NORTHWEST

SHEET NUMBER

L2.3



**1 LANDSCAPE PLAN
 NORTHWEST**

SCALE
 0' 15' 30' 60' 1"=30'
 90'

LANDSCAPE LEGEND

- EXISTING TREE
- PROPOSED SHADE TREE
- PROPOSED EVERGREEN TREE
- PROPOSED ORNAMENTAL TREE
- PROPOSED LARGE SHRUB
- PROPOSED MEDIUM SHRUB
- PROPOSED EVERGREEN SHRUB
- PROPOSED LOW SHRUB
- PROPOSED ORNAMENTAL GRASS
- PROPOSED PERENNIAL PLANTING
- SODDED LAWN
- SEEDED LAWN
- SEEDED DETENTION



PROJECT TEAM

CIVIL ENGINEER:



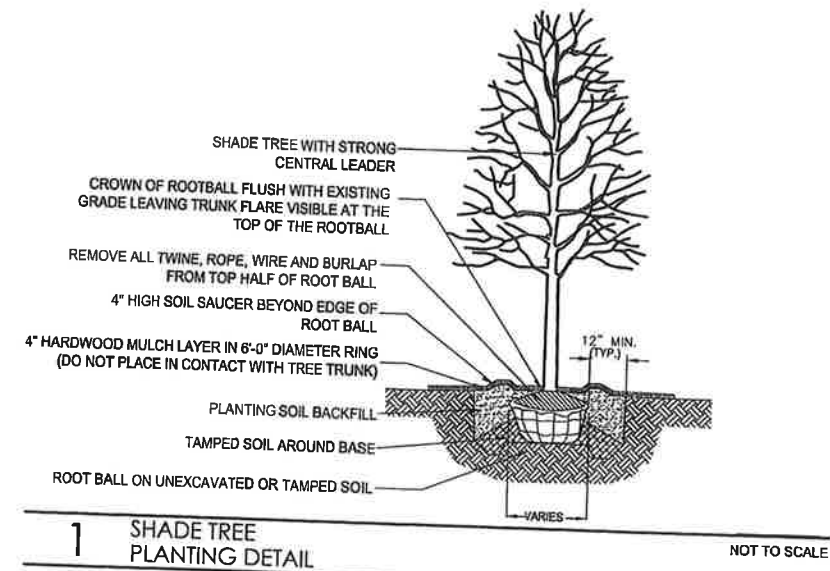
EXHIBIT F-6

LANDSCAPE NOTES

- CONTRACTOR SHALL OBTAIN ALL NECESSARY LOCAL PERMITS AND PERMISSIONS TO INSTALL THE PROPOSED IMPROVEMENTS
- ALL LANDSCAPE MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH CAROL STREAM LANDSCAPING CODES AND ZONING ORDINANCES.
- PRIOR TO COMMENCING ANY WORK, CONTRACTOR SHALL HAVE DIGGERS HOTLINE LOCATE AND MARK ALL UNDERGROUND UTILITY FACILITIES AND LINES.
- ALL PLANT MATERIALS (EXCEPT FOR GROUND COVER, ANNUALS, AND PERENNIALS) SHALL BE BALLED AND BURLAPPED STOCK AND MEET CURRENT STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN'S STANDARD FOR NURSERY STOCK (ANSI 260.1-1986) OR EQUAL. PLANT MATERIALS MUST BE SUPPLIED WITHIN A 150 MILE RADIUS OF PROJECT SITE WITHIN NORTHEAST ILLINOIS. CONTRACTOR MAY SUBSTITUTE CONTAINER STOCK FOR SHRUBS IF SIZES ARE EQUAL TO SPECIFIED B&B STOCK, WITH THE APPROVAL OF THE LANDSCAPE ARCHITECT.
- IF SPECIFIED PLANTS ARE NOT AVAILABLE AT THE TIME OF ORDERING, PLANTS WITH SIMILAR WHOLESALE VALUE AND LANDSCAPE CHARACTERISTICS MAY BE SUBSTITUTED UPON THE APPROVAL OF THE LANDSCAPE ARCHITECT AND VILLAGE STAFF.
- SOIL IN GROUND COVER BEDS SHALL BE AMENDED USING 2 INCHES OF MUSHROOM COMPOST INCORPORATED INTO THE TOP 4 INCHES OF SOIL.
- DISTURBED AREAS TO RECEIVE SOD SHALL BE TILLED TO 6" DEPTH AND FINE GRADED TO PROVIDE SMOOTH BASE SURFACE. IF EXISTING SOIL IS A MAJORITY OF CLAY OR UNSUITABLE, 2" OF FINE GRADED TOPSOIL SHALL BE ADDED PRIOR TO TILLING. EXISTING SOD AREAS SHALL HAVE TURF REMOVED WITH AUTOMATED SODCUTTER OR HAND SPACE TO REMOVE ALL BLADES AND ROOTS. 1" OF FINE GRADED TOPSOIL SHALL BE TILLED AND GRADED.
- TREE AND SHRUB BACKFILL MIXTURE SHALL BE 2 PARTS EXIST. NATIVE TOPSOIL AND 1 PART SPHAGNUM PEAT MOSS W/ DECOMPOSED MANURE.
- ALL SHRUB BEDS AND INDIVIDUAL TREE PLANTINGS, UNLESS OTHERWISE NOTED, SHALL RECEIVE A 4 INCH LAYER OF SHREDDED HARDWOOD MULCH. ALL GROUND COVER, ANNUAL AND PERENNIAL BEDS SHALL RECEIVE A 2 INCH LAYER OF THE SAME MULCH MATERIAL. COSTS FOR MULCH SHALL BE CONSIDERED INCIDENTAL AND SHALL BE INCLUDED IN THE COST OF PLANTINGS.
- NURSERY TAGS (SPECIES, SIZE) FOR ALL SHADE TREES SHALL REMAIN ATTACHED TO TREES UNTIL FINAL APPROVAL FROM MUNICIPALITY.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER A BONDED WRITTEN ONE-YEAR WARRANTY AGREEMENT (BEGINNING ON THE OWNER'S POSSESSION DATE). THIS AGREEMENT SHALL COVER MAINTENANCE, REPAIR, AND REPLACEMENT OF ALL DEAD OR DAMAGED LANDSCAPING TO PRESERVE THE SAME QUANTITY AND QUALITY AS INITIALLY APPROVED
- AN IRRIGATION SYSTEM SHALL BE DESIGNED AND BID TO PROVIDE ADEQUATE WATER TO ALL NEWLY PLANTED TREES, SHRUBS, PERENNIALS AND GROUND COVER BEDS, AND TURF AREAS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL AND APPLY FOR ALL NECESSARY PERMITS PRIOR TO COMMENCING WORK. IRRIGATION PLANS SHALL INCLUDE HUNTER OR SIMILAR PRO-C CONTROLLER W/WIRELESS SOLAR SYNC STATION AND HUNTER OR SIMILAR SPRAYHEADS AND NOZZLES. IRRIGATION WORK SHALL WARRANT ALL LABOR AND MATERIALS FOR 1 FULL YEAR AFTER INSTALLATION AND TESTING.
- SEEDED LAWN AREAS SHALL BE BID WITH A BID ALTERNATE FOR HYDROSEEDING LAWN. PRIOR TO SEEDING, 2" OF FINE TOPSOIL SHALL BE TILLED INTO EXIST SOIL MIXTURE. A MIX CONSISTING OF ROUGHLY 30% BLUEGRASS / 30% FINE FESCUES / 40% RYE GRASSES (AND TACKIFIER FOR HYDROSEEDING) SHALL BE APPLIED AT MANUFACTURERS SPECIFIED RATES FOR NEW LAWNS BETWEEN 5 AND 10 LBS PER 1,000 SF.
- TREES AND SHRUBS SHALL NOT BE LOCATED CLOSER THAN TEN (10) FEET TO FIRE HYDRANTS, TRANSFORMERS OR OTHER ABOVE GROUND UTILITIES. ANY DISCREPANCY ON THE PLAN RELATED TO THESE PROXIMATE UTILITIES SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR RESOLUTION.

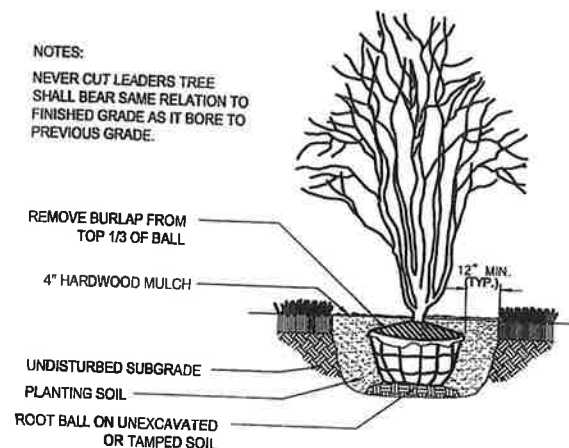
VILLAGE LANDSCAPE REQUIREMENT CALCULATIONS

DESCRIPTION	REQUIREMENT	AREA / REQ.	PROPOSED
TOTAL SITE		686,581 SQ. FT. 15.76 Acres	
TOTAL BUILDING		45,818 SQ. FT.	
TOTAL PARKING	Square footage of parking area	29,296 SQ. FT.	
PARKING LOT PERIMETER AREAS	One shrub or native grass the height of which shall not be less than three feet nor greater than four feet, shall be planted for every three feet of landscape area length, spaced linearly to adequately screen vehicle bumpers.	380 L.F. / 3 = 126 shrubs	138 shrubs
PARKING LOT INTERIOR AREAS	One parking lot island shall be provided between every 15 parking spaces. Parking lot island locations may be varied based on specific site requirements or design scheme	89 Spaces / 15 = 6 Islands	7 Islands Provided
BUILDING FOUNDATION AREAS	A minimum four foot-wide hedge row shall be planted with one shrub or native grass every three feet on center, spaced linearly 80% of the length of the facade	1556 LF X .80 = 1245 / 3' = 415	10 Evergreen Trees 83 Shrubs *Limited area because of overhead doors
TRANSITION AREAS	Type D transition yard per 100' Min. yard width = 15' 6' ht. Fence 5 Canopy / Evergreen Trees 5 Ornamental Trees 30 Shrubs / Ornamental Grasses	2231 LF/100=223 Min. Yard = 15' 6' fence 22.3 x 5 = 112 22.3 x 5 = 112 22.3 x 30 = 669	Min. Yard = 20' 6' Opaque fence 234 Exist. Trees - 72 Proposed Trees Exist. Dense Understory - 7 Om. Trees 46 Shrubs



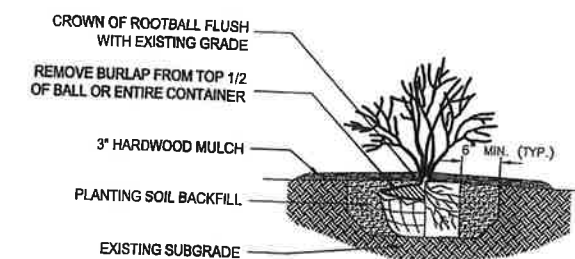
1 SHADE TREE PLANTING DETAIL

NOT TO SCALE



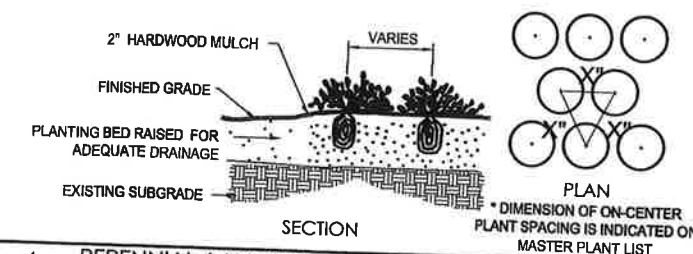
2 ORNAMENTAL TREE PLANTING DETAIL

NOT TO SCALE



3 SHRUB PLANTING DETAIL

NOT TO SCALE



4 PERENNIAL / ANNUAL PLANTING DETAIL

NOT TO SCALE

PROJECT NAME

Mainfreight
Trucking

North Ave & Morton Rd.
Carol Stream, IL

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1. For Permit 08/04/2023
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LANDSCAPE PLANS

PROJECT NUMBER
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DATE
01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG

