

Village of Carol Stream

AGENDA

REGULAR MEETING-PLAN COMMISSION/ZONING BOARD OF APPEALS

MONDAY, SEPTEMBER 25, 2023 AT 6:00 P.M.

ALL MATTERS ON THE AGENDA MAY BE DISCUSSED, AMENDED AND ACTED UPON

I. Roll Call: Present:
Absent:

II. Approval of Minutes: September 11, 2023

III. Public Hearing

23-0033 – North Avenue TH LLC / Mainfreight – 27W364 North Avenue

Zoning Map Amendment Upon Annexation

A Special Use Permit for a Motor Freight Terminal

Landscaping Variation

Loading Dock Variation

Fence Variation

North Avenue Corridor Review

Plat of Consolidation

A Special Use Permit for Outdoor Vehicle Storage

Variations from Subdivision Regulations

23-0036 – Village of Carol Stream – 500 N. Gary Avenue

Text Amendment for Gaming Cafes

IV. Presentation:

V. Old Business:

VI. New Business:

VII. Report of Officers:

VIII. Adjournment:

**Regular Meeting – Plan Commission/Zoning Board of Appeals
Gregory J. Bielawski Municipal Center, DuPage County, Carol Stream, Illinois**

***All Matters on the Agenda may be Discussed, Amended and Acted Upon
September 11, 2023.***

Mr. Farace stated since Chairman Parisi was absent an Acting Chairman would need to be nominated. Commissioner Petella made a motion to nominate Commissioner Tucek and Commissioner Meneghini seconded the motion.

Acting Chairman Tucek called the Regular Meeting of the Combined Plan Commission/Zoning Board of Appeals to order at 6:00 p.m. and directed Patty Battaglia, Planning and Permitting Assistant, to call the roll.

The results of the roll call vote were:

Present: 4 Commissioners Battisto, Meneghini, Petella, and Acting Chairman Tucek

Absent: 3 Commissioners Christopher, Morris and Chairman Parisi

Also Present: Tom Farace, Planning and Economic Development Manager; Bravo Berisha, Assistant Planner; Patty Battaglia, Planning and Permitting Assistant and a representative from County Court Reporters

MINUTES:

Commissioner Petella moved and Commissioner Battisto seconded the motion to approve the minutes of the Regular Meeting held on July 24, 2023.

The results of the roll call vote were:

Ayes: 4 Commissioners Battisto, Meneghini, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 3 Commissioners Christopher, Morris and Chairman Parisi

The motion passed by a unanimous vote.

PUBLIC HEARING:

Acting Chairman Tucek asked for a motion to open the Public Hearing. Commissioner Meneghini moved and Commissioner Petella seconded the motion.

The results of the roll call vote were:

Ayes: 4 Commissioners Battisto, Meneghini, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 3 Commissioners Christopher, Morris and Chairman Parisi

The motion passed by unanimous vote.

Case #23-0033 – North Avenue TH LLC / Mainfreight – 27W364 North Avenue

Zoning Map Amendment Upon Annexation

A Special Use Permit for a Motor Freight Terminal

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Plat of Consolidation

Acting Chairman Tucek swore in Mr. Connor Harmon, Timber Hill Group/Mainfreight, 8770 W. Bryn Mawr, Chicago, IL; Mr. James Defily, Timber Hill Group/Mainfreight, 10707 Franklin Avenue, Franklin Park, IL and Mr. Fred Thaete, Cage Engineering, 2200 Cabot Drive, Lisle, IL.

Mr. Harmon stated we are proposing to develop an office and terminal development at 27W364 North Avenue. The site is approximately 15.8 acres and consists of two buildings on site totaling just over 4,025 square feet. The property currently exists in unincorporated DuPage County. Earlier this year under the entity North Avenue TH LLC, the parent company Timber Hill Group, purchased the property and DuPage County approved our development on June 27, 2023. Our proposal is for a 37,100 square foot terminal with a 20,000 2-story office and that is a 10,000 square foot footprint. Therefore, the total footprint is 47,100 square feet. We propose 81 dock positions, 1 drive-in door, 92 auto stalls, 44 tractor stalls and 86 trailer stalls. From a setback perspective, we maintain at least a 60 foot setback to all single family residential to the north and east and exceed that in most areas. There is a 21 foot setback to the adjacent eastern property and over 150 foot setback to the southern property line abutting North Avenue, and at least a 32 foot setback to the western property line abutting Morton Road.

In regards to landscaping, we propose a 60 foot forested and natural buffer which will be established and maintained along the northern and eastern property line. A berm to the west will be installed along Morton Road which will consist of significant landscaping. We are pursuing a foundation landscape variation along the southern building façade. We plan to exceed code from a landscape perspective on other areas of the site. Finally, Mainfreight is interested in installing several sustainable design elements including drain water collection, rooftop solar panels, and EV charging stations all with a goal of nearing a net zero, building energy use.

From a photometric perspective, all exterior lighting will meet code and the light will not spill over into adjacent properties.

From a storm water perspective, all infrastructure is designed in accordance with the county wide Storm Water Management Ordinance and everything will be built to code.

From an architectural perspective, the building will feature a significant amount of glass, insulated precast panels and various outdoor amenity features which will be utilized by on-site employees. Mainfreight will be relocating their North American headquarters operation to this location and will bring up to 50-60 high quality jobs. Normal office operations will be 7 a.m.- 5 p.m., Monday through Friday. The site will see approximately 24 Mainfreight owner/operator truck drives a day. Truck traffic would preliminarily be 7-9 a.m. and returning throughout the day. Trucks will utilize both access points, will not idle and there will be no refrigerated trailers. The operation does have the ability to expand, but the majority of the yard will be for storage purposes. The traffic study states there is efficient, reserve capacity to accommodate the traffic to be generated by the proposed facility. Full access will be on Morton Road. The southern access will be right-in only. We will ensure efficient and flexible access. Signage and a modified curb design will prohibit trucks from turning right on Morton Road.

We feel our proposals are within the purpose and intent of the UDO by providing a world class terminal office building, relocating a company's headquarters to this location will provide up to 60 high quality jobs, and significantly improving the existing property. Further, our development decreases the amount of truck and auto trips daily, removes the dust nuisance from the previous use, improves storm water

infrastructure on the site and surrounding properties, and will significantly raise the assessed value of the property which will result in additional tax revenue for the Village. Overall, we believe Mainfreight will be a great addition to the Village.

Acting Chairman Tucek asked for any questions or comments from the audience and there were none.

Mr. Farace stated there are a variety of items being requested tonight.

- Rezone the property upon annexation to Industrial
- Special Use Permit for a motor freight terminal
- Variations associated with the truck docks on the south side of the building
- Variation to allow a fence to be located in the front yard
- North Avenue Corridor Review
- Plat of Consolidation to consolidate the existing lot

As Staff was reviewing plans, we discovered there were a few other requests that needed to be called out as variations as well as a Special Use request for the outdoor vehicle storage. These were not included in the initial Public Notice. Therefore, we will have the discussions tonight and then continue the hearing on September 25th, 2023, to discuss those other items. The vote for the whole project in its entirety will take place at the next meeting on September 25th.

As indicated, this proposal will be for the North American Headquarters for Mainfreight, a logistics company which is based in New Zealand, which will be relocated here in Carol Stream.

Staff is supportive of rezoning the property upon annexation to Industrial.

This property will have one user, Mainfreight, so the number of trucks coming in and out of the property will be controlled. A traffic study was provided and a portion of the trucks will be out for longer hauls which limits the amount of truck traffic on the property. Staff questioned the amount of truck trailer parking on the north side. Since this is the headquarters, there will be a surplus of trucks and trailers to accommodate the product movement. Staff would like to see some type of physical barrier to ensure trucks do not go Northbound on Morton Road. Likewise, there were some concerns with the proposed, right-in south driveway only which is very close to the Morton Road and North Avenue intersection. If you were to allow trucks to exit at that location, there would be a potential backup right at that intersection which does not have a traffic signal. The Village and Mainfreight would love to have a traffic signal at that intersection, it has been reviewed by IDOT and they deemed the amount of traffic is not sufficient enough to qualify for a signal. Parking is more than adequate and Staff is appreciative of the landscaping proposed for the property which meets UDO requirements. The proposed building elevation is very sleek and modern with color bands along the North Avenue corridor and the two-story component is sharp looking with a lot of glass and a large canopy feature for the entrance. This meets the components of our North Avenue Corridor regulations within the UDO and Staff is supportive of those.

There are three variations that are being requested tonight and then there will be a few other variations that will need to be reviewed in two weeks. The loading dock variation is requested because the UDO states that service areas should not be visible from the roadway. This is being requested due to the property being wider at the south end and then narrows as you go further North. Therefore, the building needed to be reoriented in order to work. The loading docks will be approximately 200 feet away from North Avenue which is a good distance away.

In our code, we ask that fencing not be installed passed the front of the building, but there is a 6 foot tall, open fence along the north and east side of the property and will extend passed the terminal building to the northeast corner of the detention building. It has an open end design and will not block any visibility of traffic. Staff is comfortable supporting this variation request. Although, we are recommending the fence be carried along the west side of the property for a continuity and aesthetics perspective.

There are other variations that have to do with upgrades to Morton Road and will be discussed in a few weeks as well as another Special Use Permit for outdoor vehicle storage.

Since there are separate lots, there will be a plat of consolidation into just one lot. Staff is supportive of consolidating the lot.

Staff is supportive of the project and we are recommending approval, but ask that the Plan Commission have their vote in two weeks when we review the other items.

Acting Chairman Tucek asked for questions from the Commission.

Commissioner Battisto asked about the fence extension all the way to the setback.

Mr. Harmon replied the plans will be revised before the next meeting, and it would go just north of the detention area and would not include the auto stalls.

Commissioner Battisto asked about the IDOT restriction of turning only right into the property. Are there any restrictions in regards to the egress. Can you head east or west from Morton onto North Avenue.

Mr. Harmon replied we have not heard of any restrictions by IDOT at this point.

Commissioner Battisto stated so there will be trucks potentially turning to head east on North Avenue. That median is not long enough to support a full size truck. Cars will get to that easement and wait to complete their turn east. This will not be available to Mainfreight. Do you see that being a problem.

Mr. Harmon stated both our preferences is to have a signal, but that is not an option. I think our main point is there will be a lot less trucks doing that with this new use. We haven't seen that as an issue by any means.

Commissioner Battisto stated he appreciates Mainfreight bringing their headquarters to Carol Stream, but we just want to make sure that things are going to work properly. This next question is for Mr. Farace, when you're talking about the industrial park we have, all the roads are very wide. Morton Road is basically a side street but without curbs. Is that wide enough.

Mr. Farace stated we will review all of that more thoroughly. Keep in mind at the southwest corner of this property there is another distribution facility which is somewhat similar. They can go eastbound on North easily, but if they wanted to go westbound on North, they might have to be creative and go to the next roadway.

Commissioner Petella asked how many trucks can you stack up from your north exit to North Avenue.

Mr. Harmon replied I would have to get an exact number, but if I speculate, I would say four to five.

Commissioner Petella asked what do you need 81 docks for if you're going to have 20 trucks leaving every day. Do you foresee this is going to boom. Are we talking 20 trucks today, but then 40 trucks, 60 trucks, 80 trucks in the future.

Mr. Defily stated he works for Mainfreight as a Property Manager for North America. Regarding the size of the property, right now all of that data is based off of our current in and out use on a daily basis in Franklin Park. Our future plans are when we move into this facility on day one we won't be operating at full capacity. The goal is to continue to grow throughout North America.

Commissioner Petella asked how many docks do you have in Franklin Park.

Mr. Defily replied 18.

Commissioner Petella responded so you are going to go 4 times bigger than what you have now.

Mr. Defily replied correct. And the goal here is we continue to build our network throughout the United States. Other cities and larger markets would then come on and would end up increasing our paths through Chicago here. It will also build our network within the Chicagoland area.

Commissioner Petella stated with that will come more traffic then too. Nobody ever builds more than what they think they are going to need. He then asked if the mechanicals for the office building will be shielded from the roadway.

Mr. Harmon stated we would follow all Village code related to that.

Commissioner Petella asked if they were going to put their name on top of the building and how much solar panel space are you going to need.

Mr. Harmon stated that is an option but we have not gone through final design of that but we want to maximize as much as we can per code and the sign might be on the hammerhead of the office.

Commissioner Petella asked if there would be security gates and if there would be landscaping on the other side of the parking lot in front of the retention pond.

Mr. Harmon stated there would be security gates and there are some existing trees and shrubs and we have some proposed there as well, but if we need to beef that up a little bit we can. At this point I think we meet code.

Mr. Farace stated we would want to work with our Civil and Storm Water Engineers since that is a detention basin because you are somewhat limited as to what you can plant there because it slopes into the pond.

Commissioner Petella stated your property is low. Is that going to be raised up to grade or to the height of North Avenue or will it remain. And, since you are incorporating into Carol Stream, you're going to be tying into our sewers and our water and all that. Is that all figured out.

Mr. Thaete from Cage Engineering stated yes, it will be elevated above and will not be at its current level. You've got sanitary sewer and water main along North Avenue for us to tie into.

Commissioner Meneghini had no questions but complimented the Staff on an excellent report and commentary.

Acting Chairman Tucek agreed with Commissioner Meneghini and stated it was an excellent report on both sides. He then asked if there was going to be a scale on site.

Mr. Defily replied we had intentions to do so if it was allowable by code.

Mr. Farace stated we would want to see where it will be located on the site.

Acting Chairman Tucek stated on the left turn south out onto Morton Road, my concern is the response for emergency will be from the fire station to the north on Lies Road. If they are heading south on Morton, it would make it a lot easier to make that left in if there was not an obstruction to turn there.

Mr. Farace stated we are suggesting that the north curb be modified to the point where you can't turn out. If a fire truck needs to get to this property, they are going to go in through any driveway if there is a fire.

Acting Chairman Tucek asked what are your freight commodities. Are you doing any hazardous freight.

Mr. Defily replied no dangerous good products. A lot of it is home improvement products, home fitness, industrial materials but no dangerous materials.

Acting Chairman Tucek stated docks will have blinking lights. Do you have the ability to turn those off on the docks facing North Avenue. I understand there for safety.

Mr. Defily asked are you talking about the structure itself or the vehicles.

Acting Chairman Tucek stated the dock lights. The green, red blinking lights. Some places leave them blinking all the time.

Mr. Defily stated he would have to confirm.

Acting Chairman Tucek stated since you do not have fueling on site, I assume you're going to purchase your fuel through a Carol Stream location. This comment was made in light – we do care about our neighbors.

Acting Chairman Tucek asked for a motion for a continuance. Commissioner Battisto moved and Commissioner Petella seconded the motion with no further discussion.

The results of the roll call vote were:

Ayes: 4 Commissioners Battisto, Meneghini, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 3 Commissioners Christopher, Morris and Chairman Parisi

The motion passed by unanimous vote.

Acting Chairman Tucek asked for a motion to close the Public Hearing. Commissioner Petella moved and Commissioner Meneghini seconded the motion.

The results of the roll call vote were:

Ayes: 4 Commissioners Battisto, Meneghini, Petella and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 3 Commissioners Christopher, Morris and Chairman Parisi

The motion passed by unanimous vote.

NEW BUSINESS:

PRESENTATION:

OLD BUSINESS:

OTHER BUSINESS:

ADJOURNMENT:

At 6:55 p.m. Commissioner Petella moved and Commissioner Battisto seconded the motion to adjourn the meeting.

The results of the roll call vote were:

Ayes: 4 Commissioners Battisto, Meneghini, Petella, and Acting Chairman Tucek

Nays: 0

Abstain: 0

Absent: 3 Commissioners Christopher, Morris and Chairman Parisi

The motion passed by unanimous vote.

FOR THE COMBINED BOARD

Recorded and transcribed by,

Patty Battaglia
Planning and Permitting Assistant

Minutes approved by Plan Commission on this ____ day of _____, 20____.

Acting Chairman

Village of Carol Stream Plan Commission/Zoning Board of Appeals

STAFF REPORT

September 25, 2023

TO:
Chairperson and Plan Commissioners

FROM:
Community Development Department

CASE MANAGER:
Tom Farace, Planning & Economic
Development Manager

ACTION REQUESTED:
The applicant is requesting approval of the
following:

Reviewed at the September 11, 2023 Meeting

- A Zoning Map Amendment Upon Annexation, in accordance with Section 16-8-4 (N) of the UDO
- A Special Use Permit for a Motor Freight Terminal, in accordance with Section 16-3-11 (E) of the UDO
- A Variation to allow loading docks along the south side of the building, in accordance with Section 16-5-16 (C)(1) of the UDO
- A Variation to eliminate foundation landscaping along the south side of the building, in accordance with Section 16-5-6 (B)(3)(a) of the UDO
- A Variation to allow a fence to be located in the front yard, in accordance with Section 16-5-8 (F)(2) of the UDO
- North Avenue Corridor Review, in accordance with Section 16-8-4 (J) of the UDO
- Plat of Consolidation, in accordance with Section 16-8-4 (O) of the UDO

Additional Requests to be reviewed at the
September 25, 2023 Meeting

- A Special Use Permit for Outdoor Vehicle Storage, in accordance with Section 16-3-11 (E) of the UDO
- Variations from subdivision regulations, in accordance with Article 9 of the Carol Stream Unified Development Ordinance

APPLICANT/ CONTACT:

Mr. Connor Harmon
Timber Hill Group LLC
8770 W. Bryn Mawr, Suite 1350
Chicago, IL 60631



CASE #: 23-0033

LOCATION: 27W364 North Avenue

PROJECT NAME: North Avenue TH LLC / Mainfreight



LOCATION	ZONING DISTRICT	LAND USE	COMPREHENSIVE PLAN DESIGNATION
Subject Property	Unincorporated DuPage County R-2 Residence District	Commercial (Vacant)	Industrial (Village of Carol Stream)
North	Unincorporated DuPage County R-2 Residence District	Single-Family Residential	Single-Family Residential
South	B-3 General Business District	Commercial (North Ave Pub & Grill, Affinity Auto Sales)	Corridor Commercial
East	Unincorporated DuPage County B-2 General Business District	Commercial (DuPage Water Conditioning)	Commercial
West	Unincorporated DuPage County R-3 Residence District	Institutional (Wheaton Bible Church)	Institutional

The 15.76 property highlighted above is located at the northeast corner of North Avenue and Morton Road.

Site Assessment

COMPREHENSIVE PLAN DESIGNATION:

The subject property is designated for industrial uses if annexed into Carol Stream according to the Village's 2016 Comprehensive Plan. The proposed motor freight terminal potentially fits within this designation.

AERIAL PHOTOGRAPH:



Project Summary

ATTACHMENTS:

Attached for review is the General Application, General Variation Application, Fence Variation Application, Special Use Application, Map Amendment Application, Cover Letter from Timber Hill received August 21, 2023, Public Notice, Site Location Map (Exhibit A), Existing Conditions and

Demolition Plan (Exhibit B), Overall Site Plan and Detailed Site Plan (Exhibits C-1 and C-2), Site Grading Plan, Soil Erosion and Sediment Control Plan, and Site Utility Plan (Exhibits D-1, D-2, and D-3), Turning Exhibits (Exhibits E-1 and E-2), Landscape Plans and Details (Exhibits F-1 through F-6), Floor Plans (Exhibits G-1 through G-4), Elevations (Exhibits H-1 and H-2), Plat of Consolidation (Exhibits I-1 and I-2), Fence Detail (Exhibits J-1 and J-2), Gate Detail (Exhibits K-1 and K-2) and Traffic Study (Exhibit L).