SHEET TITLE
PRELIMINARY LANDSCAPE
PLAN / DETAILS & NOTES

SHEET NUMBER

L3.0

RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT

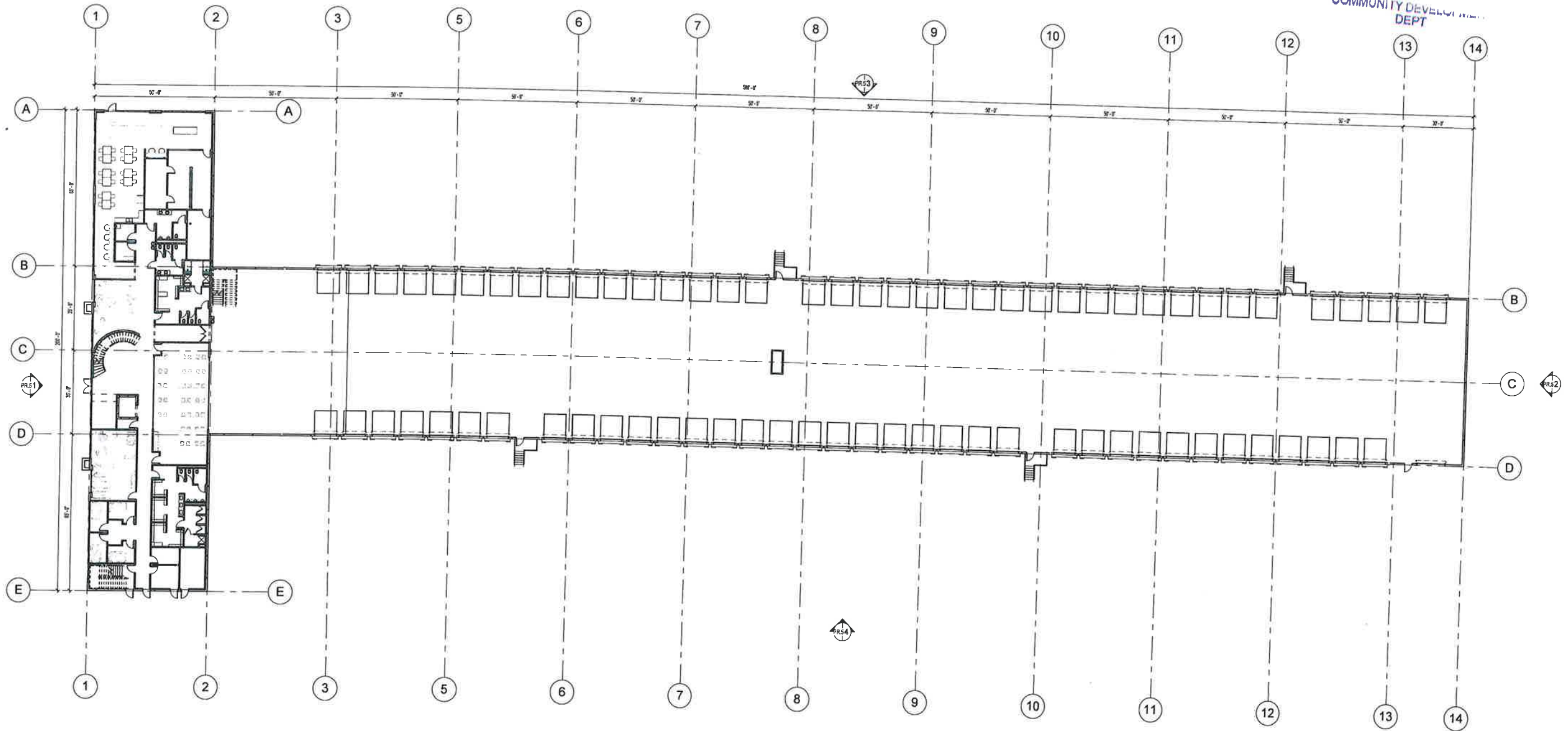


EXHIBIT G-1



MAINFREIGHT TERMINAL
NORTH AVE & MORTON RD.
Carol Stream, IL 60188

SCALE - 1" = 20'-0"

OVERALL FLOOR PLAN - LEVEL 1



[24x36] GA1271 08-21-2023

PR.1



RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT

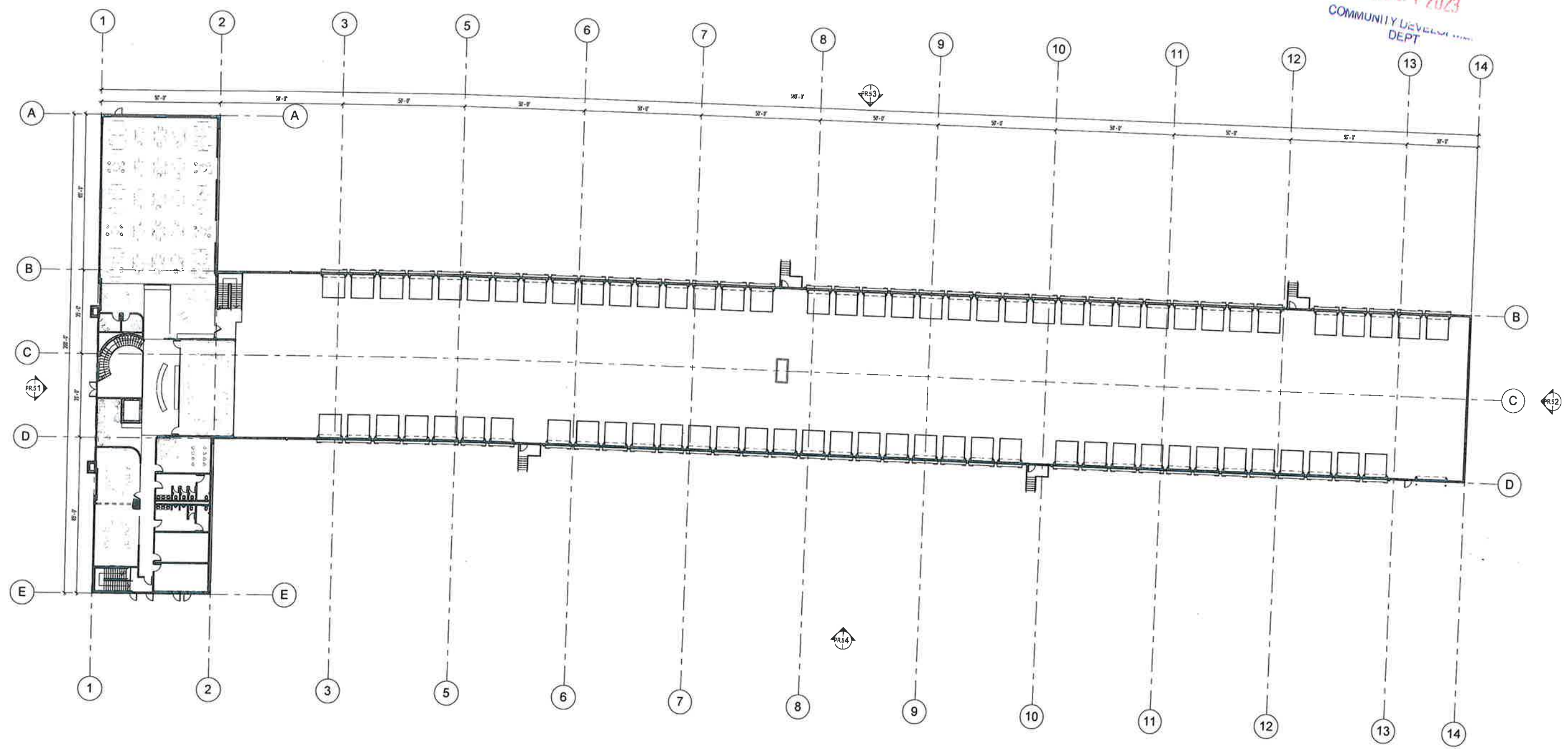


EXHIBIT G-2



MAINFREIGHT TERMINAL
NORTH AVE & MORTON RD.
Carol Stream, IL 60185

SCALE = 1" = 20'-0"

OVERALL FLOOR PLAN - LEVEL 2



(24x36) GA1271 06-21-2023

PR.2

ARCO
MURRAY

GMA
ARCHITECTS

RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT

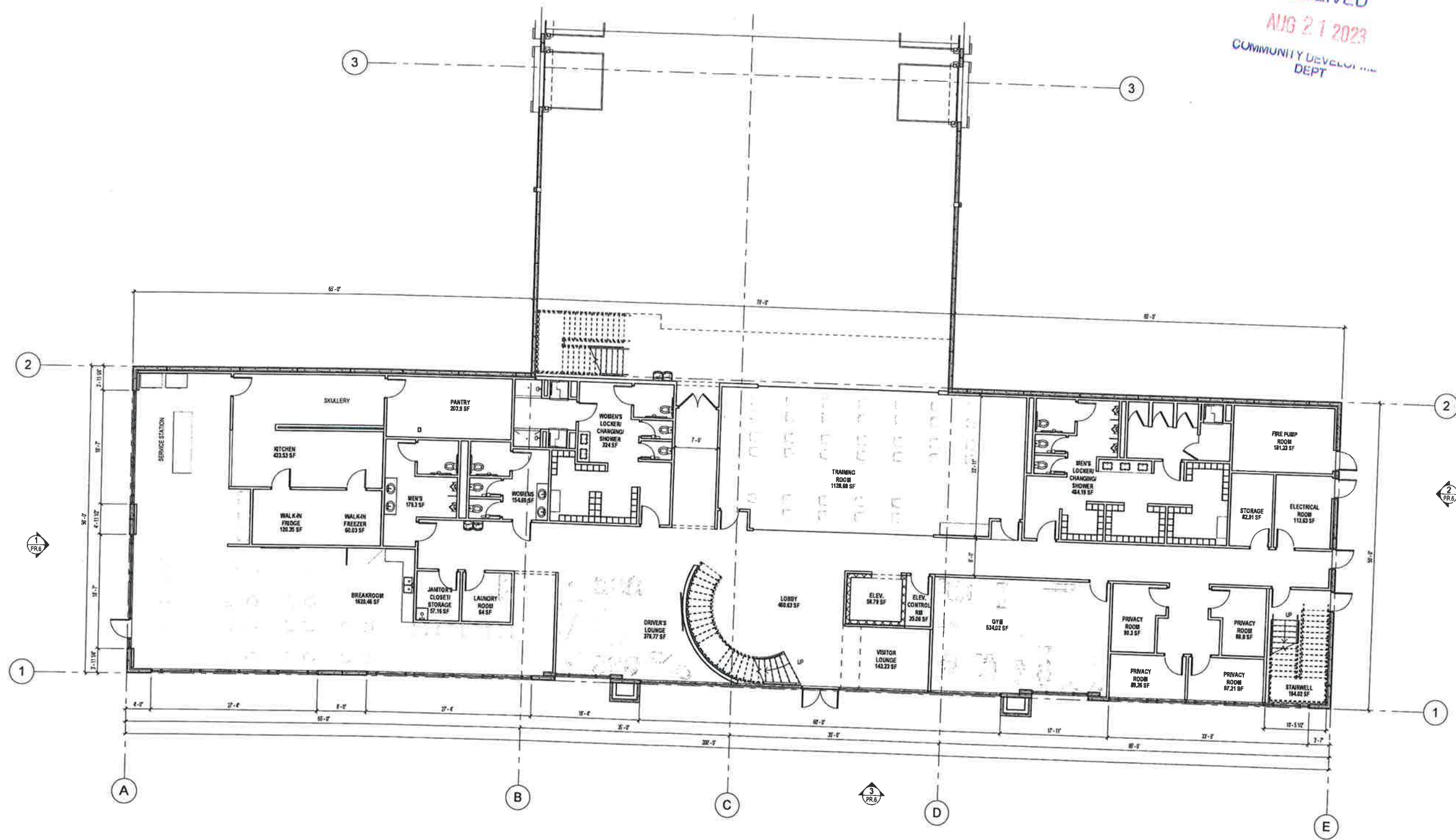


EXHIBIT G-3



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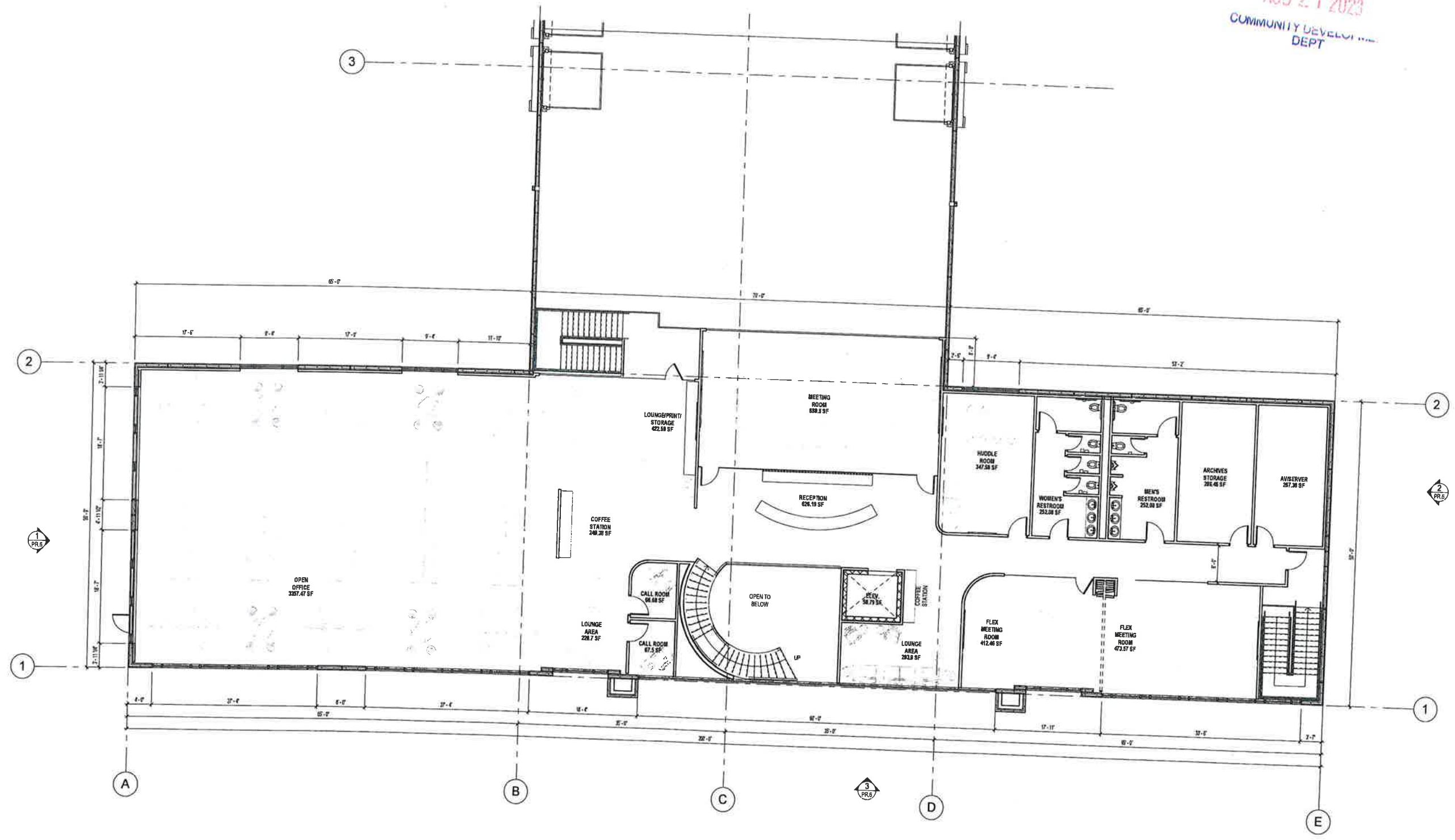