BACKGROUND:

At the September 11, 2023 PC/ZBA meeting, Connor Harmon, Associate Director of Development with the Timber Hill Group, presented requests for the following items associated with the development of a Mainfreight motor freight terminal at the northeast corner of North Avenue and Morton Road:

- A Zoning Map Amendment to rezone the property to the I Industrial District upon annexation
- A Special Use Permit for a Motor Freight Terminal
- A Variation to allow loading docks along the south side of the building
- A Variation to eliminate foundation landscaping along the south side of the building
- A Variation to allow a fence to be located in the front yard
- North Avenue Corridor (NAC) Review
- Plat of Consolidation

Additional requests for subdivision-related variations pertaining to Morton Road improvements and a special use permit for outdoor vehicle storage were noticed in the newspaper after the original public notice for the project, requiring a second public hearing before the PC/ZBA. Discussion was held on the above referenced requests at the September 11 meeting, excluding the requests noticed separately, and the PC/ZBA continued the public hearing to their September 25 meeting. No recommendations for the requests were provided at the September 11 meeting, and the PC/ZBA should make a recommendation on the project and all associated requests at this time.

Additional requests include the following:

- A Special Use Permit for Outdoor Vehicle Storage
- Subdivision-related variations associated with proposed improvements to Morton Road

As previously described, North Ave TH LLC represents the property entity under the umbrella company Timber Hill Group. Timber Hill Group specializes in the acquisition and development of logistics-related industrial real estate throughout the U.S. The subject property will be the home of a 57,100 square-foot motor freight terminal for Mainfreight, Inc. Mainfreight is a New Zealand-based freight carrier that focuses on transport and warehousing of goods throughout the world. Mainfreight proposes to relocate its North American corporate headquarters from Los Angeles to the subject property, along with the creation of up to 60 jobs for the office component of the building. Earlier this year, the applicant received approval from the DuPage County Board for the

motor freight terminal within the County. However, the applicant prefers to annex into and develop in Carol Stream, and connect to existing municipal utilities along North Avenue.

Staff Analysis

SPECIAL USE FOR OUTDOOR VEHICLE STORAGE



As previously reviewed at the September 11 PC/ZBA meeting, a special use permit for a motor freight terminal was requested. Similarly, a special use permit for outdoor vehicle storage is also requested.

- The proposed development consists of a 57,100 square-foot building, which will include 81 loading docks (41 docks on the north side of the building and 40 docks on the south side of the building), along with a two-story office component on the west side of the building.
- Additional trailer and truck storage, totaling 136 spaces, is proposed on the northern half of the development, with automobile parking on the west side of the development that is separated from the truck/trailer parking and dock areas.

With the proposed loading docks and trailer storage, a special use is required for the outdoor storage of vehicles. Staff supports the request, and believes that the submitted plans illustrate screening of said outdoor vehicle storage with landscaping, fencing, and the building itself. In addition, the majority of the outdoor vehicle storage will be internal to the site, and not readily visible along the adjacent roadways or from adjacent properties.

SPECIAL USE FINDINGS OF FACT

The Plan Commission’s recommendation regarding the requested Special Use requests must be based on the evaluation criteria set forth in the UDO. As stated in §16-8-4 (L) of the UDO, no Special Use shall be recommended by the Plan Commission nor approved by the Village Board unless the Special Use:

1. Is deemed necessary for the public convenience at the location.

The redevelopment of the property with a modern motor freight terminal and screened outdoor vehicle storage will be considered a public convenience at the subject location.

2. Will not be unreasonably detrimental to or endanger the public health, safety, morals, comfort or general welfare.

The proposed outdoor vehicle storage is separated from automobile parking areas and maneuvering lanes within the development, and all components are designed and will operate in a safe manner, and should not be detrimental to or endanger the public health, safety, morals, comfort or general welfare.

3. Will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.

The surrounding area has a mix of industrial, commercial, institutional, and residential uses. The majority of the proposed outdoor vehicle storage will be located internal to the site, and will be screened from view. Therefore, the outdoor vehicle storage should not be injurious to the use and enjoyment of other properties in the immediate vicinity for the purposes already permitted, nor diminish or impair property values within the area.

4. Will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

Surrounding properties are already developed. As such, there should be no impact on the normal and orderly development and improvement of surrounding properties.

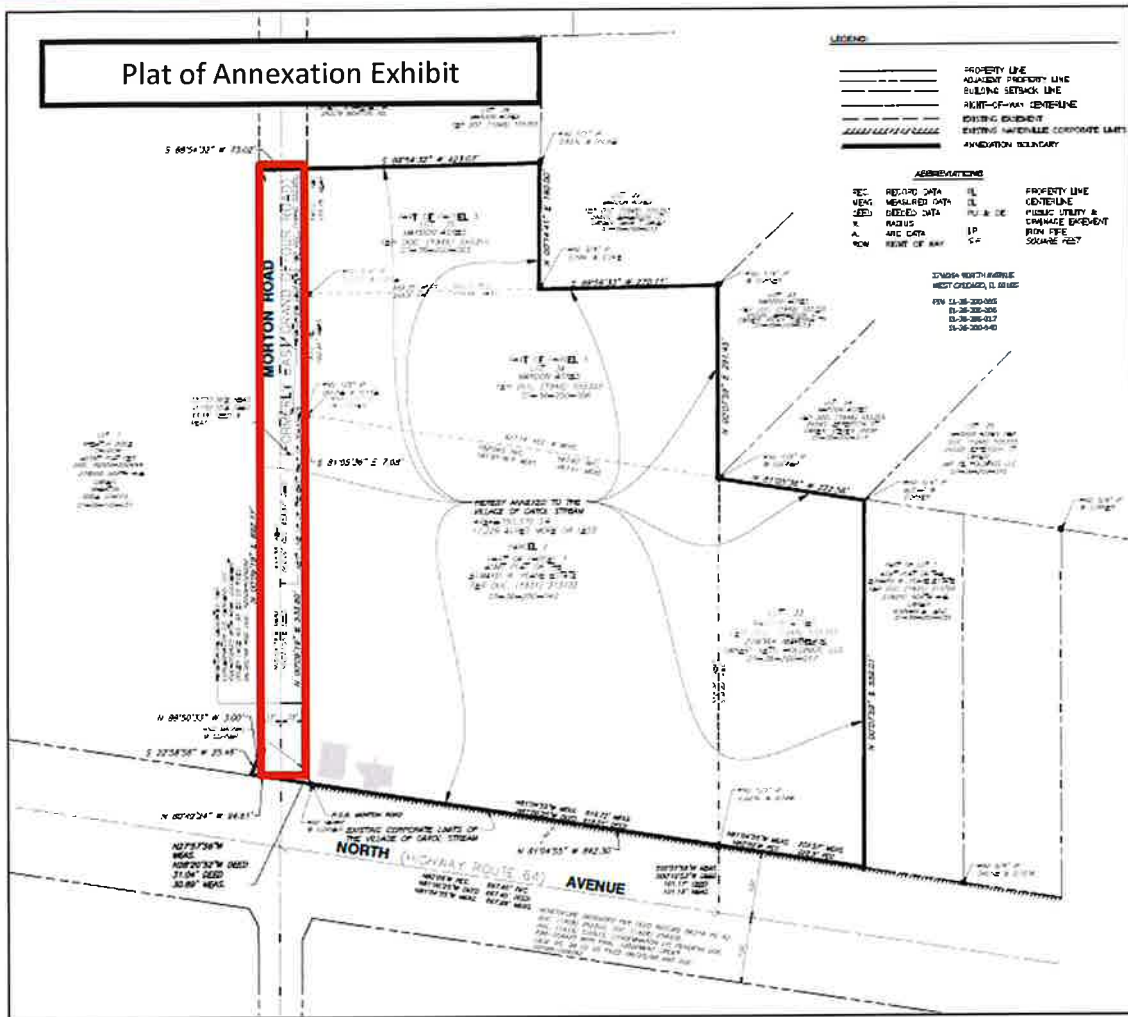
5. Will provide adequate utilities, access roads, drainage and other important and necessary community facilities.

Utilities, access roads, drainage and other public improvements will be constructed with the proposed development.

6. Will conform to the applicable regulations of the district in which it is located, except as the Village Board may in each instance modify such regulations.

The project is expected to conform to all applicable codes and requirements.

SUBDIVISION VARIATIONS – MORTON ROAD IMPROVEMENTS



The applicant is seeking to annex the property into the Village. An annexation plat and agreement are currently under review by Village staff, and will be reviewed by the Village Board at a future meeting. When a property is annexed into a municipality, said annexation typically extends to the far side of an adjacent roadway. In the case of the subject property, the adjacent roadways are North Avenue and Morton Road (see plat of annexation exhibit). When properties along the south side North Avenue (between Morton Road and County Farm Road) were annexed into Carol Stream a few years ago, the Village boundary was extended to the north side of North Avenue, and directly adjacent to the south property line of the subject property. With the requested annexation, the Village boundary will extend to the west side of Morton Road, for the entire length of the subject development (from the North Avenue intersection to roughly where the Morton Road entrance for Wheaton Bible Church is located on the west side of the roadway).

Morton Road, north of North Avenue, is currently under the jurisdiction of Wayne Township. Once the property is annexed into the Village, this section of Morton Road will be under Carol Stream’s jurisdiction. Article 9 of the UDO, and specifically Sections 16-9-3 and 16-9-4 of the UDO

and the Village's Design Standards and Construction Specifications Manual (The Manual), describe Village roadway standards. These standards include requirements for pavement and right of way width, and associated improvements including installation of sidewalks, streetlights, and parkway trees. The applicant is requesting the following variations associated with improvements to Morton Road:

Roadway Dimensions and Cross Section

As indicated, this section of Morton Road (north of North Avenue) is currently under the jurisdiction of Wayne Township. Wayne Township requires a roadway such as Morton Road to be constructed with a minimum of eight inches of asphalt on top of a four-inch stone base. Carol Stream roadway construction requirements would include four inches of aggregate (or stone) base with six inches of a Hot-Mix Asphalt base course, and four inches of Hot-Mix Asphalt binder course and two inches of Hot-Mix Asphalt surface course. Likewise, Carol Stream requires a minimum right of way width (including road pavement and parkways) of 80 feet, and for both sides of the roadway to have curb and gutter. Currently, the Morton Road right of way width is approximately 66 feet wide, and widens to approximately 73 feet further north.

The applicant will be dedicating seven feet of land along the west side of the southernmost parcel, to provide a continuous right of way width of 73 feet along Morton Road. Staff is supportive of the reduced right of way width in this instance, given the limited amount of land that could be dedicated without substantially impacting Mainfreight's site plan. In addition, staff supports allowing the applicant to resurface the section of Morton Road directly adjacent to the subject development based on Wayne Township roadway construction standards. This will include pavement removal and replacement along this section of Morton Road, up to 10 feet north of the proposed full access driveway along Morton Road. Furthermore, staff is supportive of not installing curb and gutter along the roadway. Currently, there is curb and gutter along both sides of Morton Road up to the existing driveway for the southernmost property.

Parkway Trees, Streetlights, and Sidewalks

The UDO and The Manual require the installation of parkway trees and streetlights within Village rights of way, along with the installation of a sidewalk. The right of way improvements are typically either installed by the developer and accepted by the Village, or the developer provides funds to the Village for future installation. While the submitted plans illustrate the installation of three streetlights along Morton Road, parkway trees and a sidewalk are not proposed to be installed. Staff supports the elimination of parkway trees and sidewalk installation for the proposed development, given the limited space for installation of such improvements, but recommends the plans be revised prior to Village Board review to remove proposed streetlights and curb and gutter illustrated on the plans. It should be noted that there is an existing bike path for pedestrians and bicyclists on the west side of Morton Road, which extends to the northernmost driveway of Wheaton Bible Church.

The Village Engineer has reviewed the proposed plans and is supportive of the variation requests and proposed Morton Road improvements.

VARIATION FINDINGS OF FACT

With regard to any variation, the Zoning Board of Appeals shall not recommend a variation unless it shall make findings based upon the evidence presented to it in the following case, as per Section 16-8-4 (K) of the Carol Stream Unified Development Ordinance:

1. The requested variations arise from conditions that are unique to the subject property, that are not ordinarily found in the same zoning district and that are not a result of the owner's intentional action.

The applicant proposes to improve the section of Morton Road that fronts the subject development to meet Wayne Township roadway standards. Said standards will accommodate the truck traffic proposed with the development, while still maintaining more of a rural cross section to fit with the remainder of the roadway.

2. The variations to be granted will not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property.

The proposed roadway width and associated right of way improvement variations will allow Morton Road to maintain a rural cross section while accommodating truck traffic for the proposed development. As such, the requested variations should not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property

3. The strict application of the applicable standards will constitute an unnecessary physical hardship (not economic hardship) because the property cannot be used for an otherwise allowed use without coming into conflict with applicable site development standards.

Expanding the roadway to meet the required Village right of way could impact the layout of the proposed development, which already has limitations based on the configuration of the site being wider on the south end and narrower on the north end. Likewise, parkway space is limited to install parkway trees, streetlights, and a sidewalk, especially with overhead power lines along the east of Morton Road.

4. The variations are the minimum action necessary to alleviate the hardship and observes the spirit of this UDO.

Staff believes the spirit of the UDO and roadway design standards will be met with the proposed improvements along Morton Road, while maintaining the rural cross section evident along the northern portion of the roadway.

5. The variations desired will not adversely affect the public health, safety, or general welfare or impair the purposes or intent of this UDO or the comprehensive plan.

It is not believed that the requested variations will have an adverse effect on public health, safety, or general welfare, and meets the intent of the UDO and Village Comprehensive Plan by bringing a high quality and attractively designed development into the community.

NORTH AVENUE CORRIDOR REVIEW

At the September 11 PC/ZBA meeting, review of the submitted site plan, landscape plan, and building elevations associated with the NAC review occurred. Staff provided recommendations for plan revisions prior to the September 25 PC/ZBA meeting. The following revisions have been addressed:

- Staff supports the full access driveway further north on Morton Road, but recommended that the driveway be modified to prevent trucks from traveling north on Morton Road. The applicant proposed to install signage that states “No Right Turn” onto Morton Road, but staff recommended additional measures such as installing a taller curb or placing items such as large bollards or boulders on the north side of the proposed driveway to preclude trucks from travelling northbound on Morton Road. Revised plans illustrate a modified curb line along the north side of the driveway, which reduces the curb radius and essentially eliminates space for a semi-truck to turn right and travel northbound on Morton Road. In addition, a nine-inch tall curb is proposed to further reduce semi-trucks from travelling northbound.
- Additional site details and notes were requested regarding security gates and a dumpster enclosure/trash compactor. Revised plans illustrate security gates at both driveways along Morton Road, and the applicant has provided a cut sheet for the proposed gate system. The gates are proposed approximately 100 feet east of both driveways, so trucks should not extend onto Morton Road when entering the property from either driveway. In addition, a note was placed on the site plan indicating that a future dumpster enclosure or trash compactor will be installed and meet all requirements of the UDO, and will not be located along the south side of the building to be visible from North Avenue.
- Similar to the site plan, revisions were recommended on the landscape plan. While the UDO requires that automobile parking lots contain landscaped parking lot islands, parking lots that contain truck and trailer parking spaces are not required to have islands. The site plan illustrated some striped areas at the ends of truck and trailer parking rows, but the landscape still illustrated landscaped islands. According to the applicant, Mainfreight prefers to maintain landscaped islands at the ends of truck and trailer parking rows, which staff deems acceptable.
- An outdoor employee patio at the northwest corner of the terminal building was shown on the site plan, but was not shown on the landscape plan. The patio has now been illustrated on the landscape plan.
- As reviewed at the September 11 PC/ZBA meeting, Mainfreight proposes to install a decorative fence along the north and east sides of the property. A variation was requested to allow the fence to extend past the front of the building along North Avenue. Staff was supportive of the variation request, and recommended the fence be extended along the west property line facing Morton Road, north of the full access driveway and up to the north property line, to provide additional screening of the north truck/trailer parking area in conjunction with the proposed landscaping, and provide a more consistent appearance for

the fencing around the property. In addition, the PC/ZBA questioned if the proposed fence needed to extend south of the detention basin on the east side of the property. The applicant has revised the drawings to illustrate the fence to extend along the west side of the property and terminate near the proposed gate system on the north side of the northernmost driveway along Morton Road. In addition, the proposed fence location on the east side of the property has been revised to terminate along the south side of the south pavement area and north of the detention basin. The revised fence location still requires a variation to extend past the front of the building.

PLAT OF CONSOLIDATION

A plat of consolidation has been submitted for review, which consolidates the four existing lots into one lot. In total, the newly established lot will measure approximately 15.76 acres. Staff requested the following revisions to the plat, which have all been addressed with the revised plat.

- The plat title has been revised to “Consolidation” instead of “Subdivision.”
- The DuPage County Recorder’s Office requests that the upper right corner of a plat be left blank so the recording number can be placed on the plat in that area. This area of the plat has been modified.
- A list of applicable taxing bodies has been provided on the plat.
- A dedication of seven feet for Morton Road right of way along the western edge of Lot 1 has been illustrated on the plat.
- The signature page has been updated to remove the following statement at the top: “A Plat of Subdivision shall contain all of the certificates listed above for a Plat of Easement along with the following additional signature certificates.”
- Since Morton Road is currently under Wayne Township’s jurisdiction, a certificate block for Wayne Township Highway Commissioner has been provided.

It should be noted that storm water and utility easements would need to be dedicated. Staff recommends submitting a separate plat of easement after final civil/storm water approval so the exact location of all required utility easements are known.

RECOMMENDATION

Staff has reviewed the proposed requests, and believes the development will provide an attractive new development along the North Avenue Corridor, and has been designed to illustrate safe truck maneuvering in and out of the development. Therefore, staff recommends approval of a map amendment to rezone the property upon annexation, a Special Use Permit for a Motor Vehicle Freight Terminal, a Special Use Permit for Outdoor Vehicle Storage, Loading Dock Variation, Landscaping Variation, Fence Variation, Subdivision Regulations Variation, North Avenue Corridor Review, and a plat of consolidation for North Ave TH LLC/Mainfreight, Case No. 23-0033, subject to the following conditions:

1. That should IDOT either allow a driveway on North Avenue or require that the proposed right-in driveway on Morton Road be shifted substantially further north, a special use amendment will need to be reviewed by the Plan Commission and Village Board;
2. That plans be revised prior to Village Board review to remove curb and gutter and street lights along Morton Road;
3. That all improvements, including but limited to the parking lot, fencing and landscaping, must be built, installed, and maintained in accordance with the attached plans and exhibits;
4. That the landscape materials must be installed as shown on the attached landscape plan, and that all materials shall be maintained in a healthy condition, with dead or dying materials being replaced in accordance with the approved plan on an annual basis;
5. That trucks shall not be allowed to back into the property from Morton Road;
6. That trucks and trailers shall only park in designated parking spaces or within loading docks, and not block access nor be parked or stored in drive aisles or the automobile parking lot along Morton Road;
7. That sleeping in trailers or cabs shall not be allowed;
8. That only trucks and trailers owned and operated by Mainfreight shall be allowed to be stored on the property, and the storage of recreational vehicles, materials, and other equipment not associated with Mainfreight shall not be allowed on the property;
9. That a Knox padlock or gate switch shall be provided on any security gates, and the Carol Stream Fire Protection District shall be provided keys to said gate for access onto the property;
10. That parking lot lighting shall meet requirements of the UDO;
11. That the applicant must obtain the required sign permit prior to the installation of any new signage;
12. That if any ground-mounted or roof mounted mechanical and utility equipment is installed, said equipment shall be screened per requirements of the UDO; and
13. That the site and business must be maintained and operated in accordance with all State, County and Village codes and regulations.



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JUL 24 2023

COMMUNITY DEVELOPMENT DEPT

Do Not Write in This Space	
Date Submitted:	<u>7-24-2023</u>
Fee Submitted:	<u>\$5,980.00</u>
File Number:	<u>23-0033</u>
Meeting Date:	<u>9/11/23</u>
Public Hearing Required:	<u>Y</u>

Village of Carol Stream

500 N. Gary Avenue ▪ Carol Stream, IL 60188
PHONE 630.871.6230 ▪ FAX 630.665.1064

www.carolstream.org

FORM A

GENERAL APPLICATION PUBLIC HEARINGS AND DEVELOPMENT APPROVAL REQUESTS

1. Name of Applicant North Ave TH LLC Phone 630-743-3334
 Address 8770 W. Bryn Mawr Ave, Ste 1350, Chicago IL 60631 Fax _____
 E-Mail Address charmon@timberhillgroup.com
 (required)
 Name of Attorney _____ Phone _____
 (if represented)
 Address _____ Fax _____
 Name of Owner _____ Phone _____
 (required if other than applicant)
 Address _____ Fax _____
 Name of Architect _____ Phone _____
 (if applicable)
 Address _____ Fax _____

2. *Common Address/Location of Property 27W364 North Avenue

3. Requested Action: (check all that apply)

<input checked="" type="checkbox"/> Annexation	<input checked="" type="checkbox"/> Subdivision – Minor/Major
<input type="checkbox"/> Courtesy Review	<input type="checkbox"/> Temporary Waiver/Code of Ordinances
<input type="checkbox"/> Development Staff Review	<input type="checkbox"/> Variation – Zoning (requires Form B-1)
<input checked="" type="checkbox"/> Gary/North Avenue Corridor Review	<input type="checkbox"/> Variation – Sign (requires Form B-2)
<input type="checkbox"/> Planned Development – Final	<input type="checkbox"/> Variation – Fence (requires Form B-3)
<input type="checkbox"/> Planned Development – Preliminary	<input checked="" type="checkbox"/> Special Use Permit (requires Form C)
<input type="checkbox"/> Shared Parking Facility	<input checked="" type="checkbox"/> Map Amendment (requires Form D-1)
<input checked="" type="checkbox"/> Site Plan Review	<input type="checkbox"/> Text Amendment (requires Form D2)
	<input type="checkbox"/> Staff Adjustment (requires Form E)

Describe requested action Applicant is seeking annexation into the Village of Carol Stream, a minor plat of subdivision, a map amendment, a Gary/North Avenue corridor approval, and a special use to develop a 57,100 SF freight terminal and industrial office headquarters facility.

4. Fee Schedule: *(Check all that apply)*

Total Application Fee: \$ _____

Annexation approvals:	
New development	\$2,000
Other	\$800
Annexation Agreement Amendment	\$800
Appearance fee for approvals required by the UDO but not listed herein	\$500
Courtesy Review	\$240
Development Staff Review	\$640
Easement Encroachment	\$300
Gary/North Avenue Corridor Review:	
New or replacement monument sign	\$500
One discipline (site design, architecture or landscaping)	\$500
Two disciplines	\$1,000
Three disciplines	\$1,500
New development	\$1,500
Outdoor Dining Permit	\$120
Planned Development:	
Existing Planned Development, minor change	\$500
Existing Planned Development, major change	\$1,000
New Planned Development	\$1,500
Rezoning (Zoning Map Amendment)	\$640
Shared Parking Facility Permit	\$500
Site Plan Review	\$640
Special Use:	
First	\$800
Each additional	\$200
Special Use Amendment	\$800
Staff Adjustment	\$240
Subdivision:	
Major	\$700
Minor	\$400
Temporary Building, Structure or Use	\$120
Temporary Waiver to the Code of Ordinances	\$120
Text Amendment:	
Building Codes	\$240
Unified Development Ordinance	\$640
Variation:	
First	\$640
Each additional	\$200
Zoning verification letter	\$80

5. Applicant Certification

*I authorize the Village of Carol Stream to install a temporary sign or signs on the property having the common address indicated in Item 2 on this form, for notifying the public of the upcoming public hearing, once the hearing has been scheduled.

I have reviewed a copy of the informational handout(s) for the zoning process(es) for which I am making an application. I am familiar with the code requirements that relate to this application and I certify that this submittal is in conformance with such code(s).

I understand that incomplete or substandard submittals may increase the staff review time and delay scheduling of the public hearing. I also understand that, per § 6-13-6 of the Municipal Code, the Village's costs of legal reviews, structural engineering review, and other special reviews determined to be necessary by the Community Development Director, performed by means of consultant services, shall be paid at the billed rate to the Village by the applicant.

Connor Harmon

Print Name

Signature

7/12/23

Date

Revised 11/22



FORM B-1

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DEPT

Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188
630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org ▪ website: www.carolstream.org

GENERAL VARIATIONS

Please see Section 16-8-4(K) of the Unified Development Ordinance (UDO) to learn more about Variations.

In accordance with the applicable statutes of the State of Illinois, no variation shall be made by the Village Board except after a Public Hearing is held before the Zoning Board of Appeals.

Both the Zoning Board of Appeals and Village Board must decide if the requested variation is in harmony with the general purpose and intent of the Unified Development Ordinance (UDO) and if there is a practical difficulty of hardship in carrying out the strict letter of the regulations of the UDO.

The Zoning Board of Appeals shall make findings based upon evidence presented on the following conditions: (Please respond to each of these standards in writing below as it relates to your request.)

1. The requested Variation arises from conditions that are unique to the subject property, that are not ordinarily found in the same zoning district and that are not a result of the owner's intentional actions.
 The property is oriented in a way that prohibits our building and parking lot from fully

 functioning and operating at a north/south building design. The site flows significantly better

 and trucks are able to maneuver around the site at code standards given an east/west

 orientation. As such, we also request to waive the foundation landscape requirements along North

 Ave

2. That the Variation to be granted will not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property.
 The project is a significant improvement to what currently exists on site, and will improve the

 character of the existing property and surrounding neighborhood. In the areas we cannot plant

 foundation landscaping, we propose to install the landscaping elsewhere on the site, further

 beautifying the property.

3. That the conditions of a Variation will constitute of an unnecessary physical hardship (not economic hardship), in which the property cannot be used for an otherwise allowed use without coming into conflict with applicable site development standards.
The shape of the property is not suitable for the type of building Mainfreight that is needed
_____ for their operation. Various landscape screening measures will help block view of the building
_____ from the North Avenue right-of-way.

4. The Variation is the minimum action necessary to alleviate the hardship and observes the spirit of this UDO
Both the service area variation and foundation landscape variation are both required to
_____ create a functioning site for Mainfreight, which is not otherwise possible with the service areas
_____ facing Morton Road. Further, the architectural detail of the building and reduction in trip
_____ generation are both significant improvements to what currently exists.

5. The Variation desired will not adversely affect the public health, safety, or general welfare or impair the purposes or intent of this UDO or the Comprehensive Plan.
Both variation requests will not affect the public health, safety, or general welfare or impair
_____ the purposes or intent of the UDO or comprehensive plan. In fact, our proposal accomplishes
_____ several goals in the comprehensive plan, and improves the property significantly from a
_____ public health, safety, and general welfare perspective.

6. Other pertinent information or reason for the request.



FORM B-3

Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188

630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org • website: www.carolstream.org

FENCE VARIATION

Please see Section 16-8-4(K) of the Unified Development Ordinance (UDO) to learn more about Fence Variations.

In accordance with the applicable statutes of the State of Illinois, no variation shall be made by the Village Board except after a Public Hearing is held before the Zoning Board of Appeals.

Both the Zoning Board of Appeals and Village Board must decide if the requested variation is in harmony with the general purpose and intent of the UDO and if there is a practical difficulty of hardship in carrying out the strict letter of the regulations of the UDO.

The Zoning Board of Appeals may vary the provisions of the Fence Code after making findings based upon the evidence presented with regards to the following conditions: (Please respond to each of the following as it relates to your request.)

1. An exceptional situation related to topography, surroundings or conditions of a specific piece of property, or by reason of exceptional narrowness or shallowness.

Because of the orientation of the building, where the front entrance of the building is off
_____ of Morton Road, a fence is needed to enclose the parking lot for security purposes.

While this is the side yard of the building, from a setback perspective, it is the front yard

_____ by code. Thus, variation is needed to install a fence past the North Ave facing elevation of the building.

2. Difficulties or particular hardship in the way of carrying out the strict letter of the Fence Standards.

The orientation of the building creates a fence related hardship by requiring to fence in the southern

_____ area of the development from a security perspective. The property does not function from a north/south building orientation. With an east/west orientation, the parking lot and access

_____ areas operate to Mainfreights standard. Because there are goods being stored on site (for short durations), site security is a priority.

3. The fence will not impair an adequate supply of light and air to adjacent property.

The fence will be wrought iron and decorative in nature. The fence will be setback far

_____ enough from the building and property line that light and air will not be issues from an installation and placement perspective.

4. The fence will not endanger the public safety.

The proposed fence will not endanger the public safety, rather it will enhance the safety of the property, Mainfreight employees, and deter would be criminals from committing theft related crimes.

5. The fence will not unreasonably diminish or impair established property values within the surrounding area.

The fence will be a high quality, decorative addition to the property, and further beautify the aesthetic of the development. The fence will also help screen trailers from view of North Avenue.

6. The fence will not impair the public health, safety, comfort, morals or welfare of the inhabitants of the village.

The proposed fence will not impair the public health, safety, comfort, morals or welfare of Village residents, rather it is meant as a quality addition to the development, and will only add to the wellbeing of Mainfreight employees, and success of the development.

7. Other pertinent information or reason for the request.

JUL 24 2023

COMMUNITY DEVELOPMENT
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FORM C

Village of Carol Stream

500 N. Gary Avenue • Carol Stream, IL 60188
630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org ■ website: www.carolstream.org

SPECIAL USE APPLICATION

Uses designated under the various zoning districts herein as special uses are so classified because they may have site-specific impacts that require the discretionary review of the Plan Commission and Village Board. Each special use request must be reviewed based on its unique character, with consideration being given to the proposals impact upon neighboring properties. (Please address each of the following standards as it relates to your request.)

No special use shall be recommended by the Plan Commission nor approved by the Village Board, unless the special use:

1. Is deemed necessary for public convenience at the location.
The use would significantly improve existing conditions and public convenience at _____
the subject property by reducing the amount of truck traffic.
2. Will not be unreasonably detrimental to or endanger the public health, safety, morals, comfort or general welfare.
The proposal will not be to the detriment or endanger the public health, safety, morals, _____
comfort, or general welfare of the community or surrounding properties.
3. Will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.
The proposal will improve traffic flow, stormwater management, and air quality of _____
the immediate area.
4. Will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.
The proposal will not impede or disrupt orderly development surrounding the subject _____
property, rather it will improve the area overall.

5. Will provide adequate utilities, access roads, drainage and other important and necessary community facilities.

The proposal will abide by and meet all requirements and regulations as it pertains to
utilities, access roads, drainage, and all other codes and ordinances.

6. Will conform to the applicable regulations of the district in which it is located, except as such regulations may in each instance be modified by the Village Board.

The Petitioner is committed to meeting all regulations and requirements set forth by the
Village of Carol Stream.

7. Other pertinent information or reason for request.



FORM D-1

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JUL 24 2023

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Village of Carol Stream

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630.871.6230 • FAX 630.665.1064

e-mail: communitydevelopment@carolstream.org ■ website: www.carolstream.org

MAP AMENDMENT

Please see Section 16-8-4(N) of the Unified Development Ordinance (UDO) to learn more about Map Amendments.

In accordance with the applicable statutes of the State of Illinois, an amendment shall be granted or denied by the Village Board only after a public hearing before the Plan Commission and a report of its findings and recommendations has thereafter been submitted to the Village Board.

The decision to amend the Official Zoning Map is a matter of legislative discretion that is not controlled by any single review criterion. In making recommendations and decisions on Map Amendments, the Plan Commission and Village Board must consider all relevant factors, including at minimum the following criteria: (Please respond to each of these standards in writing below as it relates to your request.)

1. The existing uses and zoning of nearby property.
All adjacent property is in unincorporated DuPage County besides the property to the
south, which is zoned B-3 General Business District. Surrounding uses include a place of
worship, single-family residential, an office complex, a restaurant, and several light
industrial uses.

2. The extent to which property values are diminished by the current zoning classification.
The existing property is currently in unincorporated DuPage County. It consists of
no stormwater infrastructure, and is a gravel surface lot which emits dust into the
air to adjacent properties. The use generates significant truck traffic daily, which
will be greatly reduced through the proposed development.

3. The extent to which the diminishment of property value of the applicant promotes the health, safety, morals or general welfare of the public.
Removing the existing use would increase safety by decreasing truck traffic, improve the air
quality, and improve stormwater management all in the immediate area.

4. The relative gain to the public as opposed to the hardship imposed upon the individual property owner.
The property is in unincorporated DuPage County. We (Petitioner) are requesting an
industrial zoning classification to improve the property, tie into Village utilities, and remove
an existing non conforming use (by Village standards) currently in the County.

5. The suitability of the subject property for the zoned purposes.
The property will fit into the into the industrial zoning category. If approved for a special
use, the property will be suitable and fit within the zoning and land use ordinance set
forth by the Village of Carol Stream.

6. The length of time the property has been vacant as zoned considered in the context of land development in the area.
The property has always existed in the County, and has been operated as a truck parking
and storage facility for over 30 years.

7. Community need for the use proposed by the applicant.
The economic development benefits by the proposed development are significant. The
development would generate 60 high quality jobs, would locate a North American
corporate HQ into Carol Stream, would significantly increase the EAV on the property
which equals additional tax revenue to the Village, and would clean up a non-
conforming use.
8. Other pertinent information or reason for the request.
Mainfreight is interested and will be installing several energy efficient designs/systems
into their development, including but not limited to; solar panels, greywater collection,
and rainwater harvesting.

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AUG 27 2023

COMMUNITY DEVELOPMENT
DEPT



Village of Carol Stream
Plan Commission, Zoning Board of Appeals, Village Board
500 N. Gary Avenue
Carol Stream, IL 60188

RE: 27W364 North Avenue Cover Letter

Dear PC, ZBA, and Village Board,

North Ave TH LLC represents the property entity under the umbrella company Timber Hill Group. Timber Hill Group is a private equity firm specializing in the acquisition and development of logistics related industrial real estate throughout the U.S. The company is based in Chicago, and we are incredibly excited to propose a high-quality, class A development in our backyard.

We recently purchased the subject property and propose to demo the existing gravel parking lot and associated smaller structures near North Avenue to construct a 57,100 (47,100 SF footprint) square foot office and truck terminal structure with associated auto and truck/trailer parking. The development is a build-to-suit project with Mainfreight Inc, who is a New Zealand based freight carrier which focuses on transport, air & ocean, and warehousing of goods throughout the world. The company has a footprint and operations in over 20 countries and has been operating for over 40 years. Mainfreight intends on moving its North American corporate headquarters from Los Angeles to the subject property, along with the creation of 60 high quality jobs to the office component of the building.

The development would consist of 92 auto stalls, 44 tractor stalls, 86 trailer stalls, and 81 dock positions. The parking lot is proposed to be setback at least 60' along all residential property lines, including a 189' setback directly to the north. The parking lot also consists of a 21.1' setback to the east, and a 40' setback to the west. The office portion of the building consists of a 153' setback from the southern property line, while the terminal portion consists of a 265' setback from the southern property line.

In terms of screening, the property will have a 60' natural/forested landscape buffer along the residential property lines, and a 6' opaque fence. The property will be fully lit, however light levels along all property lines will measure 0.0 footcandles. Mainfreight and Timber Hill will ensure no glare spills over into adjacent properties.

Mainfreight is also committed to reducing its impact on the environment while promoting several sustainable initiatives. As such, all upcoming design build projects for Mainfreight include use of solar panels, greywater collection, and EV charging stations. Mainfreight intends on installing roof mounted solar panels along the entire terminal portion of the building, a greywater system which will consist of a holding tank and be able to reuse water onsite for potable use, and several electric vehicle charging stations for EV automobiles. From a regulation perspective, we will meet all zoning regulations and further requirements by the Village during the permitting and final design stages.

From an operations standpoint, Mainfreight will operate 7AM-5PM. The site will typically see 14 in/out truck movements per day from local pickup and delivery. There are additional network line haul trailers that go cross-country that will add approximately 10 in/out a day to the operation. When not in use, many of the trailers will be stored onsite. Trucks will utilize both access points and will not travel northbound onto Morton Road. There will also be no refrigerated trailer or fueling on-site.

We engaged KLOA to conduct a traffic study at the site, and findings indicated that North Avenue and other arterial and collector roadways consist of sufficient capacity to handle the proposed development. In fact, the development will significantly improve the circulation and trip generation of the existing use on-site. Our preference is for IDOT to allow us to install a signal at North Avenue and Morton Road, however the traffic generation by the proposal is not large enough to warrant a signal. We are confident the development will improve existing traffic movements and congestion in the immediate area and will be a net positive to the community.

From an access perspective, we had several meetings with Village of Carol Stream staff to determine the best way to accommodate the southern access to the site. Without formal comment from IDOT, we determined with Mainfreight input, that the right-in off Morton Road represented the best and safest circulation pattern for the site. We are confident with Mainfreight as a single tenant user and all truck drivers as Mainfreight employees, that there will be no illegal exits utilizing the right in off Morton Road to North Avenue. Mainfreight is committed to operating the site with safety at the forefront of any operations. We are also confident that with the relatively low usage of the site and significantly less trips than what previously existed, that no queuing will take place from North Avenue onto the southern access on Morton Road. Once IDOT issues guidance on the best path forward toward southern access, we will adjust any plans accordingly. Preliminary indications with IDOT show that they will not approve any access onto North Avenue. Further, we are confident any changes to access will not substantially impact the site plan or civil design, and that we can accommodate such changes during permit review.

In this resubmittal, we have submitted responses to the review letter, updated all plans accordingly, and submitted a variation application. Please let me know if you have any questions or need additional information.

Sincerely,

Connor Harmon, Associate Director of Development
Timber Hill Group LLC

Certificate of the Publisher

Examiner Publications, Inc. certifies that it is the publisher of The Examiner of Carol Stream. The Examiner of Carol Stream is a secular newspaper, has been continuously published weekly for more than fifty (50) weeks prior to the first publication of the attached notice, is published in the Village of Carol Stream, township of Bloomingdale, County of DuPage, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 ILCS 5/5.

A notice, a true copy of which is attached, was published 1 times in The Examiner of Carol Stream, namely one time per week for 1 successive weeks. The first publication of the notice was made in the newspaper, dated and published on September 6, 2023 and the last publication of the notice was made in the newspaper dated and published on September 6, 2023. This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

In witness, Examiner Publications, Inc. has signed this certificate by Randall Petrik, its publisher, at The Village of Carol Stream, Illinois, on September 6, 2023.

Examiner Publications, Inc.

By: Publisher



Randall E. Petrik, Publisher, Examiner Publications, Inc.

Subscribed and sworn to before me this 6 day of September, A.D. 2023.

Notary Public



LEGAL NOTICE PUBLIC NOTICE FILE #23-0033

Notice is hereby given that the Carol Stream Plan Commission/Zoning Board of Appeals will hold a Public Hearing at the Carol Stream Gregory J. Bielawski Municipal Center, 500 N. Gary Avenue, Carol Stream, Illinois, on Monday, September 25, 2023 at 6:00 p.m. to consider an application from North Avenue TH LLC/Mainfreight for the following actions:

A Special Use Permit for Outdoor Vehicle Storage, in accordance with Section 16-3-11 (E) of the Carol Stream Unified Development Ordinance.

Variations from subdivision regulations, in accordance with Article 9 of the Carol Stream Unified Development Ordinance.

For the property located at 27W364 North Avenue (PINs 01-36-200-017, 01-36-200-040, 01-36-200-006, and 01-36-200-005)

The hearing will be held in-person and via a web conference meeting. The web conference will allow the public to view the meeting online or listen to the meeting.