EXHIBIT G-4



MAINFREIGHT TERMINAL
 NORTH AVE. & MORTON RD.
 Carol Stream, IL 60185

SCALE = 1/8" = 1'-0"

ENLARGED OFFICE FLOOR PLAN - LEVEL 2



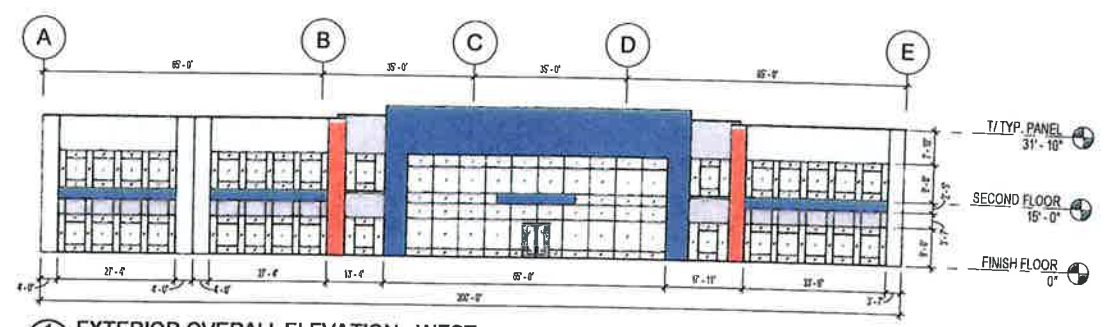
(21436) GA1271 08-21-2023

PR.4

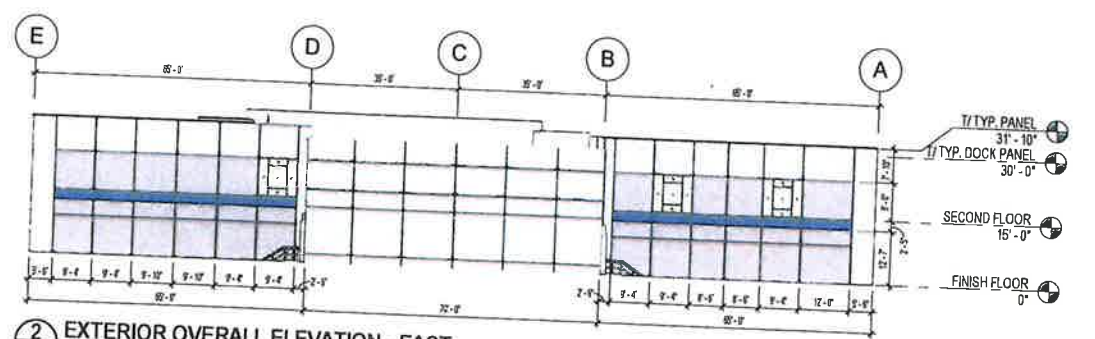


RECEIVED
 AUG 27 2023
 COMMUNITY DEVELOPMENT
 DEPT

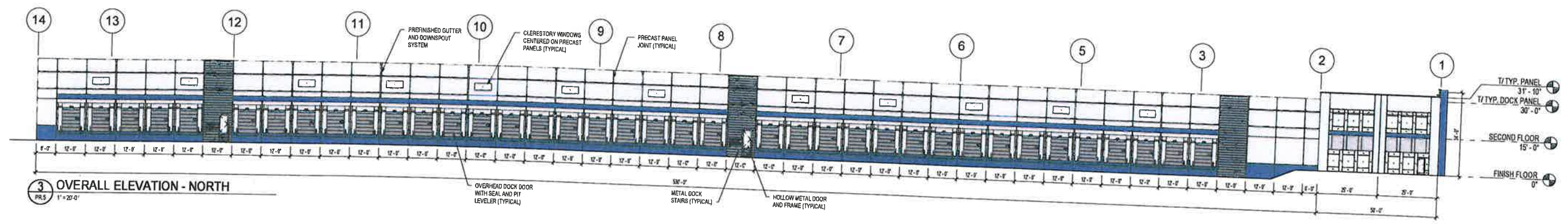
EXHIBIT H-1



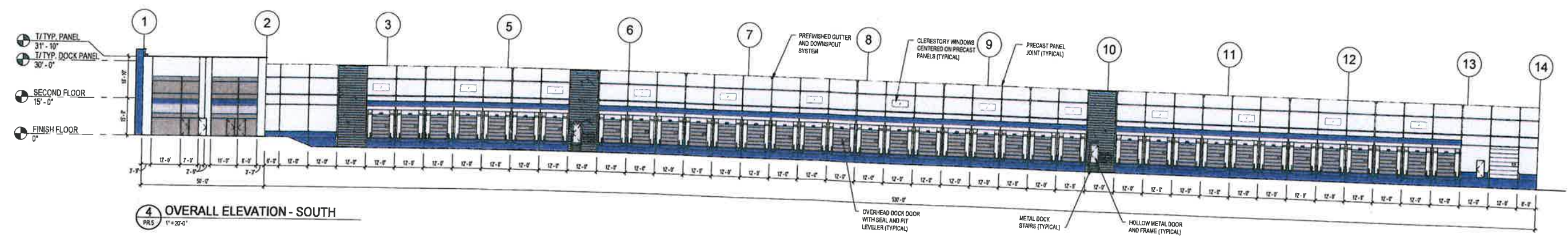
1 EXTERIOR OVERALL ELEVATION - WEST
 PR.5 1" = 20'-0"



2 EXTERIOR OVERALL ELEVATION - EAST
 PR.5 1" = 20'-0"



3 OVERALL ELEVATION - NORTH
 PR.5 1" = 20'-0"



4 OVERALL ELEVATION - SOUTH
 PR.5 1" = 20'-0"

MAINFREIGHT TERMINAL
 NORTH AVE & MORTON RD.
 Carol Stream, IL 60185

SCALE - 1" = 20'-0"

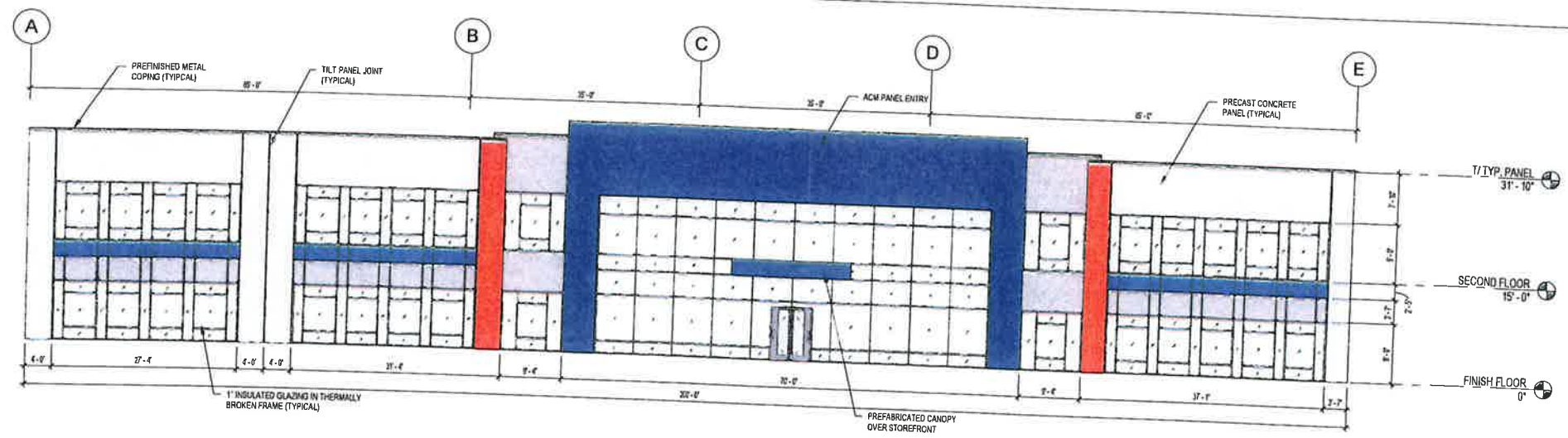
OVERALL EXTERIOR ELEVATIONS

(24x36) GA 1271 08-21-2023

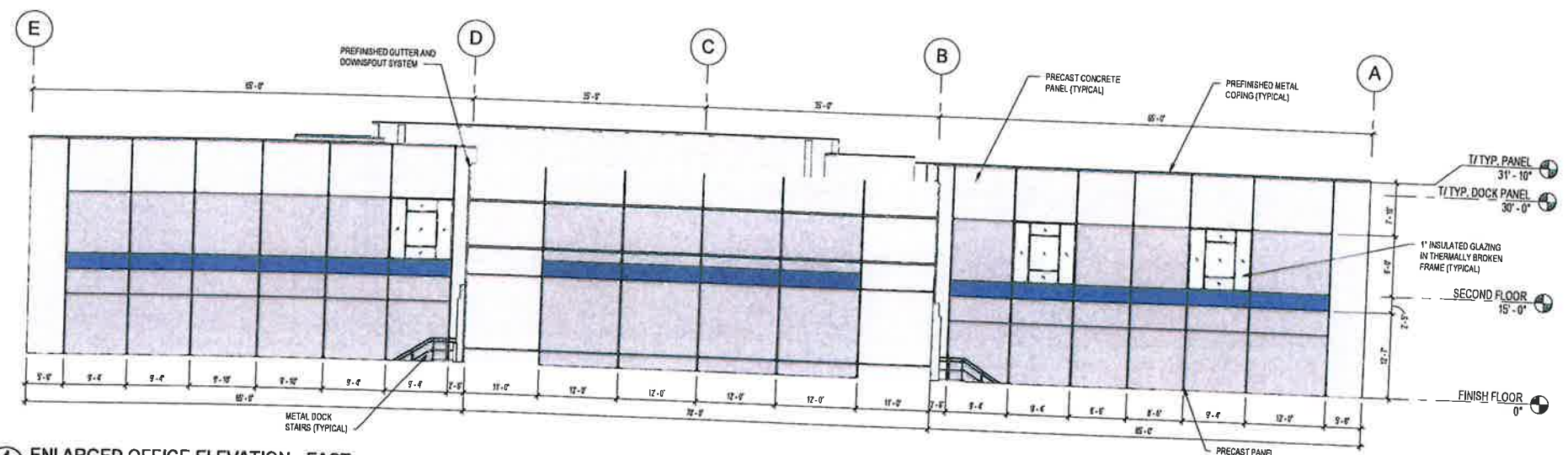
PR.5



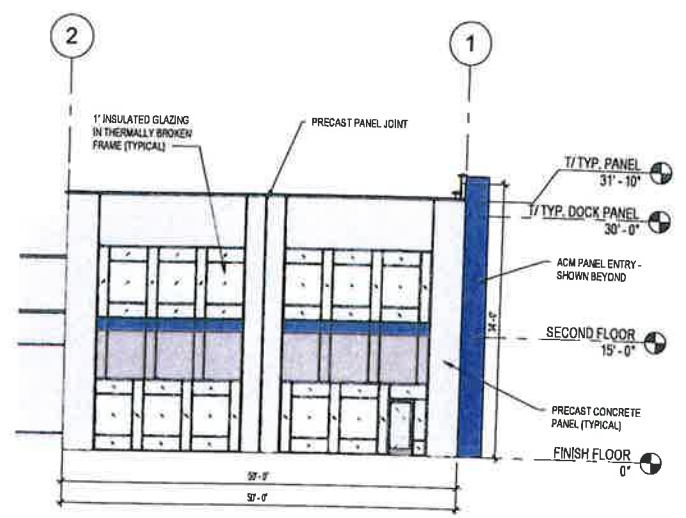
RECEIVED
 AUG 27 2023
 COMMUNITY DEVELOPMENT
 DEPT



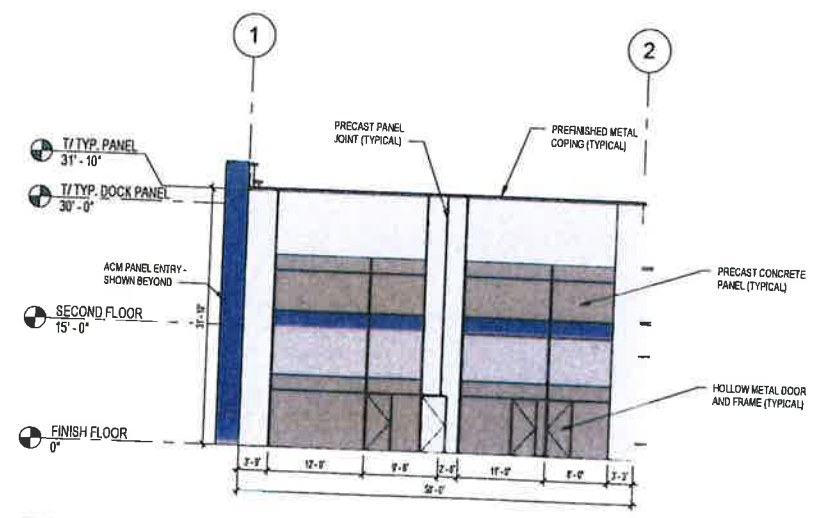
3 ENLARGED OFFICE ELEVATION - WEST
 PR.6 3/32" = 1'-0"



4 ENLARGED OFFICE ELEVATION - EAST
 PR.6 3/32" = 1'-0"



1 OFFICE ELEVATION - NORTH
 PR.6 3/32" = 1'-0"



2 OFFICE ELEVATION - SOUTH
 PR.6 3/32" = 1'-0"

EXHIBIT H-2



NORTH AVE TH SUBDIVISION

OF
PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 09, EAST OF
THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS.