Join from a PC, Mac, iPad, iPhone or Android device by using the following URL:

<https://us02web.zoom.us/j/81683339884>

Or join by phone: Dial (for higher quality, dial a number based on your current location):

+1 312 626 6799 or +1 301 715 8592 or +1 646 558 8656

Webinar ID: 816 8333 9884

International numbers available: <https://zoom.us/j/81683339884>

Those members of the public wishing to be heard with respect to this matter must attend in-person, or complete the public comment form at <http://carolstre.am/pc> by 4:00 p.m. the day of the public hearing. The public may also provide written comments prior to the public hearing by submitting them to Tom Farace, AICP, Planning & Economic Development Manager, 500 N. Gary Ave., Carol Stream, IL 60188, or may provide email comments by sending them to tfarace@carolstream.org. Comments must be provided by 4:00 p.m. the day of the public hearing. The application is available for public inspection on the Village's website at www.carolstream.org, and under the "Development Projects" button. Individuals with disabilities who plan to attend the hearing and who require certain accommodations in order to allow them to observe and participate, or who have questions regarding the accessibility of the meeting, are requested to contact the ADA Coordinator at 630-871-8250.

As published in *The Examiner* Sept. 6, 2023. 0906

PROPOSED IMPROVEMENTS FOR MAINFREIGHT TRUCKING TERMINAL

NORTH AVENUE & MORTON ROAD, CAROL STREAM, ILLINOIS

RECEIVED
SEP 19 2023
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DEPT

LOCATION MAP

Sheet List Table	
Sheet Number	Sheet Title
C0.0	Site Location Map & Civil Legend
C0.1	General Notes & Specifications
C1.0	Existing Conditions & Demolition Plan
C2.0	Overall Site Layout Plan
C2.1	Detailed Site Layout Plan
C3.0	Site Grading Plan
C4.0	Soil Erosion & Sediment Control Plan
C4.1	Soil Erosion & Sediment Control Details
C5.0	Site Utility Plan
C6.0	Construction Details
C6.1	Construction Details
C6.2	Construction Details
C6.3	Construction Details
C6.4	Construction Details
C6.5	Construction Details



SECTION 36, TOWNSHIP 40 N, RANGE 36 E

EXISTING LEGEND	PROPOSED LEGEND
EXISTING TREE	CURB & GUTTER
CURB & GUTTER	REVERSE PITCH CURB & GUTTER
EXISTING BUILDING	DEPRESSED CURB & GUTTER
PCC SIDEWALK	PROPOSED BUILDING
GAS SERVICE	PCC SIDEWALK
ELECTRIC SERVICE	STANDARD DUTY PAVEMENT
STORM SEWER	HEAVY DUTY PAVEMENT
SANITARY SEWER	GAS SERVICE
WATER MAIN	ELECTRIC SERVICE
CABLE LINE	STORM SEWER
OVERHEAD UTILITY LINE	SANITARY SEWER
COMMUNICATION LINE	WATER MAIN
FIBER OPTIC LINE	FENCE (DECORATED)
FENCE	FENCE (CHAINLINK/OPAQUE)
STORM STRUCTURE	STORM STRUCTURE
SANITARY MANHOLE	DOWNSPOUT CONNECTION
CLEANOUT	SANITARY MANHOLE
WATER METER	CLEANOUT
VALVE VAULT	WATER METER
VALVE BOX	VALVE VAULT
HYDRANT	VALVE BOX
GAS METER	HYDRANT
ELECTRIC METER	GAS METER
PARKING LOT LIGHT	ELECTRIC METER
UTILITY POLE	PARKING LOT LIGHT
GUY WIRE	FLOW ARROW
TRANSFORMER	OVERLAND FLOOD ROUTE
FIBER OPTIC BOX	TOP OF DRAINAGE GRADE
FIBER OPTIC PEDESTAL	PAVEMENT GRADE
CABLE PEDESTAL	GROUND GRADE
PHONE PEDESTAL	MAJOR CONTOUR
ELECTRIC PEDESTAL	MINOR CONTOUR
MAJOR CONTOUR	
MINOR CONTOUR	

SURFACE WATER DRAINAGE STATEMENT

TO THE BEST OF OUR KNOWLEDGE AND BELIEF, THE DRAINAGE OF THE SURFACE WATERS WILL NOT BE CHANGED BY THE CONSTRUCTION OF THIS PROJECT OR ANY PART THEREOF, OR, THAT IF DRAINAGE WILL BE CHANGED, REASONABLE PROVISION HAS BEEN MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS INTO PUBLIC AREAS, OR DRAINS APPROVED FOR USE BY THE DIRECTOR OF PUBLIC WORKS AND ENGINEERING, AND THE SUCH SURFACE WATERS ARE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGE TO ADJOINING PROPERTIES BECAUSE OF THE CONSTRUCTION OF THIS PROJECT.

07/21/2023
DATE

ENGINEER

2200 CAROL DRIVE
SUITE 420
E. CAROL, IL 60510
P. 630.598.0000
WWW.CAGEVIL.COM

REVISIONS

NO.	DESCRIPTION	DATE

THOLOGISTICS FUND I/LP
MAINFREIGHT TRUCKING TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS

PROJECT: 220302
ENGR: KH/MCK/JGN
DATE: 07.28.2023

SHEET TITLE
SITE LOCATION MAP & CIVIL LEGEND

SHEET NUMBER
C0.0
1 OF 15

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WM VV
RIM = 781.35
TP = 775.3
(TO REMAIN)

EXISTING CHURCH ENTRANCE

WM VV
RIM = 776.30
TP = 770.0
(TO REMAIN)

EX. STM MANHOLE
RIM=774.65
INV=770.35 N
INV=770.15 W
(TO REMAIN)

15" RCP
(TO REMAIN)

15" FES
INV = 766.95
(TBR)

15" FES
INV = 767.62
(TO REMAIN)

C&G REMOVAL

18" FES
INV = 765.50
(TBR)

STM DRAIN
RIM = 765.74
INV = 764.19 S
(TBR)

STM DRAIN
RIM = 765.74
INV = 764.19 S
(TBR)

EX. WATERMAIN
(TO REMAIN)

EX. CHAIN LINK FENCE
(TBR)

C&G REMOVAL

EX. SAN MH
RIM = 772.61
INV = 766.40
(TBR)

EX. SAN MH
RIM = 771.05
INV = 756.90
(TO REMAIN)

EX. WATERMAIN
(TO REMAIN)

12" RCP FES
INV = 763.36
(TO REMAIN)

12" RCP FES
INV = 761.37
(TO REMAIN)

EX. SAN MH
RIM = 761.15
INV = 746.70
(TO REMAIN)

EX. STM CATCH BASIN
INV = 764.64
INV = 761.53
(TO REMAIN)

EX. WALL
(TBR)

WM VV
RIM = 781.35
TP = 755.3
(TO REMAIN)

EX. HYDRANT
(TO REMAIN)

EX. WM VV
(TO REMAIN)

EX. 10" SANITARY
(TO REMAIN)

HIGHWAY ROUTE 64 (NORTH AVENUE)

12" HDPE
T/P = 756.48
INV = 755.5
(TBR)

EX. SURFACE WATER
±0.007 ACRES
(TO BE PARTIALLY IMPACTED)

12" RCP FES
INV = 754.18
(TO REMAIN)

EX. 30" RCP FES
INV = 752.54 S (EAST)
INV = 752.53 S (MID)
INV = 752.60 S (WEST)
(TO REMAIN)

EX. STM DRAIN
INV = 752.69 E (NORTH)
INV = 752.86 E (SOUTH)
(TO REMAIN)

DEMOLITION LEGEND

	BUILDING DEMOLITION
	ASPHALT/GRAVEL PAVEMENT REMOVAL, FULL DEPTH
	CONCRETE PAVEMENT / SIDEWALK REMOVAL, FULL DEPTH
	SAWCUT PAVEMENT, FULL DEPTH
	CURB & GUTTER REMOVAL
	UTILITY REMOVAL
	TREE/BUSH REMOVAL
	UTILITY STRUCTURE REMOVAL
(TBR)	TO BE REMOVED

SOURCE BENCHMARK:
"TRIMBLE R10 VRS NETWORK"
NAVD88

SITE BENCHMARK

1. IRON PIPE WITH CAP ALONG THE WEST EDGE OF PAVEMENT OF MORTON ROAD, 3 FEET NORTH OF FIRE HYDRANT, SAID IRON WITH CAP IS OPPOSITE THE DRIVEWAY AT 2578 MORTON ROAD ALONG THE NORTH LINE OF THE SUBJECT SITE. (ELE = 782.75)

2. CROSS CUT IN CURBLINE AT THE NORTHWEST CORNER OF NORTH (IL ROUTE 64) AVENUE AND MORTON ROAD, AT THE CORNER OF WHEATON BIBLE CHURCH, SAID CROSS IS 42 FEET SOUTHWEST OF WESTERLY OF 2 MANHOLES IN THE SOUTHBOUND LANE OF MORTON ROAD AND 104 FEET NORTHWEST OF THE CENTERLINE OF NORTH AVENUE AND MORTON ROAD (ELE = 780.87)

3. CROSS CUT IN CONCRETE ALONG THE NORTH EDGE OF NORTH AVENUE ROADWAY, SAID CROSS IS 1.5 FEET SOUTH OF SAID NORTH EDGE, OPPOSITE EASTERLY OF 3 30 IN FLARED END SECTIONS ALONG NORTH RIGHT-OF-WAY AND 166 FEET WEST OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF LOT 33. (ELE = 757.01)

2200 CABOT DRIVE
SUITE 325
LISLE, IL 60532
P: 630.595.0037
WWW.CAGECIVIL.COM



REVISIONS

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TH LOGISTICS FUND LLP
MAINFREIGHT TRUCKING
TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS



JULIE
CALL BEFORE
YOU DIG
811

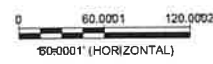


EXHIBIT B

PROJECT: 220302

DESIGNER: KHM/KHJ/KGN

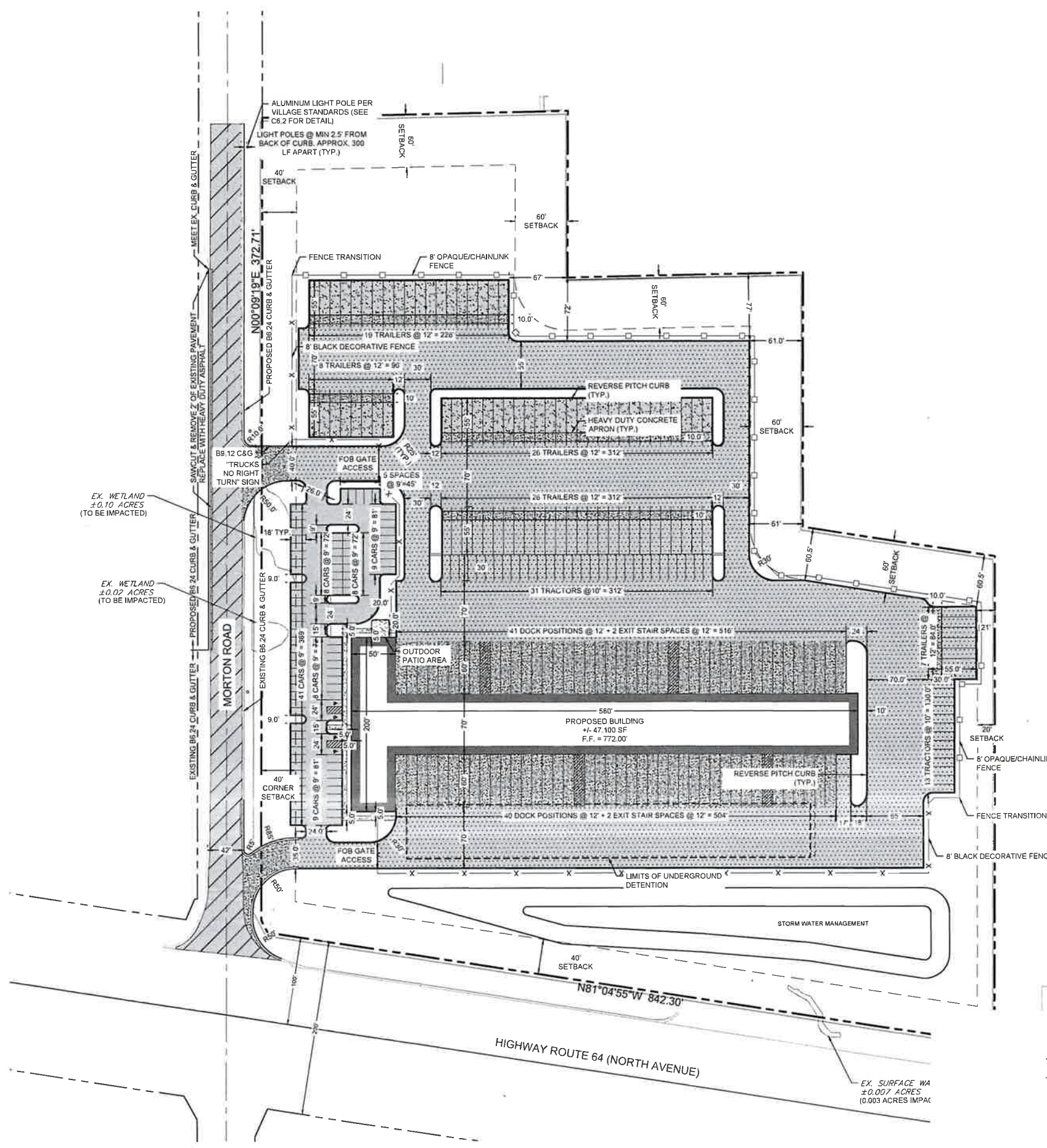
DATE: 07.28.2023

SHEET TITLE: EXISTING CONDITIONS & DEMOLITION PLAN

SHEET NUMBER: C1.0

3 OF 15

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 DEPT



PAVEMENT LEGEND

	STANDARD DUTY BITUMINOUS PAVEMENT
	HEAVY DUTY BITUMINOUS PAVEMENT
	HEAVY DUTY CONCRETE PAVEMENT
	2" MILL AND OVERLAY
	PCC SIDEWALK
	8" BLACK DECORATIVE FENCE
	8" OPAQUE CHAINLINK FENCE

SITE DATA TABLE:

BUILDING REQUIREMENTS

EXISTING ZONING CLASSIFICATION DUPAGE COUNTY R2-SINGLE FAMILY

PROPOSED ZONING CLASSIFICATION CAROL STREAM I-INDUSTRIAL DISTRICT

SITE AREA = 686,580 SF = 15.76 ACRES
 NET OFFICE AREA = 10,000 SF
 NET TERMINAL AREA = 37,100 SF
 TOTAL BUILDING AREA = 47,100 SF

BUILDING SETBACKS

FRONT	60 FT
SIDE	10 FT
REAR	20 FT

PARKING REQUIREMENTS:

I - INDUSTRIAL DISTRICT
 OFFICE PARKING = ONE (1) SPACE FOR EACH 1,500 SQUARE FEET OF FREIGHT TERMINAL NET FLOOR AREA (NFA) AND ONE (1) SPACE FOR EACH 250 SQUARE FEET OF OFFICE NET FLOOR AREA (NFA)

REQUIRED PARKING = 90 SPACES
 TOTAL PARKING PROVIDED = 88 SPACES
 ADA PARKING = 4 SPACES

SHEET NOTES:
 ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE TO THE VILLAGE OF CAROL STREAM'S REQUIREMENTS. SECTION 16-5-2 (C) "ALL PAVEMENT STRIPING SHALL BE FOUR (4) INCH HAIR-PIN LOOPELINES, 16 INCHES ON CENTER"

A FUTURE TRASH ENCLOSURE/TRASH COMPACTOR WILL BE ADDED TO THE SITE WHICH WILL MEET ALL UDO GUIDELINES. SHOULD A TRASH COMPACTOR BE INSTALLED, IT WILL BE PLACED ON THE NORTHERN SIDE OF THE TERMINAL.

GATE ACCESS LOCATIONS SHALL HAVE KNOX BOX FOR EMERGENCY VEHICLES.

SOURCE BENCHMARK:
 TRIMBLE R10 VRS NETWORK
 NAVD88

SITE BENCHMARK
 1. IRON PIPE WITH CAP ALONG THE WEST EDGE OF PAVEMENT OF MORTON ROAD, 3 FEET NORTH OF FIRE HYDRANT. SAID IRON WITH CAP IS OPPOSITE THE DRIVEWAY AT 28579 MORTON ROAD ALONGS THE NORTH LINE OF THE SUBJECT SITE. (ELE = 782.75)

2. CROSS CUT IN CURBLINE AT THE NORTHWEST CORNER OF NORTH (IL ROUTE 64) AVENUE AND MORTON ROAD, AT THE CORNER OF WHEATON BIBLE CHURCH. SAID CROSS IS 42 FEET SOUTHWEST OF WESTERLY OF 2 MANHOLES IN THE SOUTHBOUND LANE OF MORTON ROAD AND 104 FEET NORTH-WEST OF THE CENTERLINE OF NORTH AVENUE AND MORTON ROAD. (ELE = 780.87)

3. CROSS CUT IN CONCRETE ALONG THE NORTH EDGE OF NORTH AVENUE ROADWAY. SAID CROSS IS 1.5 FEET SOUTH OF SAID NORTH EDGE, OPPOSITE EASTERLY OF 3.30 IN FLARED END SECTIONS ALONG NORTH RIGHT-OF-WAY AND 166 FEET WEST OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF LOT 33. (ELE = 757.01)

2200 CAROL DRIVE
 SUITE 325
 Lisle, IL 60532
 P: 630.398.0077
 WWW.CAGECIVIL.COM



REVISIONS

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TH LOGISTICS FUND I LP
MAIN FREIGHT TRUCKING TERMINAL
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, ILLINOIS

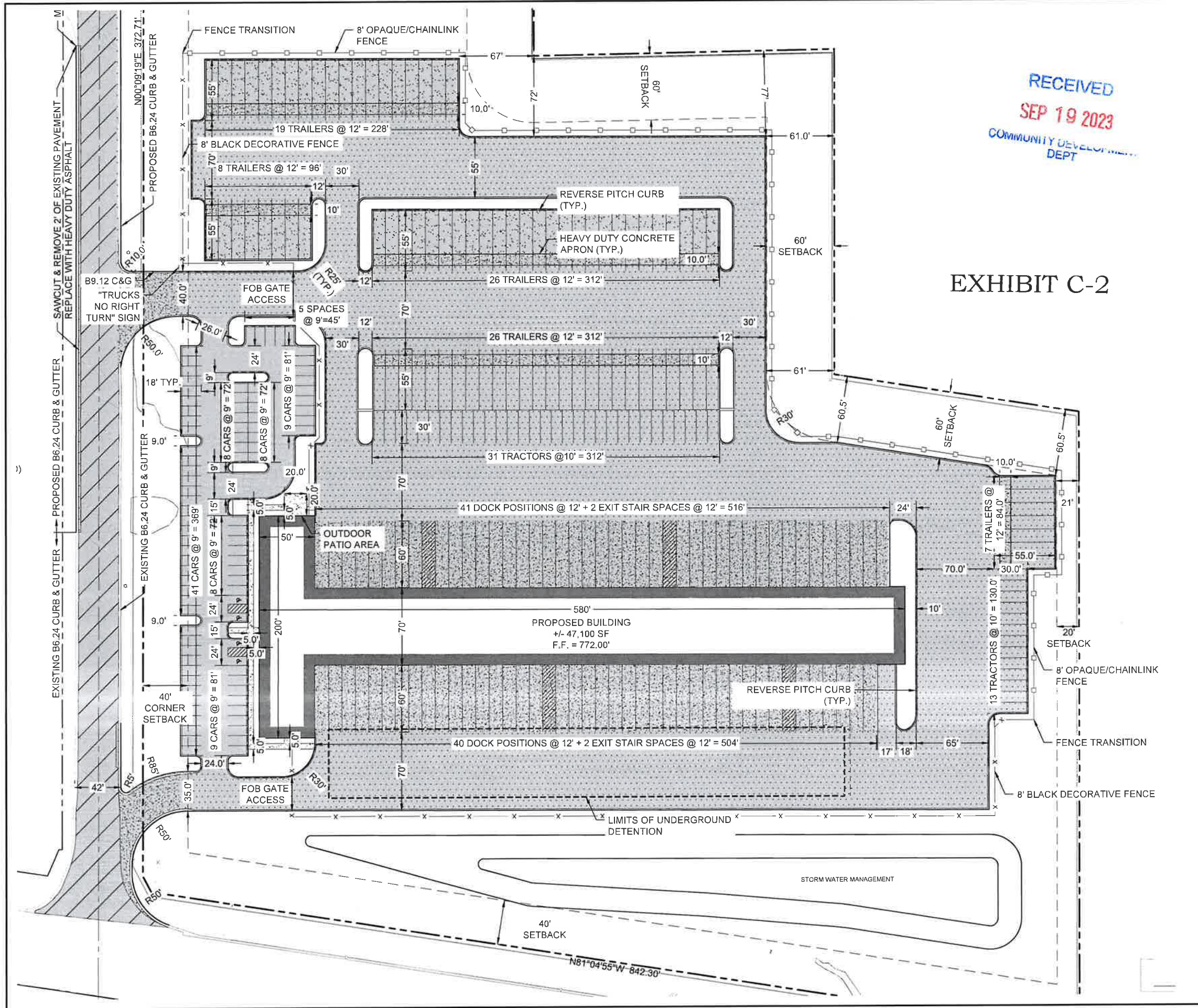
811
 JULIE CALL BEFORE YOU DIG
 811
 PROJECT: 220302
 EING: KHM/MCK/JGN
 DATE: 07.28.2023

OVERALL SITE LAYOUT PLAN

0 60.0001 120.0002
 80:0001' (HORIZONTAL)

C2.0
 4 OF 15

EXHIBIT C-1



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 COMMUNITY DEVELOPMENT
 DEPT

EXHIBIT C-2

PAVEMENT LEGEND

- STANDARD DUTY BITUMINOUS PAVEMENT
- HEAVY DUTY BITUMINOUS PAVEMENT
- HEAVY DUTY CONCRETE PAVEMENT
- 2" MILL AND OVERLAY
- PCC SIDEWALK
- 8" BLACK DECORATIVE FENCE
- 8" OPAQUE CHAINLINK FENCE

SITE DATA TABLE:

BUILDING REQUIREMENTS

EXISTING ZONING CLASSIFICATION DUPAGE COUNTY R2- SINGLE FAMILY

PROPOSED ZONING CLASSIFICATION CAROL STREAM I- INDUSTRIAL DISTRICT

SITE AREA = 686,580 SF = 15.76 ACRES
 NET OFFICE AREA = 10,000 SF
 NET TERMINAL AREA = 37,100 SF
 TOTAL BUILDING AREA = 47,100 SF

BUILDING SETBACKS

FRONT	60 FT
SIDE	10 FT
REAR	20 FT

PARKING REQUIREMENTS:

I - INDUSTRIAL DISTRICT
 OFFICE PARKING = ONE (1) SPACE FOR EACH 1,500 SPACE FEET OF FREIGHT TERMINAL NET FLOOR AREA (NFA) AND ONE (1) SPACE FOR EACH 250 SQUARE FEET OF OFFICE NET FLOOR AREA (NFA)

REQUIRED PARKING = 90 SPACES
 TOTAL PARKING PROVIDED = 88 SPACES
 ADA PARKING = 4 SPACES

SHEET NOTES:
 ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE TO THE VILLAGE OF CAROL STREAM'S REQUIREMENTS. SECTION 16-5-2 (C). ALL PAVEMENT STRIPING SHALL BE FOUR (4) INCH HAIR-PIN LOOPED LINES, 16 INCHES ON CENTER.

A FUTURE TRASH ENCLOSURE/TRASH COMPACTOR WILL BE ADDED TO THE SITE WHICH WILL MEET ALL LDDO GUIDELINES. SHOULD A TRASH COMPACTOR BE INSTALLED, IT WILL BE PLACED ON THE NORTHERN SIDE OF THE TERMINAL.

GATE ACCESS LOCATIONS SHALL HAVE KNOX BOX FOR EMERGENCY VEHICLES.

SOURCE BENCHMARK:
 TRIMBLE R10 VRS NETWORK* NAVD88

SITE BENCHMARK
 1. IRON PIPE WITH CAP ALONG THE WEST EDGE OF PAVEMENT OF MORTON ROAD, 3 FEET NORTH OF FIRE HYDRANT. SAID IRON WITH CAP IS OPPOSITE THE DRIVEWAY AT 2579 MORTON ROAD ALONG THE NORTH LINE OF THE SUBJECT SITE. (ELE = 782.75)

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2200 CAROL DRIVE
 SUITE 325
 LISLE, IL 60558
 P: 630.598.0007
 WWW.CAGECIVIL.COM



REVISIONS

NO.	DESCRIPTION

TH LOGISTICS FUND LLP
MAIN FREIGHT TRUCKING TERMINAL
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, ILLINOIS

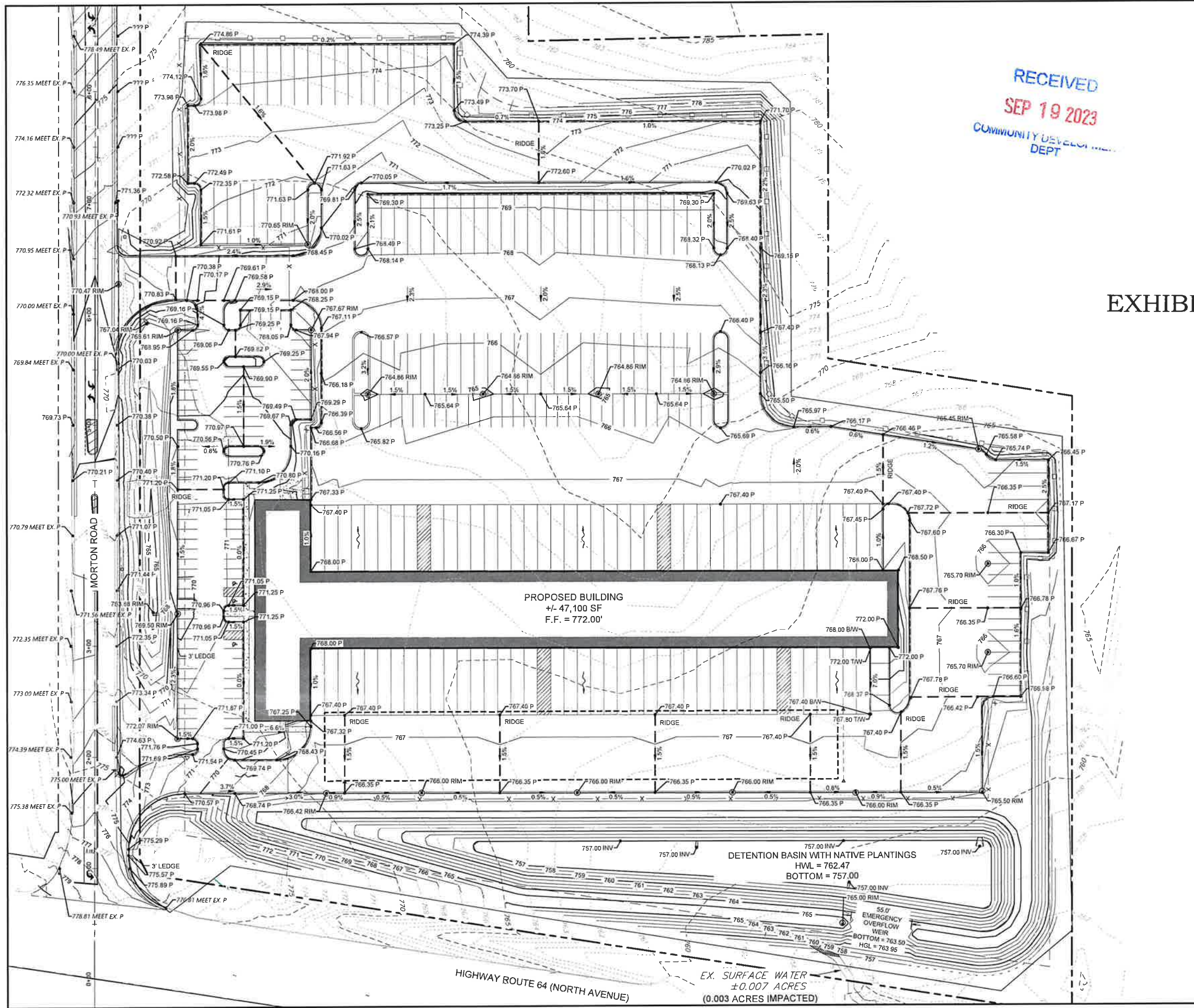
811 JULIE CALL BEFORE YOU DIG 811

PROJECT NO. 220302
 BY: KH/MCK/JGN
 DATE: 07.28.2023

SHEET TITLE
DETAILED SITE LAYOUT PLAN

SCALE: 1" = 40'-0" (HORIZONTAL)

C2.1
 5 OF 15



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COMMUNITY DEVELOPMENT DEPT

EXHIBIT D-1

- GRADING LEGEND**
- 1.0% DRAINAGE ARROW
 - OVERLAND FLOOD ROUTE
 - 100.00 P SPOT ELEVATION
 - PROPOSED MAJOR CONTOUR
 - PROPOSED MINOR CONTOUR
 - EXISTING MAJOR CONTOUR
 - EXISTING MINOR CONTOUR

- SHEET NOTES**
- STORM SEWER IS DESIGNED FOR THE 100-YEAR EVENT.
 - DETENTION BASIN SHALL BE PLANTED WITH NATIVE PLANTINGS IN ACCORDANCE WITH THE DUFAGE COUNTY STORMWATER ORDINANCE.

22003 - 4807 DRIVE
SUITE 325
LAKE ILWACO, OR 97132
P: 503-598-0007
WWW.CAGECIVIL.COM



REVISIONS

NO.	DESCRIPTION

TH LOGISTICS FUND LLP
MAINFREIGHT TRUCKING TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, ILLINOIS



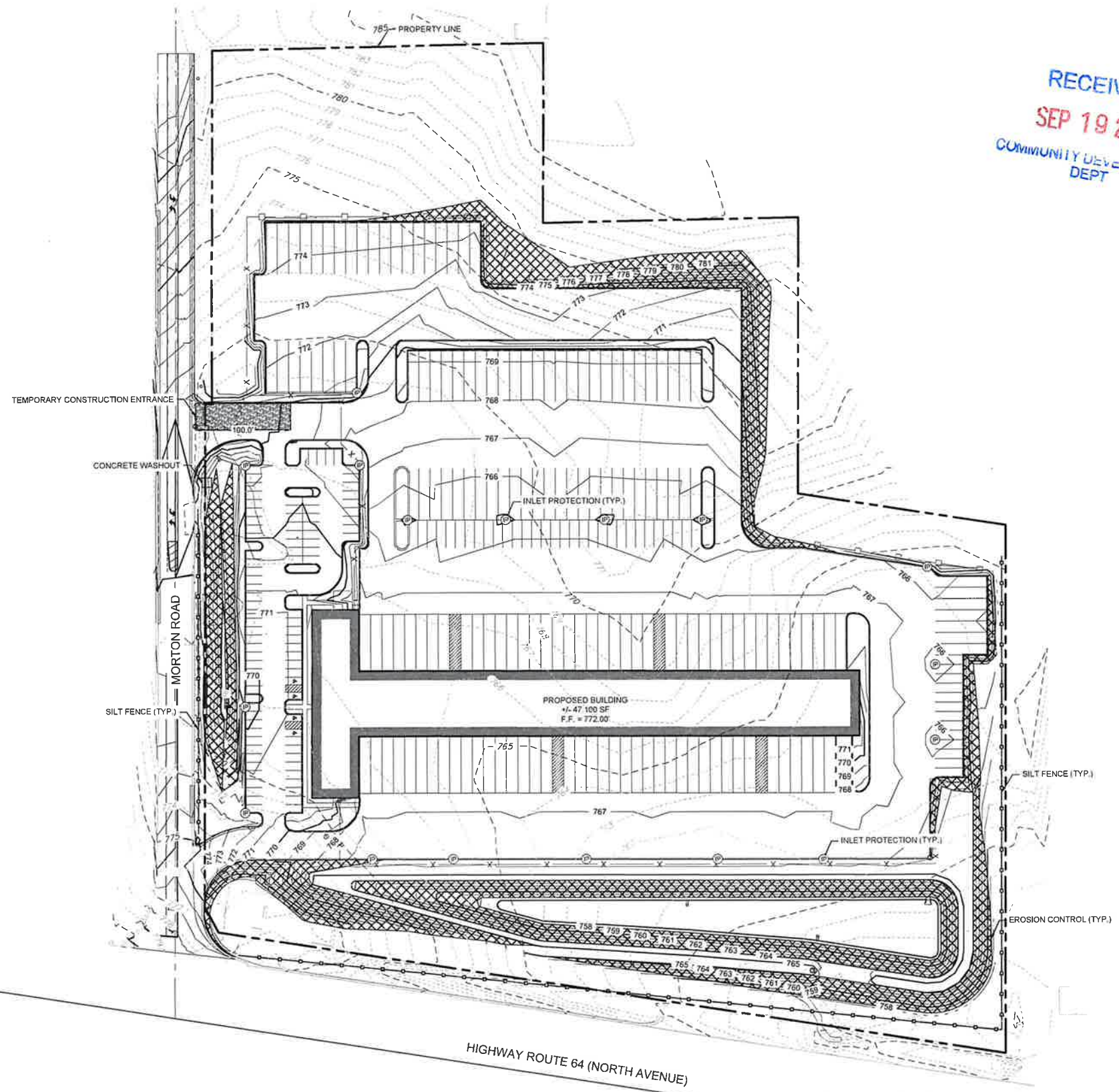
JULIE
CALL BEFORE
YOU DIG
811



PROJECT: 220302
E.D.: KHHMKJGN
DATE: 07.28.2023

SITE GRADING PLAN

SHEET: 0308
C3.0
6 OF 15



RECEIVED
 SEP 19 2023
 COMMUNITY DEVELOPMENT
 DEPT

SESC LEGEND

	SILT FENCE
	TREE PROTECTION FENCE
	EROSION CONTROL BLANKET
	RIPRAP
	TEMPORARY CONSTRUCTION ENTRANCE
	SEDIMENT LOG
	INLET PROTECTION FILTER BASKET
	CONCRETE WASHOUT

2200 CAROL DRIVE
 SUITE 520
 Lisle, IL 60532
 P: 630.595.0007
 WWW.CAGEEIL.COM



REVISIONS

NO.	DATE	DESCRIPTION

TH LOGISTICS FUND LLP
**MAINFREIGHT TRUCKING
 TERMINAL**
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, ILLINOIS



JULIE
 CALL BEFORE
 YOU DIG
811



EXHIBIT D-2

PROJ. NO. 220302
 DESIGNED BY KHMCKJGN
 DATE 07.28.2023

SOIL
 EROSION &
 SEDIMENT
 CONTROL
 PLAN

SHEET NUMBER
C4.0
 7 OF 15

RECEIVED
 SEP 19 2023
 COMMUNITY DEVELOPMENT
 DEPT

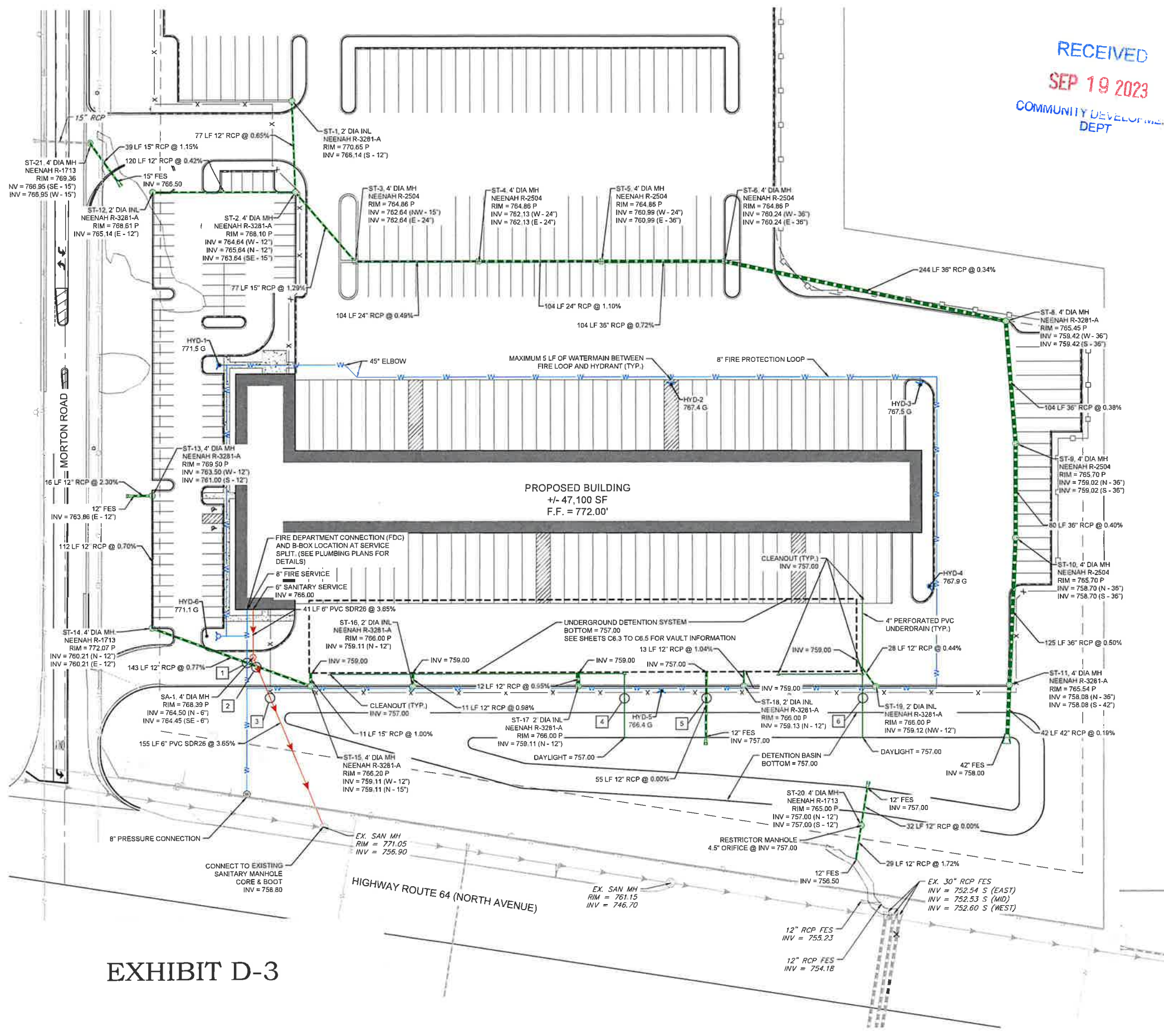


EXHIBIT D-3

UTILITY LEGEND

- PROPOSED SANITARY SEWER
- PROPOSED SANITARY MANHOLE
- PROPOSED SANITARY CLEANOUT
- PROPOSED WATER LINE
- PROPOSED WATER VALVE BOX
- PROPOSED STORM PIPE
- PROPOSED STORM STRUCTURE
- EXISTING SANITARY SEWER LINE
- EXISTING WATER LINE
- EXISTING STORM PIPE
- EXISTING FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING SANITARY MANHOLE

STORMWATER SUMMARY

REQUIRED VOLUME	7.22 AC-FT
PROVIDED VOLUME	7.56 AC-FT
EXISTING RELEASE RATE	61.73 CFS
PROVIDED RELEASE RATE	61.21 CFS
HIGH WATER LEVEL	762.47
BOTTOM OF SYSTEM	757.00

4.5" ORIFICE AT 757.00 IN THE RESTRICTOR MANHOLE

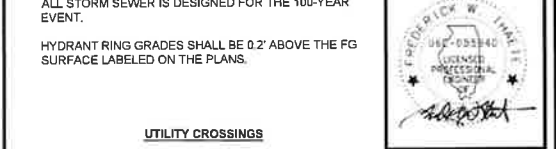
*SEE CONSTRUCTION DETAILS SHEETS C6.3 TO C6.5 FOR UNDERGROUND DETENTION DRAWINGS

EMERGENCY OVERFLOW WEIR

LENGTH	55 FT
BOTTOM	763.50
HGL	763.95
TOP OF EMBANKMENT	765.00

ALL STORM SEWER IS DESIGNED FOR THE 100-YEAR EVENT.