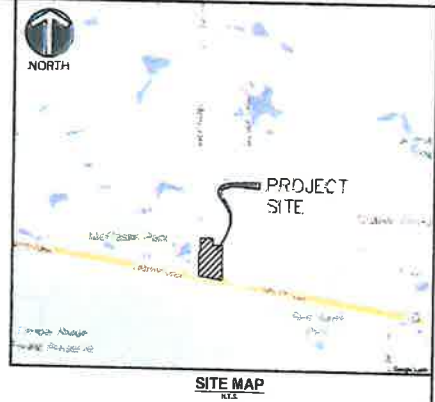
27W364 NORTH AVENUE
WEST CHICAGO, IL 60185
PIN 01-36-200-005
01-36-200-006
01-36-200-017
01-36-200-040

REVISION RECORD		
NO	DATE	DESCRIPTION

BASE OF BEARINGS:
ILLINOIS STATE PLANE - EAST ZONE

MORTON (FORMERLY EAST GRAND DETOUR ROAD) ROAD

AREA SUMMARY	
GROSS BOUNDARY AREA:	
LOT 1:	686,578 S.F. (15.762 ACRES)
ROW DEDICATION:	0 S.F. (0 ACRES)
TOTAL AREA:	686,578 S.F. (15.762 ACRES)



LINE TYPE LEGEND:

	EXISTING PROPERTY LINE
	EXISTING ADJACENT PROPERTY LINE
	EXISTING ACCESS EASEMENT
	UNDERLYING LOT PROPERTY LINE
	EXISTING CENTERLINE
	SET CONCRETE MONUMENT
	FOUND IRON PIPE

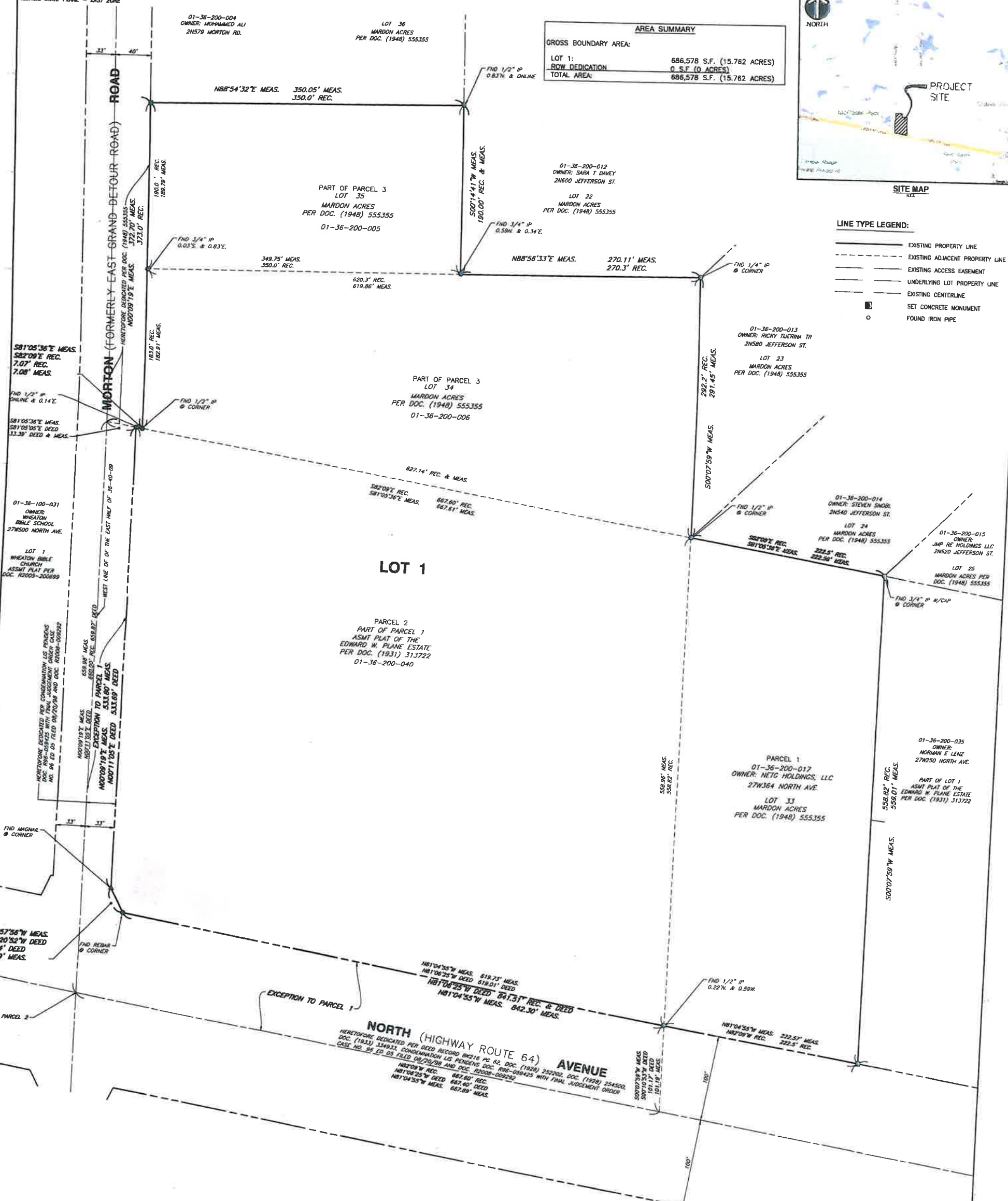
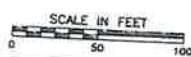


EXHIBIT I-1

CEC
Civil & Environmental Consultants, Inc.
1230 East Diehl Road, Suite 208 - Naperville, IL 60563
Ph: 630.963.6026 - 877.963.6026 - Fax: 630.963.6027
www.cecinc.com

PLAT OF SUBDIVISION
Situate in
27W364 NORTH AVENUE
WEST CHICAGO, ILLINOIS 60185
Made For
TIMBER HILL
8770 W BRYN MAWR
CHICAGO, IL 60631



DATE:	08/10/2023	SCALE:	1" = 50'	DRAWING NO.:	SV01
DRAWN BY:	BTR	CHECKED BY:	DRAFT	SHEET	1 OF 2
PROJECT NO.:	334-443	APPROVED BY:	DRAFT		

OWNER'S CERTIFICATE

(Corporate owner's certificate should be accompanied by a corporate form of acknowledgment)
STATE OF ILLINOIS)
COUNTY OF DUPAGE) ss.

THIS IS TO CERTIFY THAT THE UNDERSIGNED IS/ARE THE OWNER/OWNERS OF THE LAND DESCRIBED IN THE ANNEXED PLAT, AND HAS/HAVE CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED, AS INDICATED THEREON, FOR THE USES AND PURPOSES THEREIN SET FORTH, AND DOES/DO HEREBY ACKNOWLEDGE AND ADOPT THE SAME UNDER THE STYLE AND TITLE THEREON INDICATED.

NAME/TITLE
ADDRESS
DATED THIS ___DAY OF ___, 20__

NOTARY CERTIFICATE

STATE OF ILLINOIS } SS
COUNTY OF DUPAGE }
I, _____ A NOTARY PUBLIC IN AND FOR THE SAID COUNTY IN THE STATE AFORESAID, DO HEREBY CERTIFY THAT _____ AND _____ OF SAID OWNER, WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS, WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT, AS SUCH _____ AND _____

RESPECTFULLY APPEARED BEFORE ME THIS DAY IN PERSON AND JOINTLY AND SEVERALLY ACKNOWLEDGE THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT AND AS THE FREE AND VOLUNTARY ACT OF SAID OWNER FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL
THIS ___DATE___ DAY OF ___MONTH___, 20__

NOTARY PUBLIC SIGNATURE
PRINT NAME

MY COMMISSION EXPIRES ON ___MONTH___ DATE, 20__

SCHOOL DISTRICT BOUNDARY STATEMENT

STATE OF ILLINOIS } SS
COUNTY OF DUPAGE }
THE UNDERSIGNED, BEING DULY SWORN, UPON HIS/HER OATH DEPOSES AND STATES AS FOLLOWS:

- 1. THAT LEGALLY DESCRIBED ON THIS PLAT OF SUBDIVISION, WHICH HAS BEEN SUBMITTED TO THE VILLAGE OF CAROL STREAM FOR APPROVAL, WHICH LEGAL DESCRIPTION IS INCORPORATED HEREIN BY REFERENCE;
2. TO THE BEST OF THE OWNERS' KNOWLEDGE, THE SCHOOL DISTRICT IN WHICH TRACT, PARCEL, LOT OR BLOCK OF THE PROPOSED SUBDIVISION LIES IS: COMMUNITY CONSOLIDATED SCHOOL DISTRICT 93, 230 COVINGTON DR, BLOOMINGDALE, IL 60108

OWNER NAME:
BY: ATTEST:
ITS: ITS:

SUBSCRIBED AND SWORN BEFORE ME THIS ___DAY OF ___, A.D. 20__

NOTARY PUBLIC

VILLAGE CLERK CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) ss.
I, _____ VILLAGE CLERK OF THE VILLAGE OF CAROL STREAM, ILLINOIS,

CERTIFY THAT THIS PLAT WAS PRESENTED TO AND BY RESOLUTION _____ DULY APPROVED BY THE BOARD OF TRUSTEES OF THE VILLAGE AT ITS MEETING HELD ON ___20___ AND THAT THE REQUIRED BOND OR OTHER GUARANTEE HAS EITHER BEEN POSTED OR ADEQUATE PROVISION HAS BEEN MADE FOR SUCH BOND TO BE POSTED, FOR THE COMPLETION OF THE IMPROVEMENTS REQUIRED BY THE REGULATIONS OF THE VILLAGE.

IN WITNESS WHEREOF, I HAVE HEREUNTO SET MY HAND AND SEAL OF THE VILLAGE OF CAROL STREAM, ILLINOIS THIS ___DAY OF ___, 20__

MAYOR VILLAGE CLERK

VILLAGE ENGINEER CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) ss.
I, _____ VILLAGE ENGINEER OF THE VILLAGE OF CAROL STREAM, ILLINOIS, HEREBY CERTIFY THAT THE LAND IMPROVEMENTS DESCRIBED IN THE ANNEXED PLAT AND PLANS AND SPECIFICATIONS THEREOF MEET THE MINIMUM REQUIREMENTS OF THE VILLAGE THEREOF.

I DO FURTHER CERTIFY THAT THERE HAS BEEN FILED WITH ME AND I HAVE REVIEWED TOPOGRAPHICAL AND PROFILE STUDIES FILED WITH THIS PLAT.
DATED AT CAROL STREAM, DUPAGE COUNTY, ILLINOIS THE ___DATE OF ___ 20__

VILLAGE ENGINEER

FINANCE DIRECTOR CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) ss.
I, _____ FINANCE DIRECTOR OF CAROL STREAM, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE BEEN APPORTIONED AGAINST THE TRACT OF LAND INCLUDED IN THE PLAT.

DATED AT CAROL STREAM, DUPAGE COUNTY, ILLINOIS THIS ___DAY OF ___, 20__

FINANCE DIRECTOR

PLAT OF SUBDIVISION

A Plat of Subdivision shall contain all of the certificates listed above for a Plat of Easement along with the following additional signature certificates:

PLAN COMMISSION CERTIFICATE
STATE OF ILLINOIS)
COUNTY OF DUPAGE) ss.
APPROVED THIS ___DAY OF ___, 20__

VILLAGE OF CAROL STREAM PLAN COMMISSION
CHAIRPERSON

CERTIFICATE CONCERNING DRAINAGE

STATE OF ILLINOIS)
COUNTY OF DU PAGE) SS.
THE UNDERSIGNED HEREBY CERTIFY THAT, TO THE BEST OF OUR KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATERS WILL NOT BE CHANGED BY THE CONSTRUCTION OF SUCH SUBDIVISION OR ANY PART THEREOF, OR THAT SUCH SURFACE WATER DRAINAGE WILL NOT BE CHANGED WITHOUT ADEQUATE PROVISION BEING MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS INTO PUBLIC AREAS OR DRAINS WHICH THE SUBDIVIDER HAS A RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL NOT BE DEPOSITED ON THE PROPERTY OF ADJOINING ADJOINING PROPERTY BECAUSE OF THE CONSTRUCTION OF THE SUBDIVISION.

DATED THIS ___DAY OF ___, A.D. 20__
BY: _____ REGISTERED PROFESSIONAL ENGINEER

MORTGAGEE CERTIFICATE

STATE OF ILLINOIS } SS
COUNTY OF DUPAGE }
I, _____ AS MORTGAGEE UNDER THE PROVISIONS OF A CERTAIN MORTGAGE DATED ___A.D. 20___ AND RECORDED IN THE RECORDER OF DEEDS OFFICE OF ___COUNTY, ILLINOIS ON THE ___DATE___ OF ___MONTH___, A.D. 20___ AS DOCUMENT NO. _____ HEREBY CONSENTS TO AND APPROVES THE SUBDIVISION OF THE LAND AND THE GRANTING OF THE EASEMENT(S) DEPICTED HEREON.