HYDRANT RING GRADES SHALL BE 0.2' ABOVE THE FG SURFACE LABELED ON THE PLANS.



UTILITY CROSSINGS

1	12" ST B/P = 759.40± 8" WM T/P = 757.90±
2	6" SAN B/P = 764.30± 12" ST T/P = 760.70±
3	6" SAN B/P = 763.10± 8" WM T/P = 760.22±
4	4" ST B/P = 757.00± 8" WM T/P = 755.50±
5	12" ST B/P = 756.83± 8" WM T/P = 755.33±
6	4" ST B/P = 757.00± 8" WM T/P = 755.50±

* - INDICATES WM TO BE DIPPED MIN 1.5' BELOW CROSSING PIPE

REVISIONS

NO.	DESCRIPTION

TH LOGISTICS FUND I LP
MAIN FREIGHT TRUCKING TERMINAL
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, ILLINOIS

PROJNO: 220302
 E.N.S. KHHMCKJGN
 DATE: 07.28.2023

3 FEET TYP
SITE UTILITY PLAN

811 JULIE CALL BEFORE YOU DIG 811

0 40' 80'
 1" = 40' (HORIZONTAL)

C5.0

9 15

2700 CARBOT DRIVE
 SUITE 325
 LITTLE ROCK, AR 72205
 P: 631-398-0007
 WWW.CACECIVIL.COM

CACE CIVIL ENGINEERS

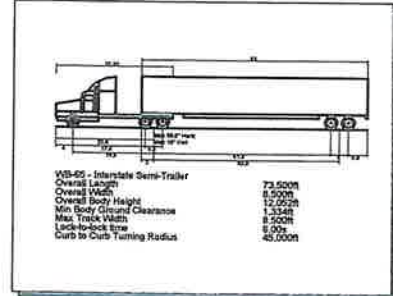
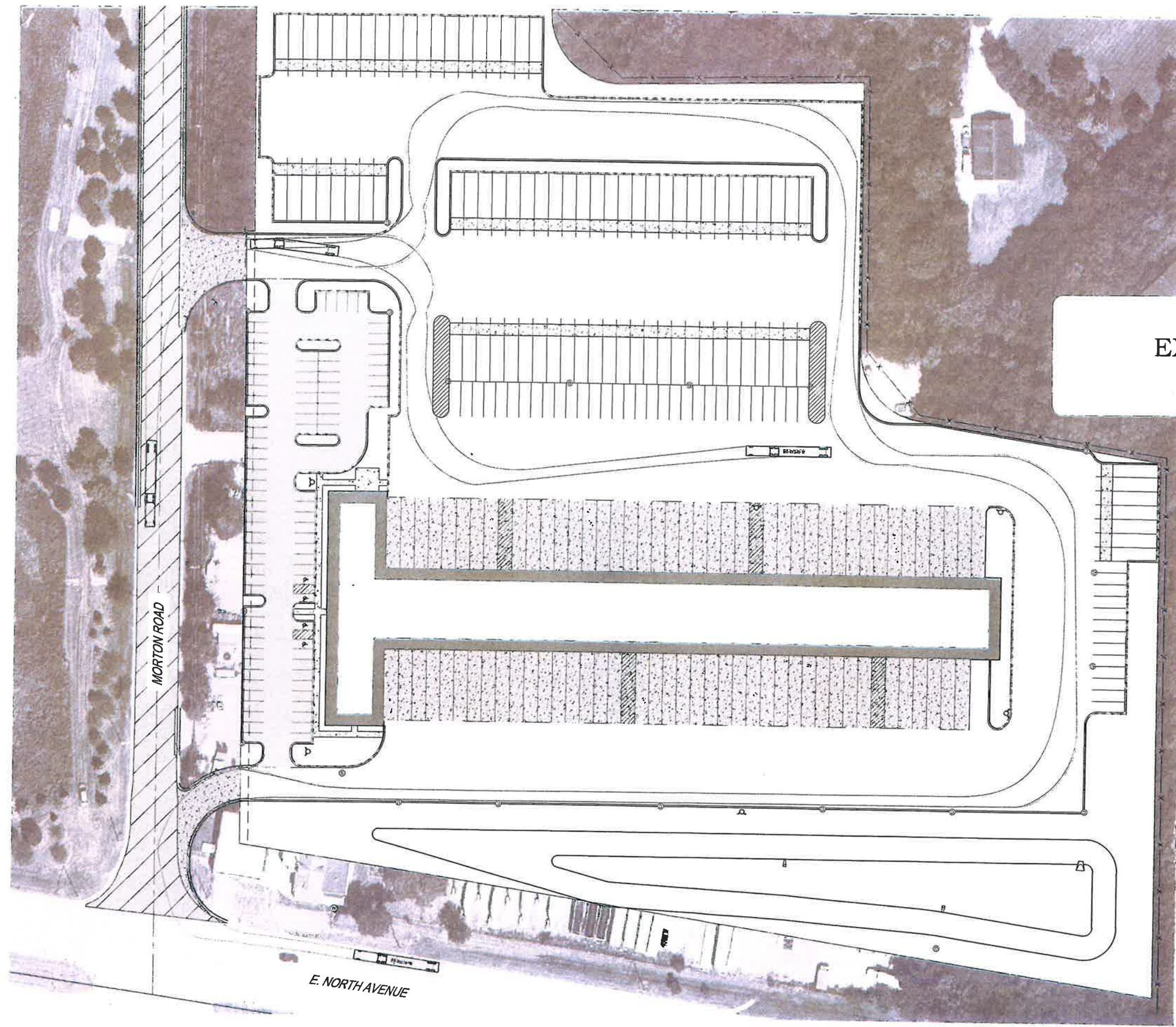


EXHIBIT E-1

RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT



0 40' 80'
1" = 40' (HORIZONTAL)

2208 CARROT DRIVE
STATE 035
LISLE, IL 60532
P. 630.598.0007
WWW.CAGECIVIL.COM



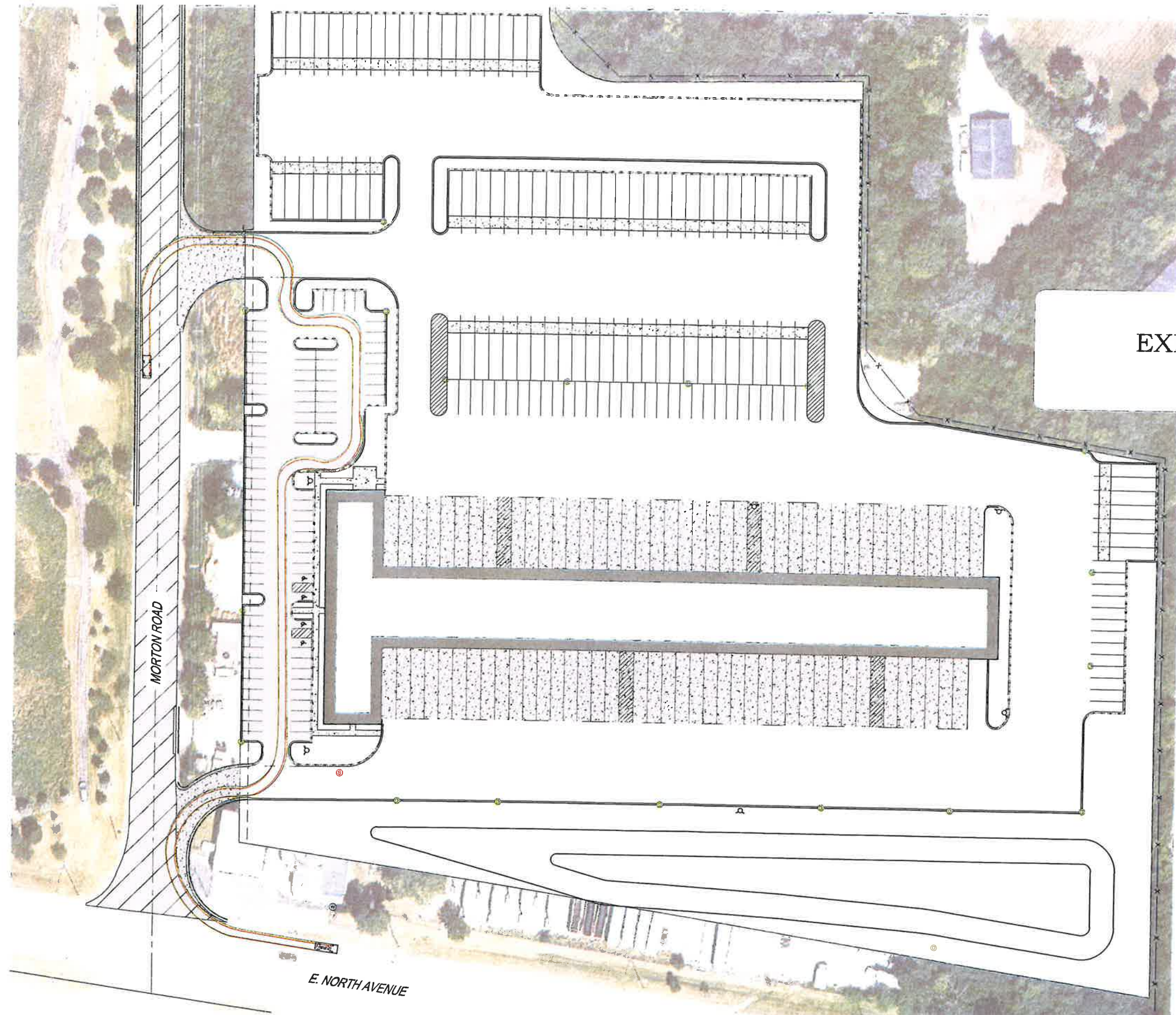
REVISIONS Δ

TH LOGISTICS FUND I/P
PROPOSED FREIGHT
TERMINAL
NORTH AVENUE & MORTON ROAD
CAROL STREAM, IL

PROJ NO 220302
ENG KHH
DATE 08/18/2023

SHEET TITLE
TURNING
EXHIBIT - WB65

SHEET NUMBER
EX-1
NA OF NA



	P - Passenger Car	19,000
	Overall Length	7,000
	Overall Width	5,100
	Min. Body Ground Clearance	7,110
	Track Width	6,000
	Lock-to-lock time	2,000
	Curb to Curb Turning Radius	24,000

EXHIBIT E-2

RECEIVED
 AUG 21 2023
 COMMUNITY DEVELOPMENT
 DEPT



2200 CABOT DRIVE
 SUITE 225
 LISLE, IL 60532
 P: 630.898.0007
 WWW.CAGECIVIL.COM



REVISIONS	DATE	BY

TH LOGISTICS FUND ILLP
**PROPOSED FREIGHT
 TERMINAL**
 NORTH AVENUE & MORTON ROAD
 CAROL STREAM, IL

PROJ NO 220302
 ENG KHH
 DATE 08/18/2023

SHEET TITLE
**TURNING
 EXHIBIT - WB65**

SHEET NUMBER
EX-1
 NA OF NA

SEAL



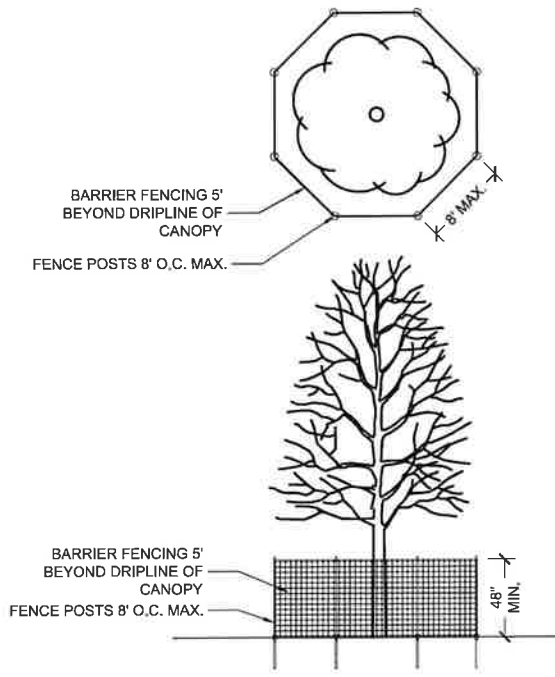
PROJECT TEAM

CIVIL ENGINEER:



EXHIBIT F-1

RECEIVED
SEP 21 2023
COMMUNITY DEVELOPMENT
DEPT



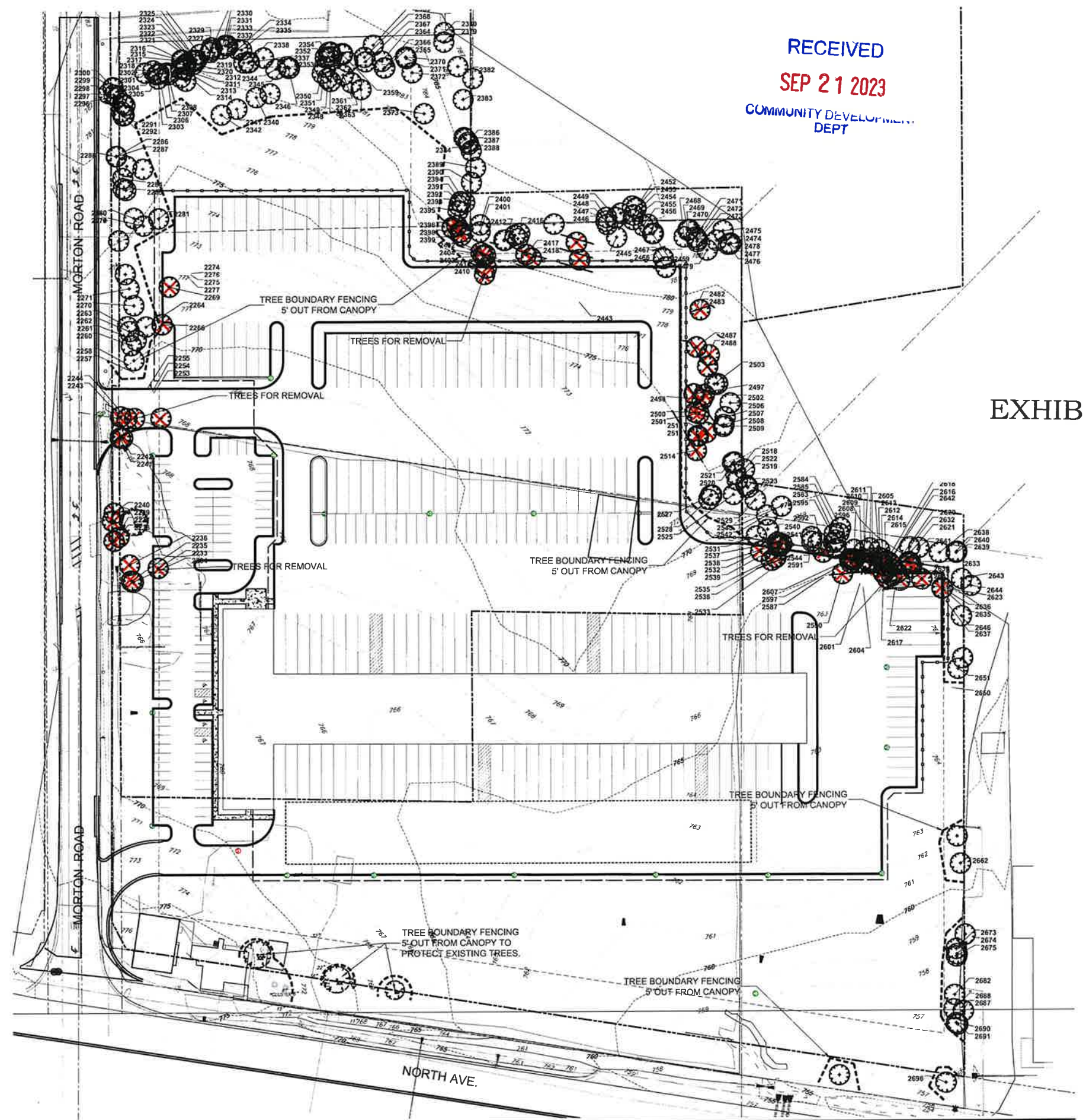
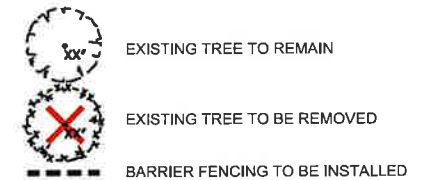
5 TREE PRESERVATION FENCING DETAIL NOT TO SCALE

EXISTING VEGETATION DESCRIPTION
THE PROJECT SITE CONSISTS OF AN EXISTING TRUCKING LOT OF 15.73 ACRES. DENSE TREE STANDS COVER APPROXIMATELY 8.25 ACRES OF THE SITE. SITE GRADING AND LAYOUT OF PAVING WILL REQUIRE REMOVING TREES ON SITE WITH APPROXIMATELY 3.04 ACRES OF TREE STAND REMAINING ON-SITE. 40 TREES WILL BE REMOVED AS PART OF THIS CONTRACT.

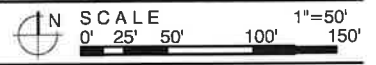
TREE PROTECTION & REMOVAL NOTES

1. CONTRACTOR SHALL OBTAIN ALL NECESSARY STATE AND LOCAL PERMITS AND PERMISSIONS TO PRUNE, REMOVE, AND/OR TRANSPLANT ANY TREES ON SITE.
2. DEAD AND DYING MATERIAL ON THE SITE SHALL BE REMOVED OR PRUNED. MATERIALS NOT LABELED ON THE PROTECTION PLAN SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR REMEDIATION.
3. DURING CONSTRUCTION EXISTING TREES OVER FOUR INCHES IN CALIPER SHALL BE PROTECTED WITH BARRIER FENCING.
4. BARRIER SHALL BE CONSTRUCTED OF A MIN. 3' TALL SNOW FENCE OR SIMILAR AND SUPPORT POSTS MIN. 6' O.C. AND SHALL BE ERECTED FIVE (5') FEET BEYOND THE DRIP LINE OFF ALL EXISTING TREES ON SITE AND ADJACENT SITES TO REMAIN.
5. BARRIER FENCING SHOWN ON THE PLAN IS APPROXIMATE. CONTRACTOR SHALL ADJUST LOCATION OF BARRIER TO POSITION OUTLINED IN COMMENT 4.
6. NO EXCESS SOIL OR ADDITIONAL FILL, BUILDING MATERIALS OR DEBRIS SHALL BE PLACED WITHIN THE PROTECTIVE BARRIER.
7. NO VEHICLES OR HEAVY MACHINERY SHALL BE ALLOWED TO WORK WITHIN THE BARRIER AREA.
8. NO ATTACHMENTS OR WIRES, OTHER THAN PROTECTIVE GUY WIRES, SHALL BE ATTACHED TO ANY OF THE TREES WHICH ARE WITHIN PROTECTIVE BARRIER.
9. STUMPS OR TREE REMAINS NOT TO BE FULLY EXCAVATED SHALL BE REMOVED. A STUMP GRINDER SHALL BE USED TO REMOVE ALL REMAINING ROOTS AND WOODY MATERIAL. WITHIN A 24" RADIUS OF THE TREE TRUNK TO MIN. 6" BELOW GRADE. DISTURBED AREA SHALL BE BACKFILLED WITH COMPACTED TOPSOIL TO MEET SURROUNDING GRADES.