DATED THIS ___DATE___ DAY OF ___MONTH___, A.D. 20___
PRINT MORTGAGEE NAME:
BY: ATTEST:
ITS: ITS:

NOTARY'S CERTIFICATE - MORTGAGEE

STATE OF ILLINOIS } SS
COUNTY OF ___ }
THE UNDERSIGNED, A NOTARY PUBLIC IN THE COUNTY AND STATE AFORESAID, DO HEREBY CERTIFY THAT (NAME) _____ (TITLE) _____ OF _____ AND (NAME) _____ (TITLE) _____ OF _____ WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT AS SUCH (TITLE) _____ AND (TITLE) _____ RESPECTIVELY, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT OF SAID _____ AS MORTGAGEE, FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND SEAL
THIS ___DATE___ DAY OF ___MONTH___, 20__

NOTARY PUBLIC

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS } SS
COUNTY OF DUPAGE }
I, DOUGLAS R. MCCLINTIC, AN ILLINOIS LICENSED PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT I HAVE SURVEYED AND SUBDIVIDED THE FOLLOWING DESCRIBED PROPERTY AND THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION THEREOF. ALL DIMENSIONS ARE GIVEN IN FEET AND DECIMALS THEREOF.

LEGAL DESCRIPTION

- PARCEL 1: LOT 33 IN MARDON ACRES, BEING A SUBDIVISION OF PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 31, TOWNSHIP 40 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 4, 1948, AS DOCUMENT 555355, IN DUPAGE COUNTY, ILLINOIS.
PARCEL 2: THAT PART OF PARCEL 1 IN THE ASSESSMENT PLAT OF THE EDWARD W. PLANE ESTATE, BEING A SUBDIVISION OF PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 31, TOWNSHIP 40 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHWEST CORNER OF SAID PARCEL 1, BEING A CROSS IN THE CENTER LINE OF STATE BOND ISSUE HIGHWAY ROUTE 64 ALSO KNOWN AS NORTH AVENUE, FOR A POINT OF BEGINNING, AND RUNNING THENCE NORTH ALONG THE CENTER LINE OF THE PUBLIC HIGHWAY KNOWN AS MORTON ROAD, FOR A DISTANCE OF 660.0 FEET; THENCE SOUTH 82 DEGREES 09 MINUTES EAST PARALLEL WITH THE CENTER LINE OF SAID NORTH AVENUE FOR A DISTANCE OF 667.60 FEET TO THE NORTHWEST CORNER OF LOT 33 IN MARDON ACRES RECORDED AS DOCUMENT 555355; THENCE SOUTH PARALLEL WITH THE WEST LINE OF THE EAST 1/2 OF SAID SECTION 36 AND BEING ALONG THE WEST LINE OF SAID LOT 33 AND SAID WEST LINE EXTENDED SOUTH, 660 FEET TO THE CENTER LINE OF NORTH AVENUE; THENCE NORTH 82 DEGREES 09 MINUTES WEST ALONG SAID CENTER LINE 667.60 FEET TO THE POINT OF BEGINNING, ACCORDING TO THE ASSESSMENT PLAT OF EDWARD W. PLANE ESTATE RECORDED JUNE 15, 1931 AS DOCUMENT 313722, (EXCEPTING THEREFROM THAT PART TAKEN BY CONDEMNATION IN CASE 96ED-5), IN DU PAGE COUNTY, ILLINOIS.
PARCEL 3: LOTS 34 AND 35 IN MARDON ACRES, BEING A SUBDIVISION OF PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 31, TOWNSHIP 40 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 4, 1948, AS DOCUMENT 555355, IN DU PAGE COUNTY, ILLINOIS. SAID DESCRIBED PROPERTY IS LOCATED WITHIN AN AREA HAVING A ZONE DESIGNATION X, AREA OF MINIMAL FLOOD HAZARD, BY THE SECRETARY OF HOUSING AND URBAN DEVELOPMENT, ON FLOOD INSURANCE RATE MAP NUMBER 17043C0042J, WITH AN EFFECTIVE DATE OF AUGUST 1, 2019, FOR FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PROPERTY IS SITUATED.

THIS PLAT HAS BEEN PREPARED BY CIVIL & ENVIRONMENTAL CONSULTANTS, INC., ILLINOIS LICENSED PROFESSIONAL DESIGN FIRM NO. 184.004002, LICENSE EXPIRES APRIL 30, 2025, UNDER MY PERSONAL DIRECTION FOR THE EXCLUSIVE USE OF THE CLIENT NOTED HEREON.

GIVEN UNDER MY HAND AND SEAL THIS ___DAY OF ___, 20__

ILLINOIS LICENSED PROFESSIONAL LAND SURVEYOR NO. 035-002992 LICENSED VALID THROUGH NOVEMBER 30, 2024

REVISION RECORD table with columns NO, DATE, DESCRIPTION

DUPAGE COUNTY RECORDER'S CERTIFICATE

STATE OF ILLINOIS } SS
COUNTY OF DUPAGE }
THIS INSTRUMENT _____ WAS FILED FOR RECORD IN THE RECORDER'S OFFICE OF DUPAGE COUNTY, ILLINOIS ON THE ___DAY OF ___, A.D. 20__ AT ___O'CLOCK ___M.

RECORDERS OF DEEDS

DUPAGE COUNTY CLERK'S CERTIFICATE

STATE OF ILLINOIS } SS
COUNTY OF DUPAGE }
I, _____ COUNTY CLERK OF DUPAGE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT GENERAL TAXES, NO UNPAID CURRENT TAXES, NO UNPAID FORFEITED TAXES, AND NO REDEEMABLE TAX SALES AGAINST ANY OF THE LAND INCLUDED IN THE ANNEXED PLAT. I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE ANNEXED PLAT. GIVEN UNDER MY HAND AND SEAL OF THE COUNTY CLERK AT WHEATON, ILLINOIS, THIS ___DAY OF ___, 20__

COUNTY CLERK

ILLINOIS DEPARTMENT OF TRANSPORTATION CERTIFICATE

THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT OF § 2 OF "AN ACT TO REVISE THE LAW IN RELATION TO PLATS," AS AMENDED. A PLAN THAT MEETS THE REQUIREMENTS CONTAINED IN THE DEPARTMENT'S "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS" WILL BE REQUIRED BY THE DEPARTMENT.

JOSE RIOS, P.E. REGION ONE ENGINEER DATE ___A.D., 20__

EXHIBIT I-2

Civil & Environmental Consultants, Inc. logo and address: 1230 East Ditch Road, Suite 200 - Naperville, IL 60563. PLAT OF SUBDIVISION Situate In 27W364 NORTH AVENUE WEST CHICAGO, ILLINOIS 60185 Made For TIMBER HILL 8770 BRYN MAWR SUITE 1350 CHICAGO, IL 6066

Table with columns DATE, DRAWN BY, PROJECT NO., SCALE, CHECKED BY, APPROVED BY, SHEET 2 OF 2

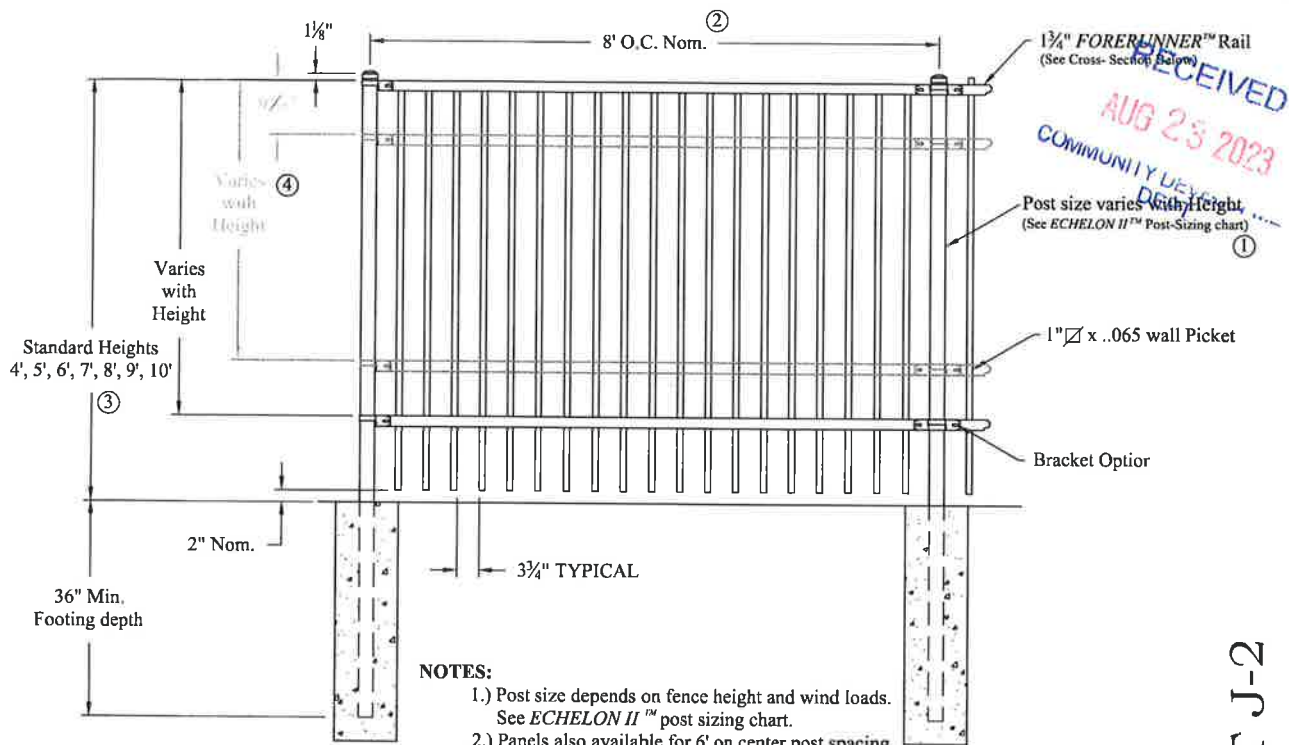
RECEIVED

AUG 23 2023

COMMUNITY DEVELOPMENT



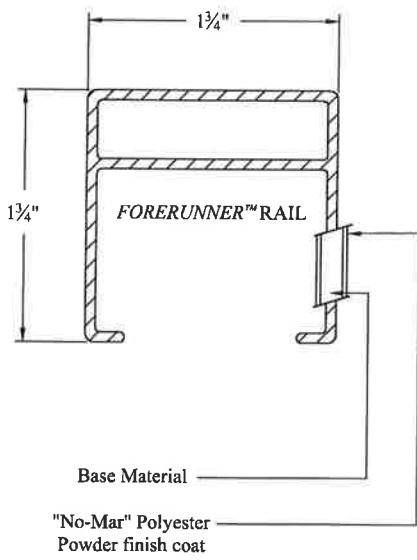
EXHIBIT J-1



NOTES:

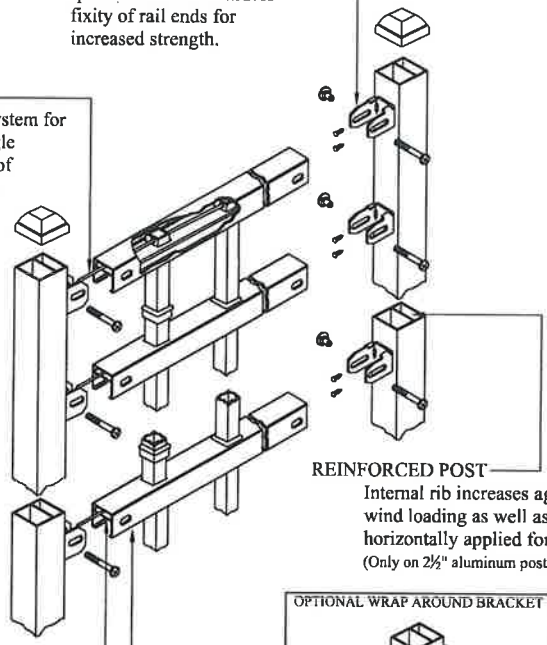
- 1.) Post size depends on fence height and wind loads. See *ECHELON II*™ post sizing chart.
- 2.) Panels also available for 6' on center post spacing
- 3.) Additional heights available on request. Some heights noted require a third and/or fourth rail.
- 4.) Third and Fourth rail optional.

EXHIBIT J-2



PANEL BRACKET
Specially designed two-point connection ensures fixity of rail ends for increased strength.

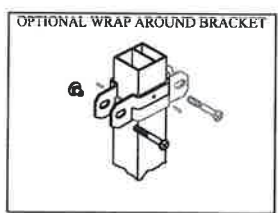
INTERNAL RETAINING ROD
Variable pitch connection system for ease of installation, high angle bias ability and elimination of unsightly external fasteners.



REINFORCED POST
Internal rib increases against wind loading as well as other horizontally applied forces (Only on 2 1/2 inch aluminum posts).

INTERIOR GUIDE CHANNEL
Channel forms lower limit of raceway for retaining rod and allows for high angle bias ability.

FORERUNNER™ RAIL
"U"-Channel specially formed high strength architectural shape.



Values shown are nominal and not to be used for installation purposes. See product specification for installation requirements.

INDUSTRIAL STRENGTH ALUMINUM

Title: ECHELON II MAJESTIC 2/3/4-RAIL			
DR: NJB	SH. 1 of 1	SCALE: DO NOT SCALE	
CK: BS	Date 2-07-12	REV: e	



AMERISTAR®

1555 N. Mingo
Tulsa, OK 74116
1-888-333-3422
www.ameristarfence.com

Traffic Impact Study Proposed Freight Terminal Facility

Unincorporated DuPage County, Illinois



March 15, 2023

1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed freight terminal facility located in the northeast quadrant of the intersection of North Avenue with Morton Road in Unincorporated DuPage County, Illinois. As proposed, the site (which is currently occupied by a truck parking facility) will be redeveloped to provide a freight terminal facility consisting of an approximately 47,100 square-foot building, 92 light vehicle parking spaces, 44 tractor parking spaces, 86 trailer parking spaces, and 81 loading docks. Access to the site will continue to be provided via Morton Road.

The purpose of this study was to examine background traffic conditions, assess the impact that the proposed facility will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate traffic generated by the proposed facility. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed facility
- Directional distribution of the facility traffic
- Vehicle trip generation for the facility
- Future traffic conditions including access to the facility
- Traffic analyses for the weekday morning and weekday evening peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning and weekday evening peak hours for the following conditions:

1. Existing Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area.
2. No-Build Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes increased by an ambient area growth not attributable to any particular facility.
3. Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the existing traffic volumes, ambient area growth not attributable to any particular development, and the net increase in traffic estimated to be generated by the proposed facility.