TREE PROTECTION & REMOVAL LEGEND



1 TREE REMOVAL & PROTECTION PLAN



PROJECT NAME

Mainfreight
Trucking

North Ave & Morton Rd,
Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
1.	For Permit	08/04/2023
2.	Per Village Comments	08/18/2023
3.	Per Village Comments	09/21/2023

SET TYPE

LANDSCAPE PLANS

PROJECT NUMBER

2301004

DATE
01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG

SHEET TITLE
PRELIMINARY TREE
PROTECTION & REMOVAL
PLAN
SHEET NUMBER

L1.0

PRELIMINARY PLANT LIST

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT	SALT	DROUGHT
DECIDUOUS SHADE TREES							
AFA	2.5" cal.	7	Acer x freemanii 'Autumn Blaze'	Autumn Blaze Freeman Maple	B&B	M	M
ARF	2.5" cal.	11	Acer rubrum 'Frank Jr.'	Redpointe Red Maple	B&B	L	L
AMM	2.5" cal.	8	Acer miyabei 'Morton'	State Street Miyabe's maple	B&B	H	H
CEO	2.5" cal.	14	Celtis occidentalis	Common Hackberry	B&B	H	H
GBP	2.5" cal.	6	Ginkgo biloba 'Princeton Sentry'	Princeton Sentry Maidenhair Tree	B&B	H	H
GDE	2.5" cal.	7	Gymnocladus dioica 'Espresso'	Espresso Kentucky Coffeetree	B&B	H	H
GTS	2.5" cal.	15	Gleditsia triacanthos 'Shademaster'	Shademaster Honeylocust	B&B	H	H
LSM	2.5" cal.	6	Liquidambar styraciflua 'Moraine'	Moraine American Sweetgum	B&B	H	H
QUB	2.5" cal.	9	Quercus bicolor	Swamp White Oak	B&B	M	H
QUR	2.5" cal.	3	Quercus rubra	Northern Red Oak	B&B	L	H
NYS	2.5" cal.	10	Nyssa sylvatica	Black Tupelo	B&B	H	H
TAM	2.5" cal.	18	Tilia americana 'MckSentry'	American Sentry Linden	B&B	L	L
TCR	2.5" cal.	5	Tilia cordata 'Greenspire'	Greenspire Littleleaf Linden	B&B	L	L
ULA	2.5" cal.	10	Ulmus davidiana var. japonica 'Morton'	Accolade Elm	B&B	M	H
ULH	2.5" cal.	8	Ulmus 'Regal'	Regal Elm	B&B	H	H

ORNAMENTAL TREES							
AGF	6' Multi.	6	Acer ginnala 'flame'	Flame Amur Maple	B&B	M	M
AGP	6' multi.	2	Amelanchier x grandiflora.	Shadblow Serviceberry	B&B	H	M
BEN	6' multi.	15	Betula nigra	River Birch	B&B	H	M

EVERGREEN TREES							
ACO	6' hl.	30	Abies concolor	White Fir	B&B	L	H
PIS	6' hl.	40	Pinus strobus	White Pine	B&B	L	L
PGD	6' ht.	38	Picea glauca 'Densata'	Black Hills Spruce	B&B	M	H

DECIDUOUS SHRUBS							
AAB	24" ht.	29	Aronia arbutifolia 'Brilliantissima'	Brilliant Red Chokeberry	B&B	M	M
AMA	24" ht.	40	Aronia melanocarpa 'Elata'	Elata Chokeberry	B&B	M	M
CAM	24" ht.	75	Cornus alba 'Elegantissima'	Variegated Dogwood	B&B	H	L
CSF	24" ht.	19	Cornus stolonifera 'Farrow'	Arclic Fire Redtwig Dogwood	B&B	H	M
FOS	24" ht.	36	Forsythia x intermedia 'Sunrise'	Sunrise Forsythia	B&B	H	M
HYA	24" ht.	20	Hydrangea arbor. 'Haas Halo'	Haas Halo Hydrangea	B&B	M	L
HYQ	24" ht.	55	Hydrangea quercifolia 'Alice'	Alice Oakleaf Hydrangea	B&B	M	M
POB	24" ht.	21	Physocarpus opulifolius 'Monlo'	Diabolo Ninebark	B&B	H	H
SBT	18" w.	25	Spiraea betulifolia 'tor'	Tor Birchleaf Spirea	B&B	H	H
SYM	24" ht.	4	Syringa patula 'Miss Kim'	Miss Kim Korean Lilac	B&B	H	H
VTC	24" ht.	33	Viburnum trilobum 'Alfredo'	Alfredo American Cranberrybush	B&B	M	H

EVERGREEN SHRUBS							
JCS	24" w.	28	Juniperus chinensis 'Sargentii'	Green Sargent Juniper	B&B	M	H
TMT	24" w.	20	Taxus x media 'Taunton'	Taunton's Yew	B&B	L	H

ORNAMENTAL GRASSES							
CAA	#1 cont.	36	Calamagrostis acutifolia 'Strictus'	Strictus Feather Reed Grass		L	H

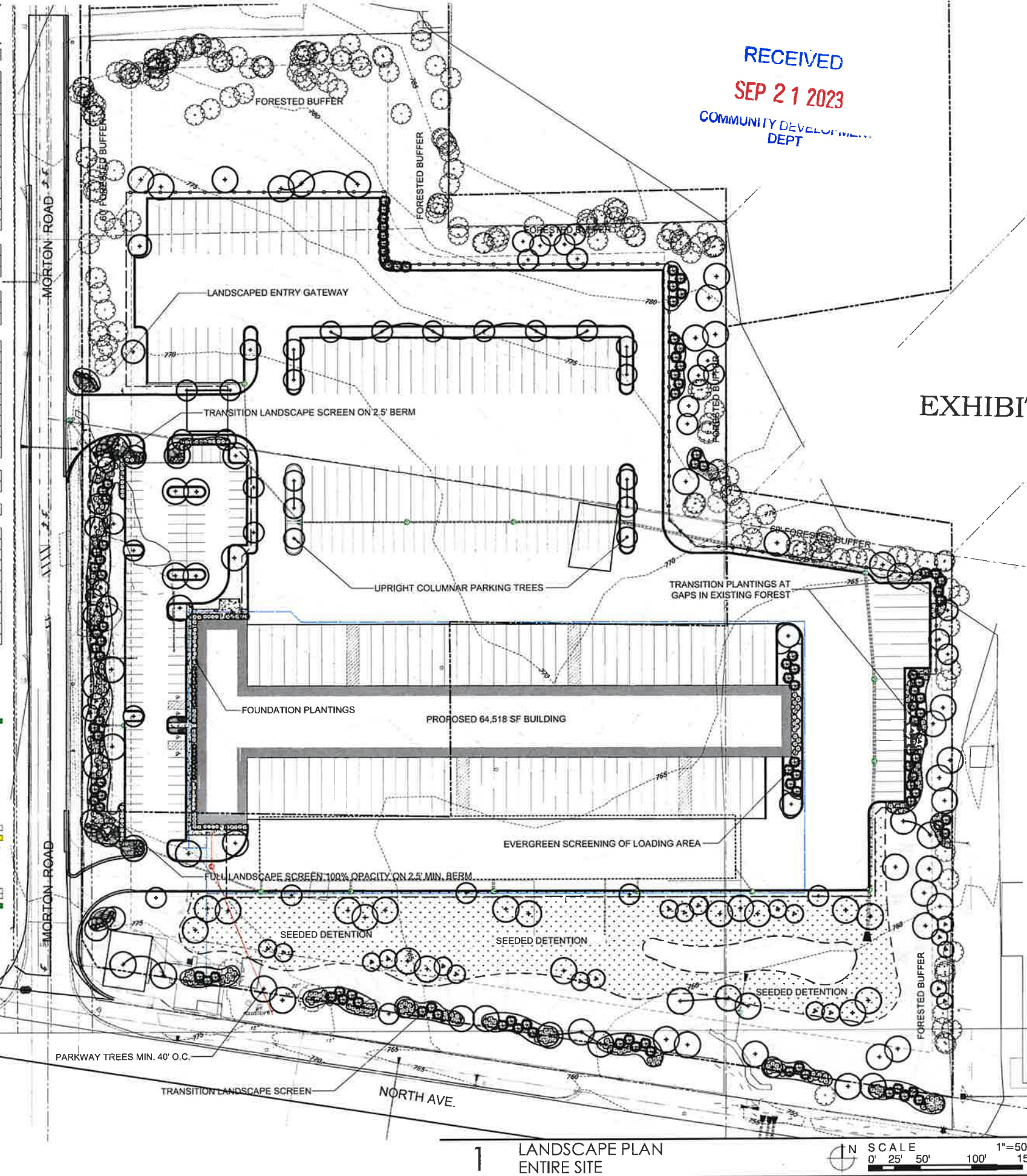
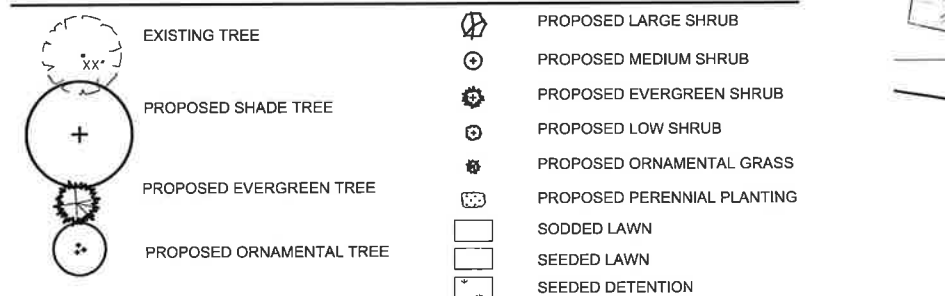
GROUND COVER / PERENNIALS							
ALS	#1 cont.	80	Allium 'summer beauty'	Summer Beauty Wild Onion	18" O.C.	H	H
CVM	#1 cont.	16	Coreopsis verticillata 'Moonbeam'	Moonbeam Coreopsis	18" O.C.	H	H
ECP	#1 cont.	30	Echinacea purpurea 'Magnus'	Magnus Purple Coneflower	24" O.C.	H	H
HEC	#1 cont.	92	Hemerocallis x. 'Chicago Apache'	Chicago Apache Daylily	24" O.C.	H	H
LAV	#1 cont.	23	Lavandula 'Munstead strain'	Munstead English Lavender	24" O.C.	H	H
NFW	#1 cont.	8	Nepeta fasseni 'Walker's low'	Walkers Low Catmint	24" O.C.	H	H
PHD	#1 cont.	16	Phlox divaricata	Blue Phlox	15" O.C.	H	H
RUD	#1 cont.	60	Rudbeckia fulgida 'Goldsturm'	Goldsturm Black-Eyed Susan	18" O.C.	H	H

SOD sq. yd. 2822 Sodded Lawn
SEED sq. yd. 10686 Seeded Lawn
DET sq. yd. 7360 Salt Tolerant Wetland / Detention Seed - See Below

Salt Tolerant Wetland / Detention Seed mix available through: Genesis Nursery Inc. Tampico, IL - 877-817-5325.
Apply at manufacturers recommended rates. All slopes over 4:1 to receive BioNet SC150BN or similar Erosion Blanket.

SYM	SIZE	QTY	BOTANICAL NAME	COMMON NAME	COMMENT	SALT	DROUGHT
ORNAMENTAL TREES							
AGF	6' Multi.	6	Acer ginnala 'flame'	Flame Amur Maple	B&B	M	M
AGP	6' multi.	2	Amelanchier x grandiflora.	Shadblow Serviceberry	B&B	H	M
BEN	6' multi.	15	Betula nigra	River Birch	B&B	H	M

LANDSCAPE LEGEND



LG Workshop LLC
Landscape Architecture
Site Planning
Illustration
1955 N. Wilmot Ave.
Chicago, IL 60647
ph. 773.697.4388
www.LGWLA.com

SEAL
LICENSED LANDSCAPE ARCHITECT
LARRY C. GLASSCOCK, JR.
STATE OF ILLINOIS
EXPIRES 08/2025

PROJECT TEAM
CIVIL ENGINEER:
CAGE
CIVIL ENGINEERS

PROJECT NAME
Mainfreight
Trucking
North Ave & Morton Rd.
Carol Stream, IL
DRAWING ISSUED
NO. TITLE DATE
1. For Permit 08/04/2023
2. Per Village Comments 08/18/2023
3. Per Village Comments 09/21/2023

SET TYPE
LANDSCAPE PLANS
PROJECT NUMBER
2301004
DATE
01-06-2022
DRAWN BY: APPROVED BY:
LCG LCG
SHEET TITLE
LANDSCAPE PLAN
ENTIRE SITE
SHEET NUMBER
L2.0

1 LANDSCAPE PLAN
ENTIRE SITE
SCALE
1"=50'
0' 25' 50' 100' 150'

RECEIVED
 SEP 21 2023
 COMMUNITY DEVELOPMENT
 DEPT



PROJECT TEAM

CIVIL ENGINEER:
 CAGE
 CIVIL ENGINEERING

PROJECT NAME

Mainfreight
 Trucking

North Ave & Morton Rd.
 Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
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SET TYPE
 LANDSCAPE PLANS

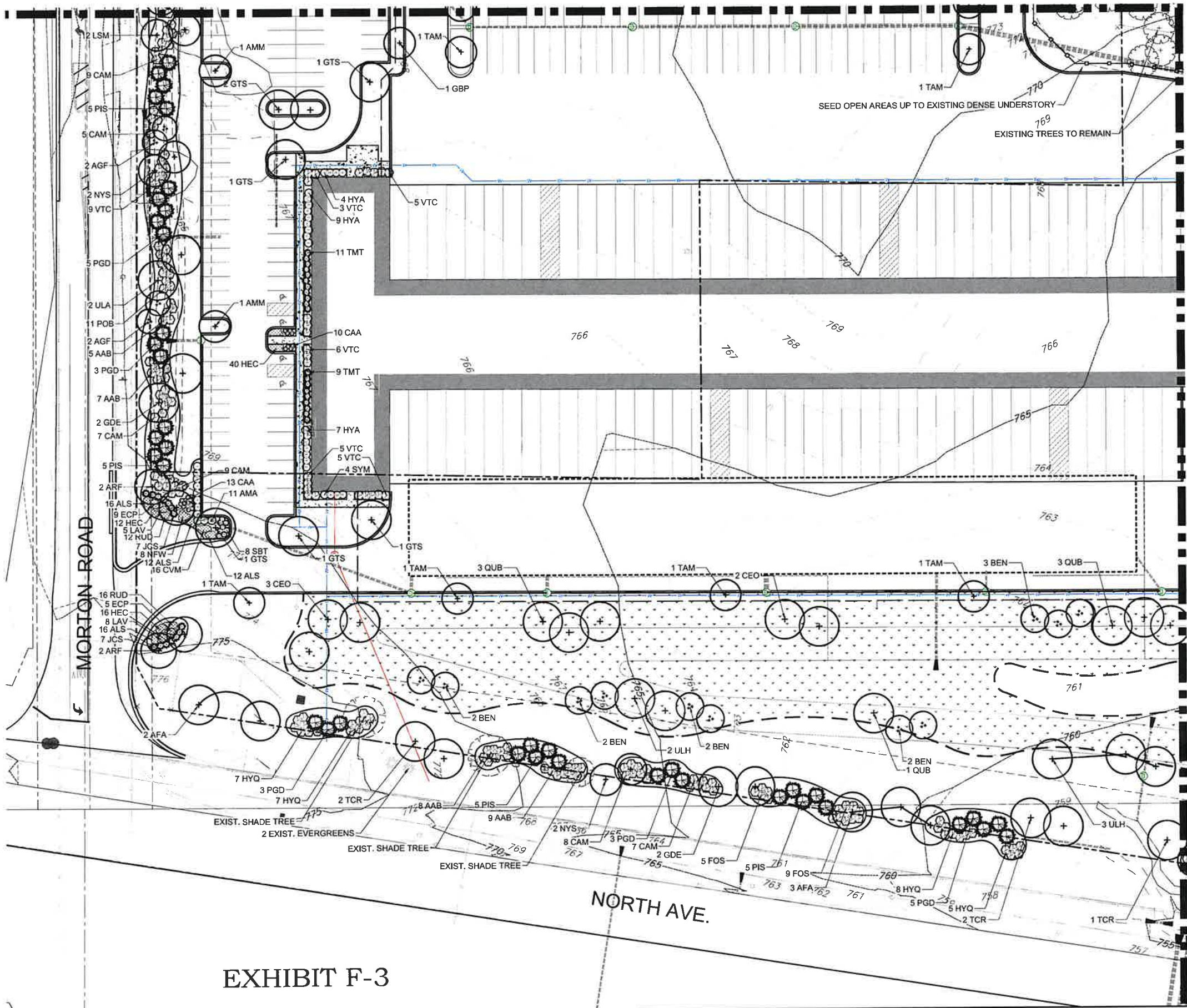
PROJECT NUMBER
 2301004

DATE
 01-06-2022
 DRAWN BY: LCG APPROVED BY: LCG

SHEET TITLE
 LANDSCAPE PLAN -
 SOUTHWEST

SHEET NUMBER

L2.1

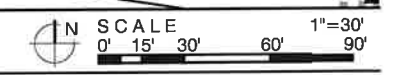


LANDSCAPE LEGEND

- EXISTING TREE
- PROPOSED SHADE TREE
- PROPOSED EVERGREEN TREE
- PROPOSED ORNAMENTAL TREE
- PROPOSED LARGE SHRUB
- PROPOSED MEDIUM SHRUB
- PROPOSED EVERGREEN SHRUB
- PROPOSED LOW SHRUB
- PROPOSED ORNAMENTAL GRASS
- PROPOSED PERENNIAL PLANTING
- SODDED LAWN
- SEEDED LAWN
- SEEDED DETENTION

EXHIBIT F-3

1 LANDSCAPE PLAN
 SOUTHWEST



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 DEPT



PROJECT TEAM

CIVIL ENGINEER:
CAGE
 CIVIL ENGINEERS

PROJECT NAME

Mainfreight
 Trucking

North Ave & Morton Rd.
 Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
1.	For Permit	08/04/2023
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SET TYPE
 LANDSCAPE PLANS