Aerial View of Site

Figure 2

2. Existing Conditions

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future conditions. The following provides a description of the geographical location of the site, physical characteristics of the area roadway system including lane usage and traffic control devices, and existing peak hour traffic volumes.

Site Location

The site is located in the northeast quadrant of the intersection North Avenue with Morton Road, is bounded by commercial uses to the east, North Avenue to the south, and Morton Road to the west, and is currently occupied by a truck parking facility. Land uses in the vicinity of the site include single family residences to the north, DuPage Water Conditions, Smoke Vapor, Dunkin Donuts and a strip retail development to the east, North Avenue Pub & Grill and Affinity Auto Sales to the south, a warehouse-distribution facility to the southwest, and Wheaton Bible Church to the west.

Existing Roadway System Characteristics

The characteristics of the existing roadways near the site are described below and illustrated in **Figure 3**.

North Avenue (Illinois Route 64) is an east-west arterial roadway that generally provides three lanes in each direction divided by a landscaped median. The median is wide enough to provide refuge for left-turn and U-turn movements as well as northbound and southbound through movements. At its unsignalized intersection with Morton Road, North Avenue provides exclusive left-turn and right-turn lanes on both approaches. North Avenue is under the jurisdiction of the Illinois Department of Transportation (IDOT), carries an Average Annual Daily Traffic (AADT) volume of 35,000 vehicles (IDOT AADT 2021), of which 4,050 vehicles are trucks, and has a posted speed limit of 45 mph. It should be noted that North Avenue is designated as a Strategic Regional Arterial (SRA) route. SRA routes are intended to carry larger volumes of traffic including industrial, commercial and passenger vehicles as a complement to the region's expressway system. Furthermore, North Avenue is the main arterial roadway through the central part of DuPage county that serves the existing industrial warehouse facilities along the roadway, connecting the City of St. Charles to the I-355 expressway.




Morton Road is a north-south collector roadway generally providing one lane in each direction separated by a two-way left-turn lane, that extends between Lies Road to the north and the Glasshagel Fields access drive to the south. Morton Road is under stop sign control at its intersection with North Avenue, where the northbound and southbound approaches provide a striped left-turn lane and a shared through/right-turn lane. Morton Road south of North Avenue is under the jurisdiction of the Village of Carol Stream. North of North Avenue, Morton Road carries an AADT volume of 1,750 vehicles (IDOT 2020) and has a posted speed limit of 40 miles per hour.



NOT TO SCALE



LEGEND

-  - TRAVEL LANE
-  - STOP SIGN
-  - SPEED LIMIT

Existing Traffic Volumes

In order to determine current traffic conditions within the study area, KLOA, Inc. conducted peak period traffic counts at the intersection of North Avenue with Morton Road. The traffic counts were conducted on Tuesday, March 7, 2023 during the weekday morning (6:00 A.M. to 9:00 A.M.) and weekday evening (3:00 P.M. to 6:00 P.M.) peak periods. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:15 A.M. to 8:15 A.M. and the weekday evening peak hour of traffic occurs from 4:30 P.M. to 5:30 P.M.

Traffic counts were also conducted at the existing access drive serving the truck parking facility off Morton Road to determine the trip generation of the facility during the weekday morning and weekday evening peak hours. The results of the traffic counts indicated that the facility generated 11 total trips during the weekday morning peak hour (five inbound and six outbound) and 16 total trips during the weekday evening peak hour (nine inbound and seven outbound). Of the total trips generated by the existing facility, two trips and eight trips during the weekday morning and weekday evening peak hours, respectively, were trucks.

Figure 4 illustrates the existing traffic volumes inclusive of truck traffic. **Figure 5** illustrates the existing semi-trailer truck traffic volumes. Copies of the traffic count summary sheets are included in the Appendix.

Crash Analysis

KLOA, Inc. obtained crash data¹ for the most recent available past five years (2017 to 2021) for the intersection of North Avenue with Morton Road. The crash data for the intersection is summarized in **Table 1**. A review of the crash data indicated that no fatalities were reported at the intersection between 2014 and 2018.

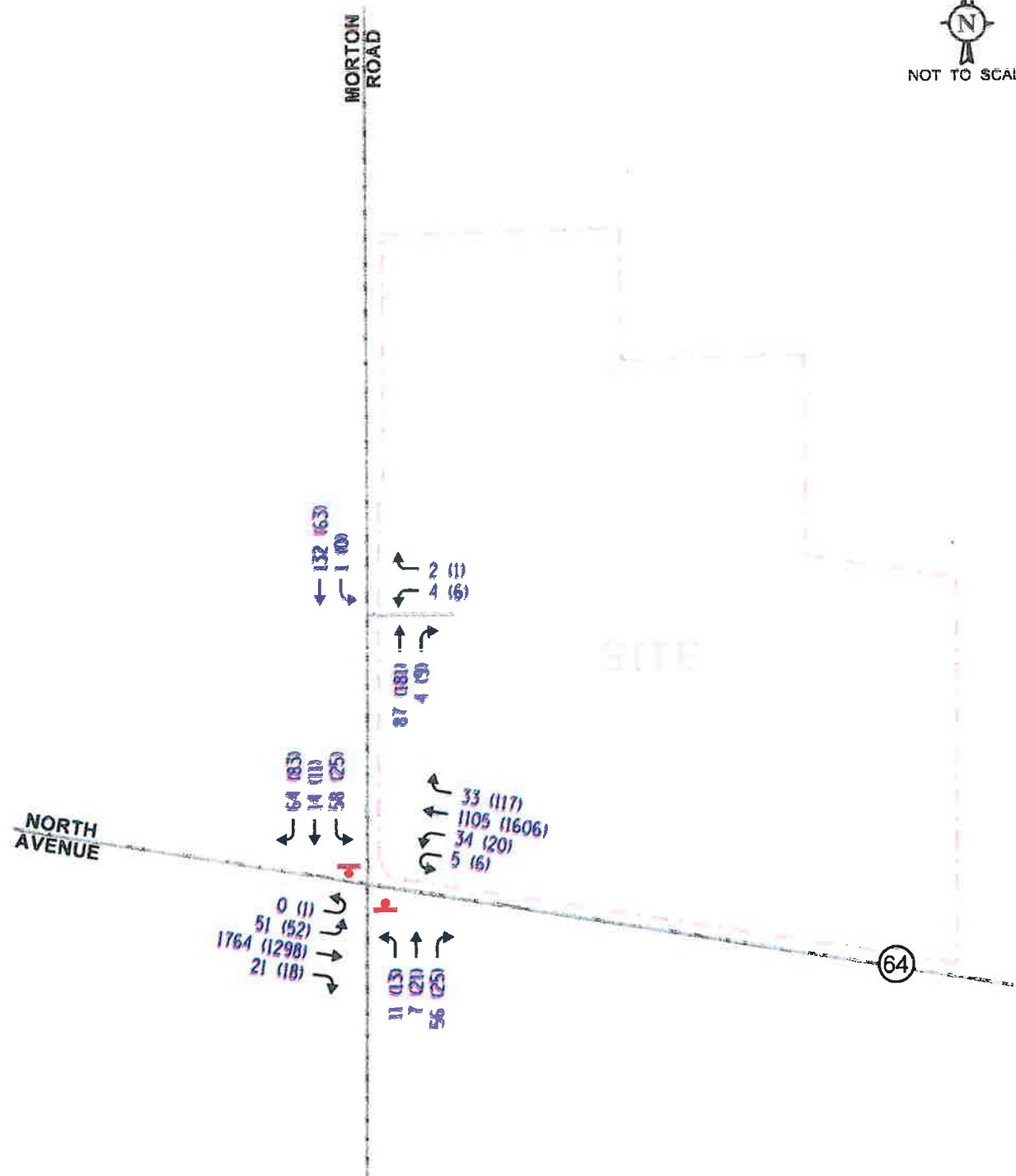
Table 1
NORTH AVENUE WITH MORTON ROAD – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2017	0	0	0	2	0	2	0	4
2018	1	0	1	0	0	2	0	4
2019	0	0	0	1	0	2	0	3
2020	0	0	0	1	1	1	0	3
2021	1	0	0	0	0	3	0	4
Total	2	0	1	4	1	10	0	18
Average	< 1	0	< 1	< 1	< 1	2	0	3.6

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s).



NOT TO SCALE



LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:30-5:30 PM)

Freight Terminal Facility
 Unincorporated DuPage
 County, Illinois

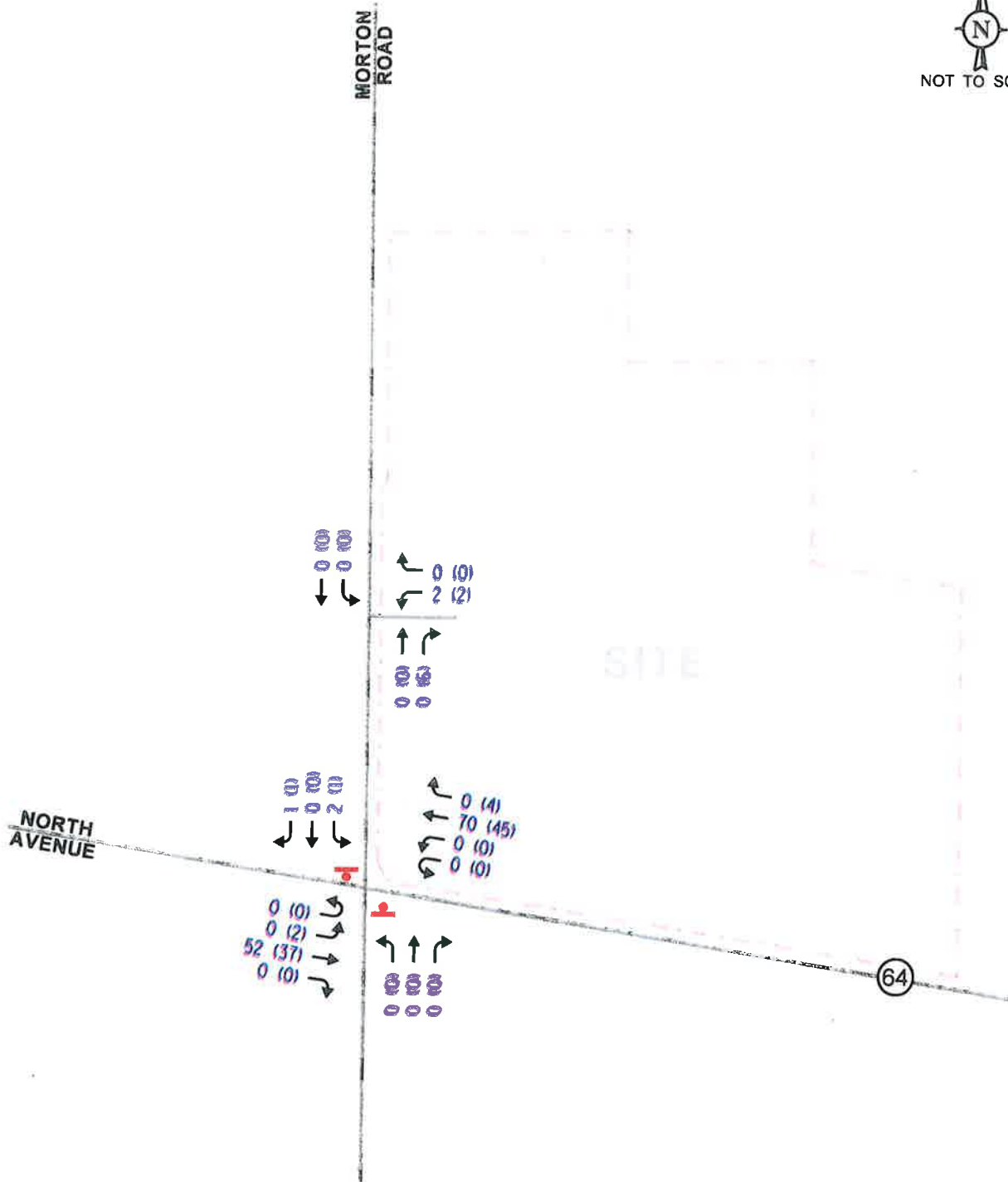
Existing Traffic Volumes

KLOA
 Kerig, Lindgren, O'Hara, Aboona, Inc.

Job No: 23-068 Figure: 4



NOT TO SCALE



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3. Traffic Characteristics of the Proposed Facility

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed facility, including the directional distribution and volumes of traffic that it will generate.

Proposed Site and Facility Plan

As proposed, the site will be redeveloped to provide a freight terminal consisting of an approximately 47,100 square-foot building and 81 loading docks of which 40 docks will be located on the south side of the building and 41 docks will be located on the north side of the building. A total of 92 parking spaces will be provided for light/passenger vehicles and these parking spaces will be located on the west side of the building. The northerly portion of the site will provide 44 parking spaces for tractors and 86 parking spaces for trailers.

Access to the facility will be provided via a proposed full movement access drive on Morton Road located approximately 635 feet north of North Avenue (centerline-to-centerline). This access drive will provide one inbound lane and one outbound lane and outbound movements should be under stop-sign control. It should be noted that this access drive will be the relocation of the existing full movement access drive serving the site approximately 170 feet to the north.

Additional access to the facility will be provided via a proposed access drive on Morton Road located approximately 180 feet north of North Avenue (centerline-to-centerline). Overall, it is recommended that this access drive be restricted via channelization to northbound right-turn in movements only based on the following:

- Due to the proximity of the drive to North Avenue and the operations of the North Avenue with Morton Road intersection (as discussed later), there will be times when a westbound left-turn from the access drive onto Morton Avenue will be difficult, particularly by tractor-trailer vehicles.
- As discussed later, only a small percentage traffic is projected to arrive to and depart the site from the north on Morton Road and it is recommended that this traffic be restricted to light/passenger vehicles only. With the provision of a full access further to the north on Morton Road, accommodating southbound left-turns in is not necessary at this location.
- Given that this access drive will be utilized by tractor-trailer vehicles, any channelization with an island will need to be mountable and as such, the left-turn out restriction for passenger vehicles will be difficult to enforce.

However, the provision of a right turn in access drive at this location will be beneficial as it will allow for site traffic to access the site just north of North Avenue, minimizing the volume of traffic on Morton Road. Additionally, it will allow trucks to access the southerly loading docks directly, minimizing the volume of truck activity on the north side of the proposed building.

To help enforce the right-in only access restriction, “Do Not Enter” signs should be placed at the connection of the access drive to the passenger vehicle parking lot and where the access drive connects to the paved area serving the loading docks.

Due to the residential nature of Morton Road to the north of the site, all truck traffic should be restricted to/from North Avenue. This restriction should be enforced via signage as well as by designing the northeast curb radii at the full movement access drive to physically prohibit trucks vehicles from making a westbound to northbound right-turn movement.

A copy of the proposed site plan illustrating the site and proposed access locations is included in the Appendix.

Proposed Facility Operations

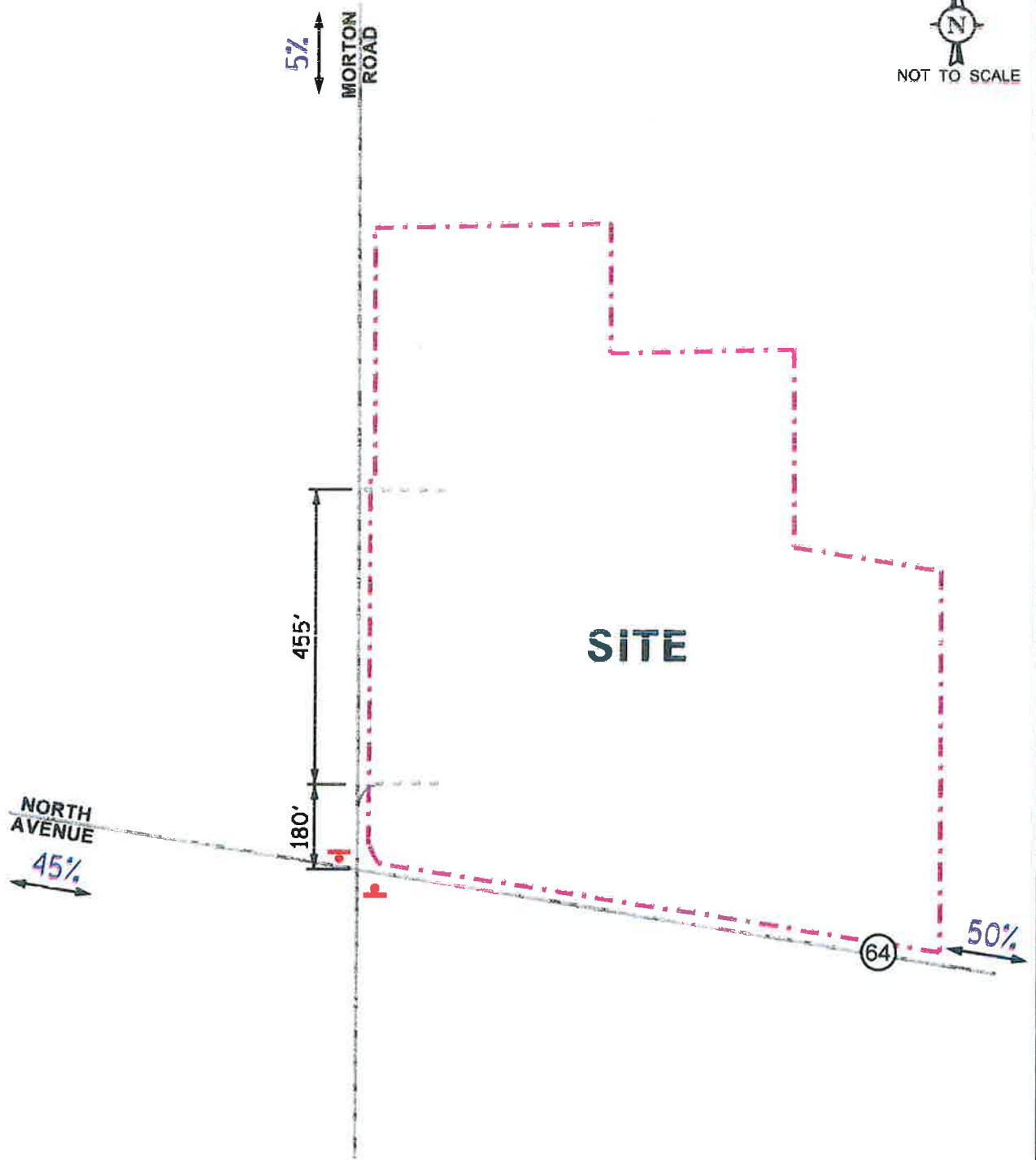
As proposed, the facility will operate seven days a week, with 24-hour operation Monday through Friday. During a typical peak weekday, it is anticipated that the facility will see approximately 20 trucks daily that make local pick-up and deliveries to the Chicagoland Area. Trucks typically departing in the morning between 5:00 A.M. and 8:30 A.M. and returning at various times throughout the day. On-site employees arrive between 6:00 A.M. and 9:00 A.M. and depart between 5:00 P.M. and 10:00 P.M. There are approximately 50 to 60 employees anticipated to be on site at any given time. During off peak times, the facility is estimated to have between 70 and 80 inbound/outbound line-haul trucks per week. The outbound trucks generally depart between the hours of 10:00 P.M. and 3:00 A.M. with the inbound trucks arriving between 4:00 A.M. and 11:00 A.M.

Directional Distribution

The directions from which traffic generated by the proposed facility will approach and depart the site were estimated based on existing travel patterns, as determined from the traffic counts. **Figure 6** illustrates the directional distribution of the facility-generated traffic. As indicated earlier, trucks will not be permitted to travel to and from the north on Morton Road



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00% - PERCENT DISTRIBUTION

00' - DISTANCE IN FEET

Facility Traffic Generation

The total number of vehicle trips to be generated by the proposed facility was estimated based on information provided by the operator of the proposed facility, as previously described. It was conservatively assumed that 50 percent of the truck and employee passenger vehicle activity occurs during the weekday morning and weekday evening peak hour. However, as previously indicated, these activities are dispersed over a three or more-hour period and thus result in a more evenly distribution of the arrival and departure of traffic. **Tables 2** summarizes the estimated trip generation for the facility. It should be noted that not all of the vehicle trips generated by the proposed facility will be new to the area, as the proposed facility will be replacing an existing truck parking facility that is currently generating traffic.

Table 2
ESTIMATED PEAK HOUR AND DAILY TRIP GENERATION – OPERATOR DATA

Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour			Daily Trips	
	In	Out	Total	In	Out	Total	In	Out
Truck Terminal	35	10	45	10	35	45	80	80
Truck Trips	5	5	10	5	5	10	20	20
Passenger Vehicle Trips	30	5	35	5	30	35	60	60

4. Projected Traffic Conditions

The total projected traffic volumes include the existing traffic volumes, increase in background traffic due to growth, and the traffic estimated to be generated by the proposed subject facility.

Facility Traffic Assignment

The estimated weekday morning and weekday evening peak hour traffic volumes that will be generated by the proposed facility were assigned to the roadway system in accordance with the previously described directional distribution (Figure 6). **Figure 7** illustrates the traffic assignment of the expansion generated trips. As previously recommended, all traffic to/from the north on Morton Road is representative a passenger vehicle with all trucks traveling to/from North Avenue.

Background Traffic Conditions

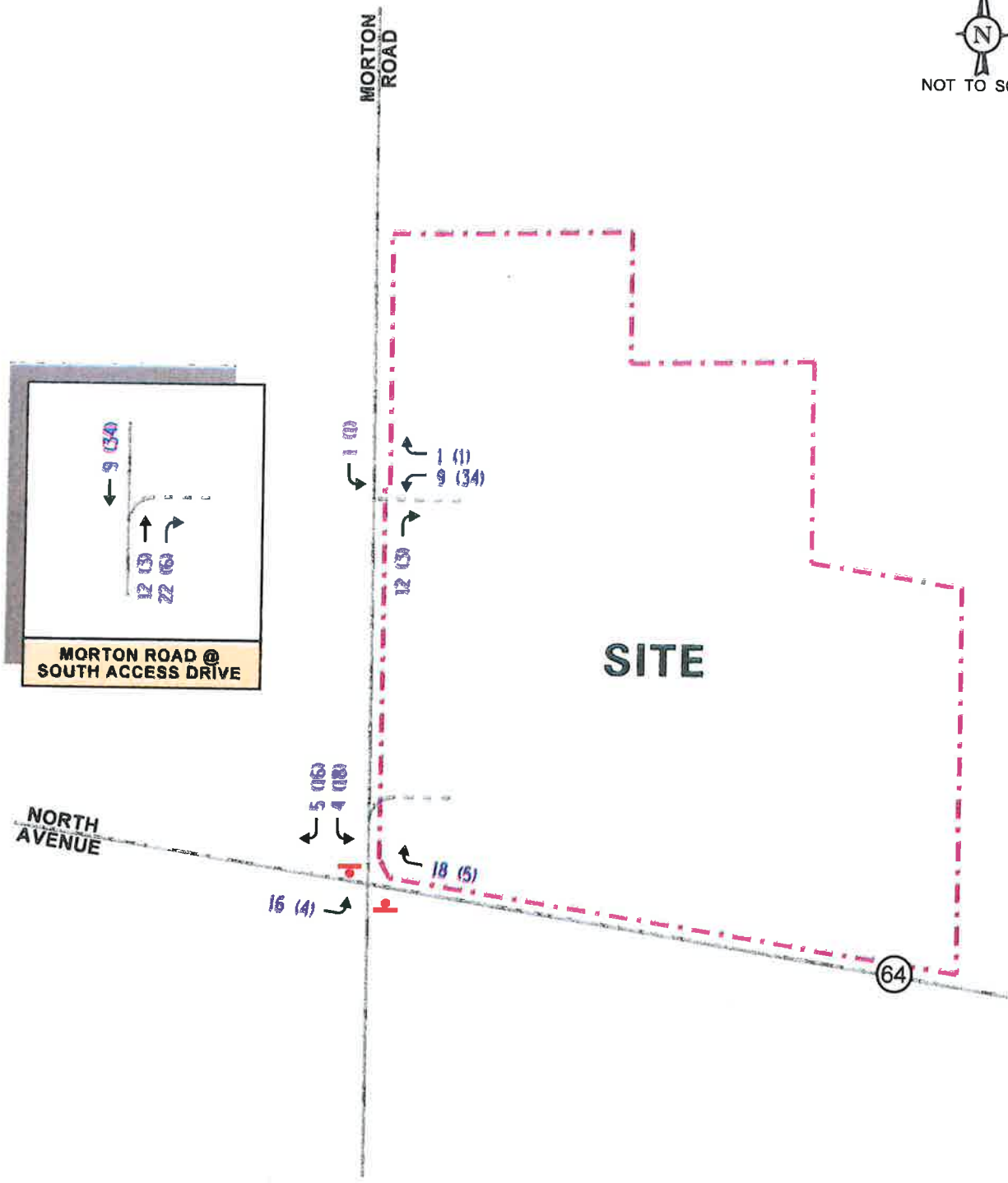
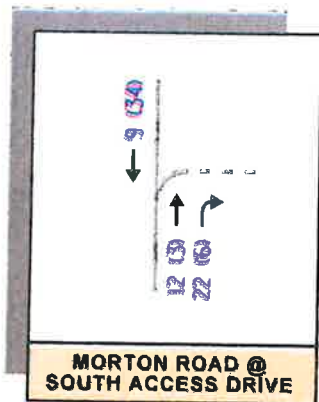
The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on ADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP) in a letter, the existing traffic volumes are projected to increase by a compound annual growth rate of 0.22 percent per year. As such, traffic volumes were increased by approximately 1.3 percent total to represent Year 2029 conditions (one-year buildout plus five years). A copy of the CMAP projections letter is included in the Appendix. The Year 2029 no-build traffic volumes are illustrated in **Figure 8**.

Total Projected Traffic Volumes

The facility-generated traffic (Figure 7) was added to the year 2029 no-build volumes (Figure 8) to determine the Year 2029 total projected traffic volumes, as shown in **Figure 9**. It should be noted that the results of the capacity analyses are conservative as the traffic generated by the existing truck parking facility was not removed from the area intersections.



NOT TO SCALE



LEGEND

- 00 - AM PEAK HOUR (7:15-8:15 AM)
- (00) - PM PEAK HOUR (4:30-5:30 PM)

Freight Terminal Facility
Unincorporated DuPage
County, Illinois

Site-Generated Traffic Volumes

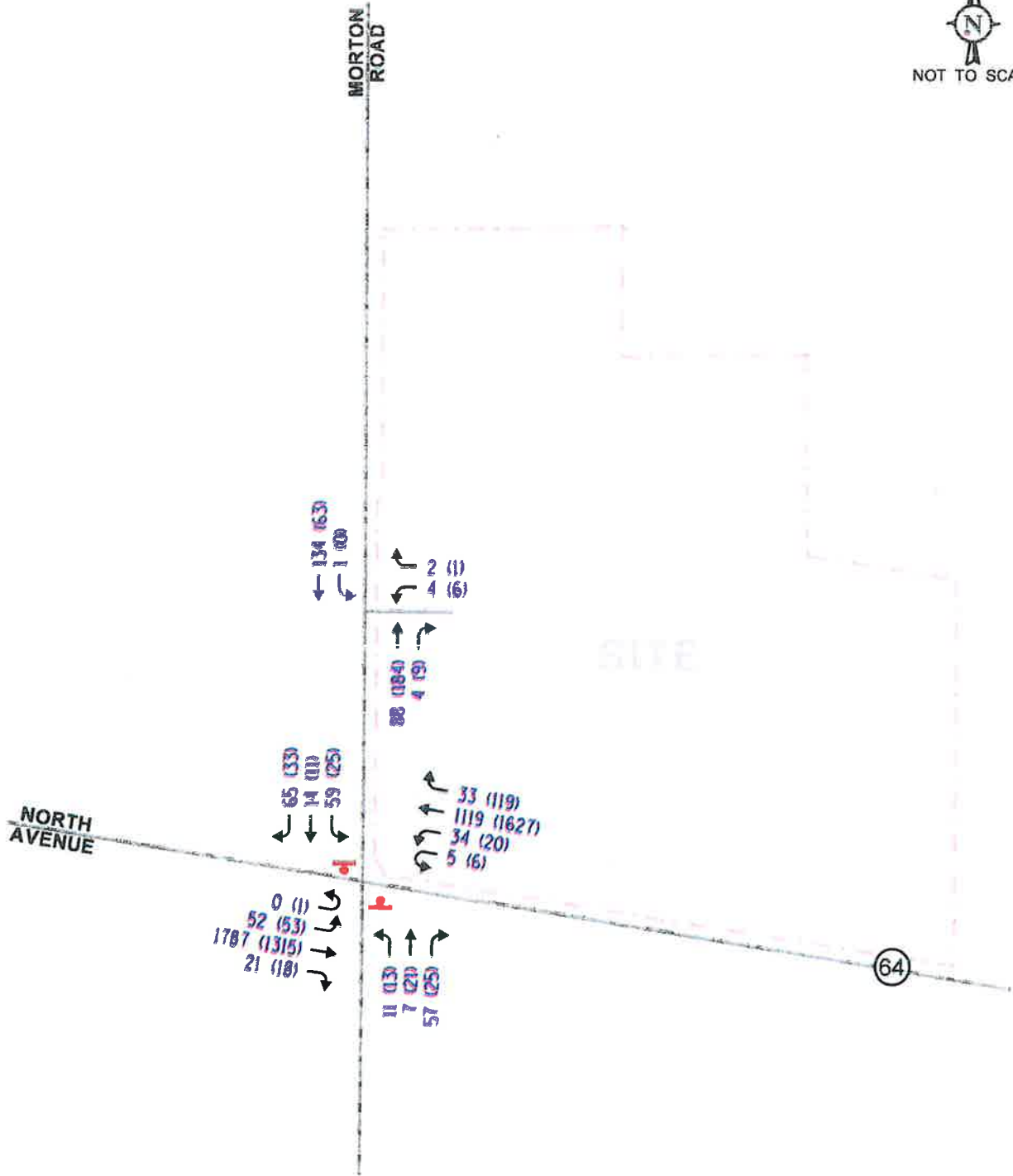


Job No: 23-068

Figure: 7

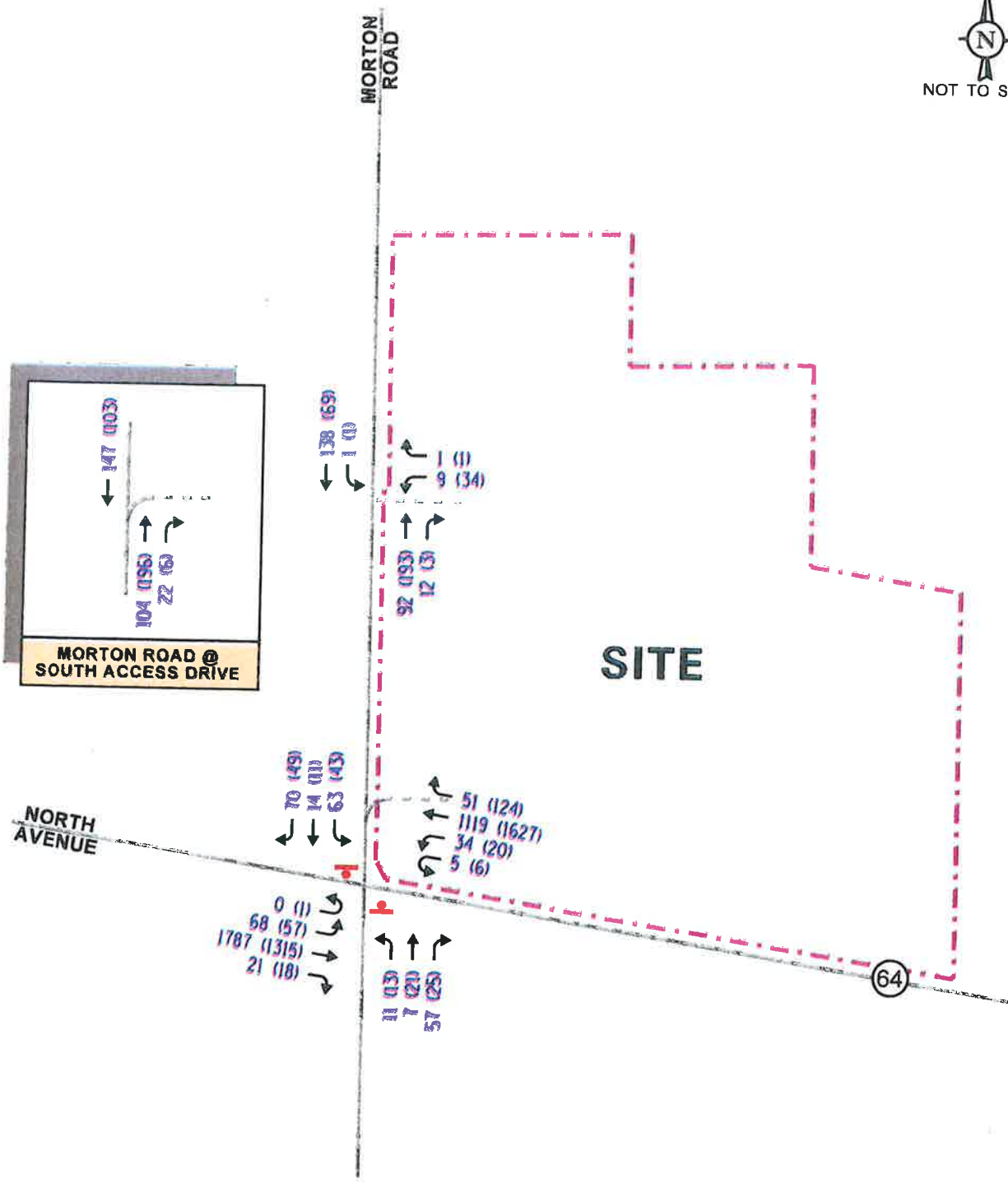
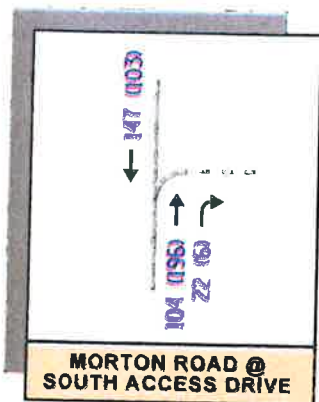


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- (00) - PM PEAK HOUR (4:30-5:30 PM)

5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning and weekday evening peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any roadway improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning and weekday evening peak hours for the existing, no-build, and Year 2029 total projected traffic volumes.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6th Edition and analyzed using Synchro/SimTraffic 11 software.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the existing, no-build and Year 2029 total projected conditions are presented in **Tables 3, 4, and 5**, respectively. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 3
CAPACITY ANALYSIS RESULTS – EXISTING CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
North Avenue with Morton Road				
• Northbound Approach	F	50.6	E	39.5
• Southbound Approach	E	40.6	C	23.3
• Eastbound Left Turn	A	9.7	B	12.0
• Westbound Left Turn	E	37.9	C	20.9
LOS = Level of Service Delay is measured in seconds.				

Table 4
CAPACITY ANALYSIS RESULTS – NO-BUILD CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
North Avenue with Morton Road				
• Northbound Approach	F	52.0	E	41.9
• Southbound Approach	E	43.5	C	24.1
• Eastbound Left Turn	A	9.8	B	12.4
• Westbound Left Turn	E	39.2	C	21.3
LOS = Level of Service Delay is measured in seconds.				

Table 5
CAPACITY ANALYSIS RESULTS – YEAR 2029 TOTAL PROJECTED CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
North Avenue with Morton Road				
• Northbound Approach	F	54.8	E	42.5
• Southbound Approach	F	52.4	D	25.5
• Eastbound Left Turn	B	10.1	B	12.6
• Westbound Left Turn	E	39.2	C	21.3
Morton Road with Full Access Drive				
• Westbound Approach	B	10.7	B	11.4
• Southbound Left Turn	A	7.4	A	7.7
LOS = Level of Service Delay is measured in seconds.				

Discussion and Recommendations

The following summarizes how the intersections are projected to operate and identifies any roadway and traffic control improvements necessary to accommodate the facility-generated traffic.

North Avenue with Morton Road

The results of the capacity analyses indicate that the southbound approach currently operates at level of service (LOS) E during the weekday morning peak hour and at LOS C during the weekday evening peak hour. The northbound approach currently operates at LOS F during the weekday morning peak hour and at LOS E during the weekday evening peak hour. Furthermore, eastbound left-turning movements from North Avenue onto Morton Road currently operate at LOS A during the weekday morning peak hour and LOS B during the weekday evening peak hour. Westbound left-turning movements from North Avenue onto Morton Road currently operate at LOS E during the weekday morning peak hour and at LOS C during the weekday evening peak hour.

It should be noted that the existing levels of service for the northbound and southbound approaches and the westbound left-turn movements are common for stop sign-controlled intersections along higher volume arterial roadway similar to North Avenue. Typical of other unsignalized intersections along North Avenue, the minor approaches rely on the gaps in traffic created by the platooning on North Avenue as well as the wide landscaped median, which provides refuge for turning and through movements on Morton Road. It should be noted that the analyses did take into consideration the traffic signal located at the intersection of North Avenue with County Farm Road which creates gaps in the westbound North Avenue traffic stream.

Under Year 2029 no build traffic volumes, all of the approaches and movements are projected to continue operating at existing levels of service with increases in delay of approximately three seconds or less over existing conditions.

Under Year 2029 total projected traffic volumes, all of the approaches and movements are projected to continue operating at no-build levels of service with increases in delay of approximately three seconds or less, except for the eastbound left-turn movement during the weekday morning peak hour which is projected to operate at LOS B and the southbound approach during the weekday evening peak hour which is projected to operate at LOS D. The volume to capacity ratios for the northbound and southbound approaches are projected to be less than one, indicating that both approaches have sufficient reserve capacity to accommodate the traffic estimated to be generated by the proposed facility. Furthermore, the 95th percentile queues for the northbound and southbound approaches are projected to be two to three vehicles which is an increase of one vehicle over existing conditions. Overall, the proposed development is only projected to increase the volume of traffic traversing this intersection by approximately one percent during the peak hours.

As such, the traffic estimated to be generated by the proposed development will have a limited impact on the operations of this.

It should be noted that since North Avenue is an SRA route, a traffic signal is only warranted based on an 8-hour traffic signal warrant with the minor approach criteria modified to be a minimum of 150 vehicles for a two-lane minor approach. The weekday morning and weekday evening traffic volumes do not exceed 150 vehicles and, as such, it is anticipated that this intersection will not meet the 8-hour traffic signal warrant criteria. This intersection should be monitored in the future to determine the need for a signal.

Morton Road with Proposed Access Drives

The results of the capacity analyses indicate that outbound movements from the full-movement access drive onto Morton Road are projected to operate at LOS B during both peak hours. Furthermore, southbound left-turning movements are projected to operate at LOS A during the peak hours and these movements will continue to be accommodated via the existing two-way left-turn lane on Morton Road. As such, this access drive will be adequate in accommodating the traffic estimated to be generated by the proposed facility and no roadway or traffic control improvements will be required.

As previously indicated, it is recommended that the southerly access drive be restricted to right-turn movements in, given its proximity to North Avenue and limited volume of traffic accessing the site to/from the north on Morton Road. All inbound traffic will be able to operate under free flow conditions and vehicles will be able to access the employee parking lot or loading dock area without impacting the operations of Morton Road. Furthermore, the provision of this access drive will allow trucks to access the southerly loading docks directly, minimizing the truck activity along the north side of the proposed building. In order to accommodate inbound trucks efficiently, it is recommended that a 50-foot tapered right turn lane be provided on Morton Road at the right-in only access drive.

Overall, the proposed access system with the recommended configuration will be adequate in accommodating the traffic estimated to be generated by the proposed facility and will provide efficient and flexible access.

6. Conclusion

Based on the preceding analyses and recommendations, the following conclusions have been made:

- Unsignalized intersections along North Avenue rely heavily on the use of the wide landscaped medians and platooning of North Avenue traffic in order to complete minor movements.
- The results of the capacity analyses show that the existing roadway system generally has sufficient reserve capacity to accommodate the traffic to be generated by the proposed facility.
- Access to the site will continue to be provided via a full movement access drive on Morton Road with will be improved to better accommodate truck turning maneuvers. The supplemental right-in only access drive will increase access flexibility and minimize the truck activity on the north side of the site.
- Both access drives will be designed to accommodate truck turning maneuvers efficiently.
- In order to accommodate inbound trucks efficiently, it is recommended that a 50-foot tapered right turn lane be provided on Morton Road at the right-in only access drive.
- The proposed access system will be adequate in accommodating traffic estimated to be generated by the truck parking facility and will ensure efficient and flexible access will be provided.