PROJECT NUMBER
 2301004

DATE
 01-06-2022
 DRAWN BY: LCG
 APPROVED BY: LCG

SHEET TITLE
 LANDSCAPE PLAN -
 SOUTHEAST

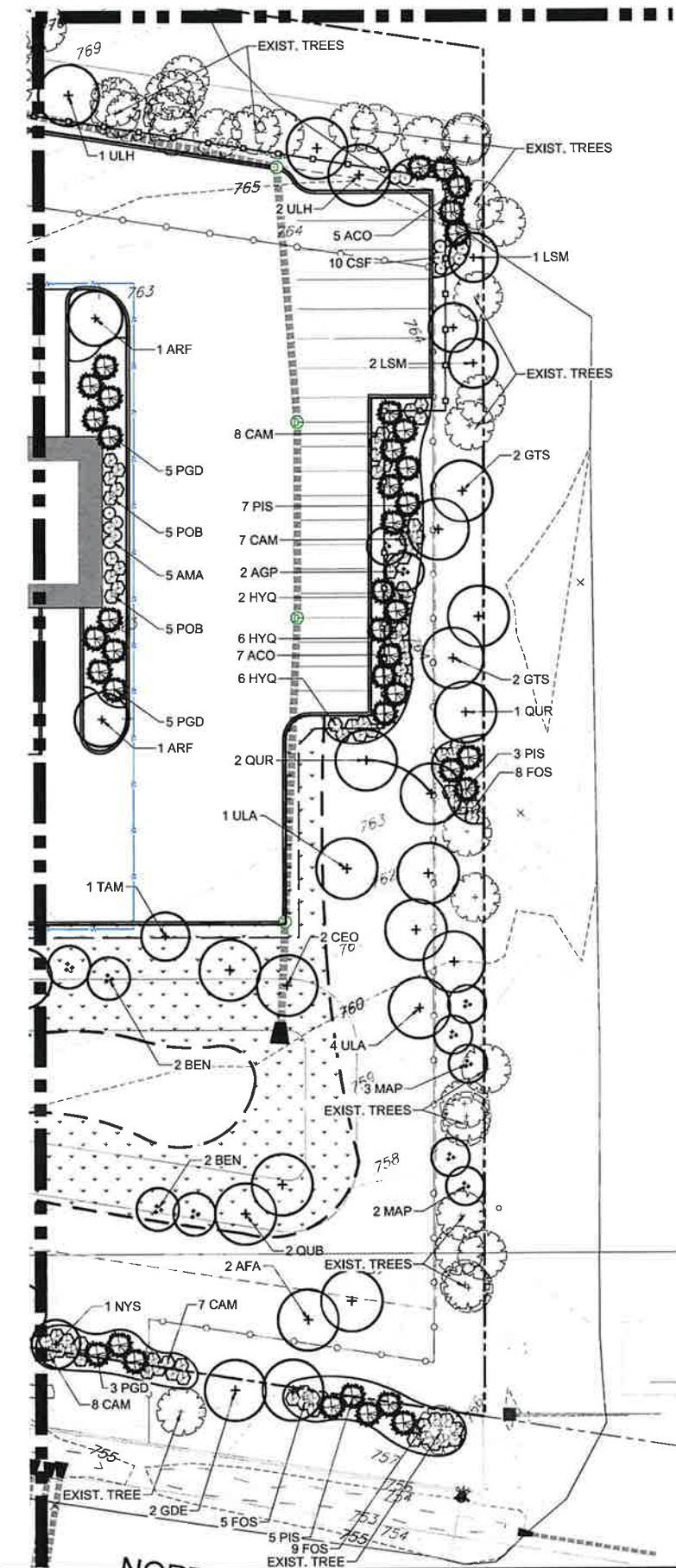
SHEET NUMBER

L2.2

EXHIBIT F-4

LANDSCAPE LEGEND

- EXISTING TREE
- PROPOSED SHADE TREE
- PROPOSED EVERGREEN TREE
- PROPOSED ORNAMENTAL TREE
- PROPOSED LARGE SHRUB
- PROPOSED MEDIUM SHRUB
- PROPOSED EVERGREEN SHRUB
- PROPOSED LOW SHRUB
- PROPOSED ORNAMENTAL GRASS
- PROPOSED PERENNIAL PLANTING
- SODDED LAWN
- SEEDED LAWN
- SEEDED DETENTION



RECEIVED
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 COMMUNITY DEVELOPMENT
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LG Landscape Architecture
 Site Planning
 Illustration
Workshop LLC
 1955 N. Wilmot Ave.
 Chicago, IL 60647
 ph. 773.697.4388
 www.LGWLA.com

SEAL



PROJECT TEAM

CIVIL ENGINEER:



EXHIBIT F-5

PROJECT NAME

Mainfreight
 Trucking

North Ave & Morton Rd.
 Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
1.	For Permit	08/04/2023
2.	Per Village Comments	08/18/2023
3.	Per Village Comments	09/21/2023

SET TYPE
 LANDSCAPE PLANS

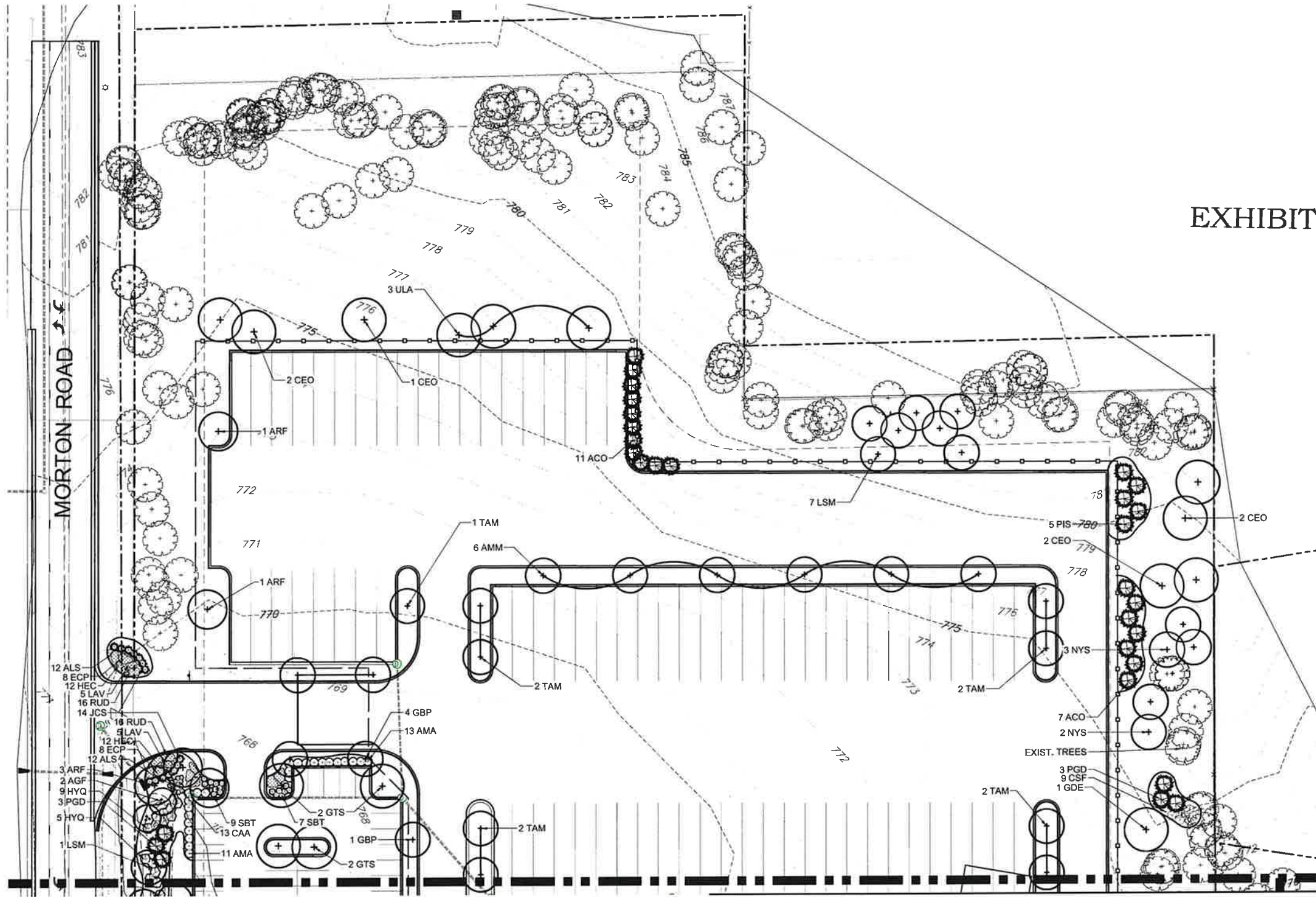
PROJECT NUMBER
 2301004

DATE
 01-06-2022
 DRAWN BY: LCG
 APPROVED BY: LCG

SHEET TITLE
 LANDSCAPE PLAN -
 NORTHWEST

SHEET NUMBER

L2.3



LANDSCAPE LEGEND

- EXISTING TREE
- PROPOSED SHADE TREE
- PROPOSED EVERGREEN TREE
- PROPOSED ORNAMENTAL TREE
- PROPOSED LARGE SHRUB
- PROPOSED MEDIUM SHRUB
- PROPOSED LOW SHRUB
- PROPOSED EVERGREEN SHRUB
- PROPOSED ORNAMENTAL GRASS
- PROPOSED PERENNIAL PLANTING
- SODDED LAWN
- SEEDED LAWN
- SEEDED DETENTION

1 LANDSCAPE PLAN
 NORTHWEST

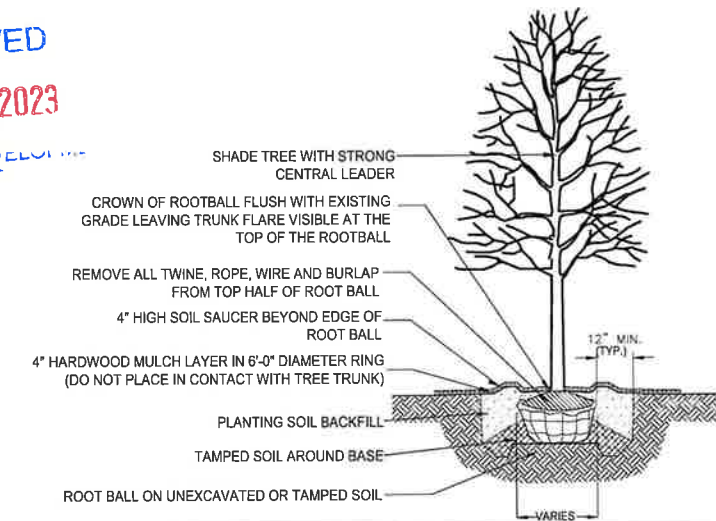
N SCALE 1"=30'
 0' 15' 30' 60' 90'

RECEIVED
SEP 21 2023
COMMUNITY DEVELOPMENT
DEPT



PROJECT TEAM

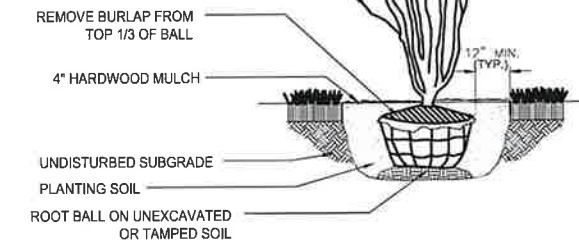
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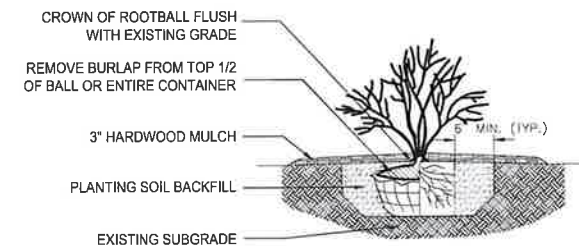
1 SHADE TREE PLANTING DETAIL NOT TO SCALE

NOTES:

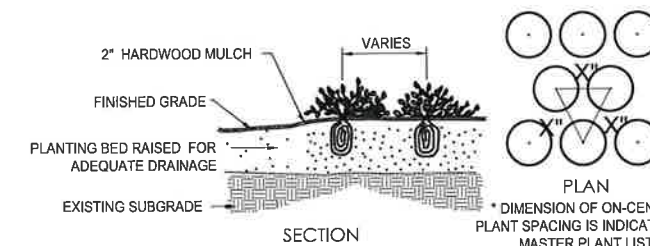
NEVER CUT LEADERS TREE SHALL BEAR SAME RELATION TO FINISHED GRADE AS IT BORE TO PREVIOUS GRADE.



2 ORNAMENTAL TREE PLANTING DETAIL NOT TO SCALE



3 SHRUB PLANTING DETAIL NOT TO SCALE



4 PERENNIAL / ANNUAL PLANTING DETAIL NOT TO SCALE

LANDSCAPE NOTES

- CONTRACTOR SHALL OBTAIN ALL NECESSARY LOCAL PERMITS AND PERMISSIONS TO INSTALL THE PROPOSED IMPROVEMENTS
- ALL LANDSCAPE MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH CAROL STREAM LANDSCAPING CODES AND ZONING ORDINANCES.
- PRIOR TO COMMENCING ANY WORK, CONTRACTOR SHALL HAVE DIGGERS HOTLINE LOCATE AND MARK ALL UNDERGROUND UTILITY FACILITIES AND LINES.
- ALL PLANT MATERIALS (EXCEPT FOR GROUND COVER, ANNUALS, AND PERENNIALS) SHALL BE BALLED AND BURLAPPED STOCK AND MEET CURRENT STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN'S STANDARD FOR NURSERY STOCK (ANSI 260.1-1986) OR EQUAL. PLANT MATERIALS MUST BE SUPPLIED WITHIN A 150 MILE RADIUS OF PROJECT SITE WITHIN NORTHEAST ILLINOIS. CONTRACTOR MAY SUBSTITUTE CONTAINER STOCK FOR SHRUBS IF SIZES ARE EQUAL TO SPECIFIED B&B STOCK, WITH THE APPROVAL OF THE LANDSCAPE ARCHITECT.
- IF SPECIFIED PLANTS ARE NOT AVAILABLE AT THE TIME OF ORDERING, PLANTS WITH SIMILAR WHOLESALE VALUE AND LANDSCAPE CHARACTERISTICS MAY BE SUBSTITUTED UPON THE APPROVAL OF THE LANDSCAPE ARCHITECT AND VILLAGE STAFF.
- SOIL IN GROUND COVER BEDS SHALL BE AMENDED USING 2 INCHES OF MUSHROOM COMPOST INCORPORATED INTO THE TOP 4 INCHES OF SOIL.
- DISTURBED AREAS TO RECEIVE SOD SHALL BE TILLED TO 6" DEPTH AND FINE GRADED TO PROVIDE SMOOTH BASE SURFACE. IF EXISTING SOIL IS A MAJORITY OF CLAY OR UNSUITABLE, 2" OF FINE GRADED TOPSOIL SHALL BE ADDED PRIOR TO TILLING. EXISTING SOD AREAS SHALL HAVE TURF REMOVED WITH AUTOMATED SODCUTTER OR HAND SPACE TO REMOVE ALL BLADES AND ROOTS. 1" OF FINE GRADED TOPSOIL SHALL BE TILLED AND GRADED.
- TREE AND SHRUB BACKFILL MIXTURE SHALL BE 2 PARTS EXIST. NATIVE TOPSOIL AND 1 PART SPHAGNUM PEAT MOSS W/ DECOMPOSED MANURE.
- ALL SHRUB BEDS AND INDIVIDUAL TREE PLANTINGS, UNLESS OTHERWISE NOTED, SHALL RECEIVE A 4 INCH LAYER OF SHREDDED HARDWOOD MULCH. ALL GROUND COVER, ANNUAL AND PERENNIAL BEDS SHALL RECEIVE A 2 INCH LAYER OF THE SAME MULCH MATERIAL. COSTS FOR MULCH SHALL BE CONSIDERED INCIDENTAL AND SHALL BE INCLUDED IN THE COST OF PLANTINGS.
- NURSERY TAGS (SPECIES, SIZE) FOR ALL SHADE TREES SHALL REMAIN ATTACHED TO TREES UNTIL FINAL APPROVAL FROM MUNICIPALITY.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER A BONDED WRITTEN ONE-YEAR WARRANTY AGREEMENT (BEGINNING ON THE OWNER'S POSSESSION DATE). THIS AGREEMENT SHALL COVER MAINTENANCE, REPAIR, AND REPLACEMENT OF ALL DEAD OR DAMAGED LANDSCAPING TO PRESERVE THE SAME QUANTITY AND QUALITY AS INITIALLY APPROVED
- AN IRRIGATION SYSTEM SHALL BE DESIGNED AND BID TO PROVIDE ADEQUATE WATER TO ALL NEWLY PLANTED TREES, SHRUBS, PERENNIALS AND GROUND COVER BEDS, AND TURF AREAS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL AND APPLY FOR ALL NECESSARY PERMITS PRIOR TO COMMENCING WORK. IRRIGATION PLANS SHALL INCLUDE HUNTER OR SIMILAR PRO-C CONTROLLER W/WIRELESS SOLAR SYNC STATION AND HUNTER OR SIMILAR SPRAYHEADS AND NOZZLES. IRRIGATION WORK SHALL WARRANTY ALL LABOR AND MATERIALS FOR 1 FULL YEAR AFTER INSTALLATION AND TESTING.
- SEEDED LAWN AREAS SHALL BE BID WITH A BID ALTERNATE FOR HYDROSEEDING LAWN. PRIOR TO SEEDING, 2" OF FINE TOPSOIL SHALL BE TILLED INTO EXIST SOIL MIXTURE. A MIX CONSISTING OF ROUGHLY 30% BLUEGRASS / 30% FINE FESCUES / 40% RYE GRASSES (AND TACKIFIER FOR HYDROSEEDING) SHALL BE APPLIED AT MANUFACTURERS SPECIFIED RATES FOR NEW LAWNS BETWEEN 5 AND 10 LBS PER 1,000 SF.
- TREES AND SHRUBS SHALL NOT BE LOCATED CLOSER THAN TEN (10) FEET TO FIRE HYDRANTS, TRANSFORMERS OR OTHER ABOVE GROUND UTILITIES. ANY DISCREPANCY ON THE PLAN RELATED TO THESE PROXIMATE UTILITIES SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR RESOLUTION.

VILLAGE LANDSCAPE REQUIREMENT CALCULATIONS

DESCRIPTION	REQUIREMENT	AREA / REQ.	PROPOSED
TOTAL SITE		686,581 SQ. FT. 15.76 Acres	
TOTAL BUILDING		45,818 SQ. FT.	
TOTAL PARKING	Square footage of parking area	29,296 SQ. FT.	
PARKING LOT PERIMETER AREAS	One shrub or native grass the height of which shall not be less than three feet nor greater than four feet, shall be planted for every three feet of landscape area length, spaced linearly to adequately screen vehicle bumpers.	380 L.F. / 3 = 126 shrubs	138 shrubs
PARKING LOT INTERIOR AREAS	One parking lot island shall be provided between every 15 parking spaces. Parking lot island locations may be varied based on specific site requirements or design scheme	89 Spaces / 15 = 6 Islands	7 Islands Provided
BUILDING FOUNDATION AREAS	A minimum four foot-wide hedge row shall be planted with one shrub or native grass every three feet on center, spaced linearly 80% of the length of the facade	1556 LF X .80 = 1245 / 3 = 415	10 Evergreen Trees 83 Shrubs *Limited area because of overhead doors
TRANSITION AREAS	Type D transition yard per 100' Min. yard width = 15' 6' ht. Fence 5 Canopy / Evergreen Trees 5 Ornamental Trees 30 Shrubs / Ornamental Grasses	2231 LF/100=223 Min. Yard = 15' 6' fence 22.3 x 5 = 112 22.3 x 5 = 112 22.3 x 30 = 669	Min. Yard = 20' 6' Opaque fence 234 Exist. Trees - 72 Proposed Trees Exist. Dense Understory - 7 Orn. Trees 46 Shrubs

EXHIBIT F-6

PROJECT NAME

Mainfreight
Trucking

North Ave & Morton Rd.
Carol Stream, IL

DRAWING ISSUED

NO.	TITLE	DATE
1.	For Permit	08/04/2023
2.	Per Village Comments	08/18/2023
3.	Per Village Comments	09/21/2023

SET TYPE
LANDSCAPE PLANS

PROJECT NUMBER
2301004

DATE
01-06-2022
DRAWN BY: LCG
APPROVED BY: LCG

SHEET TITLE
PRELIMINARY LANDSCAPE
PLAN / DETAILS & NOTES

SHEET NUMBER

L3.0

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AUG 21 2023
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DEPT

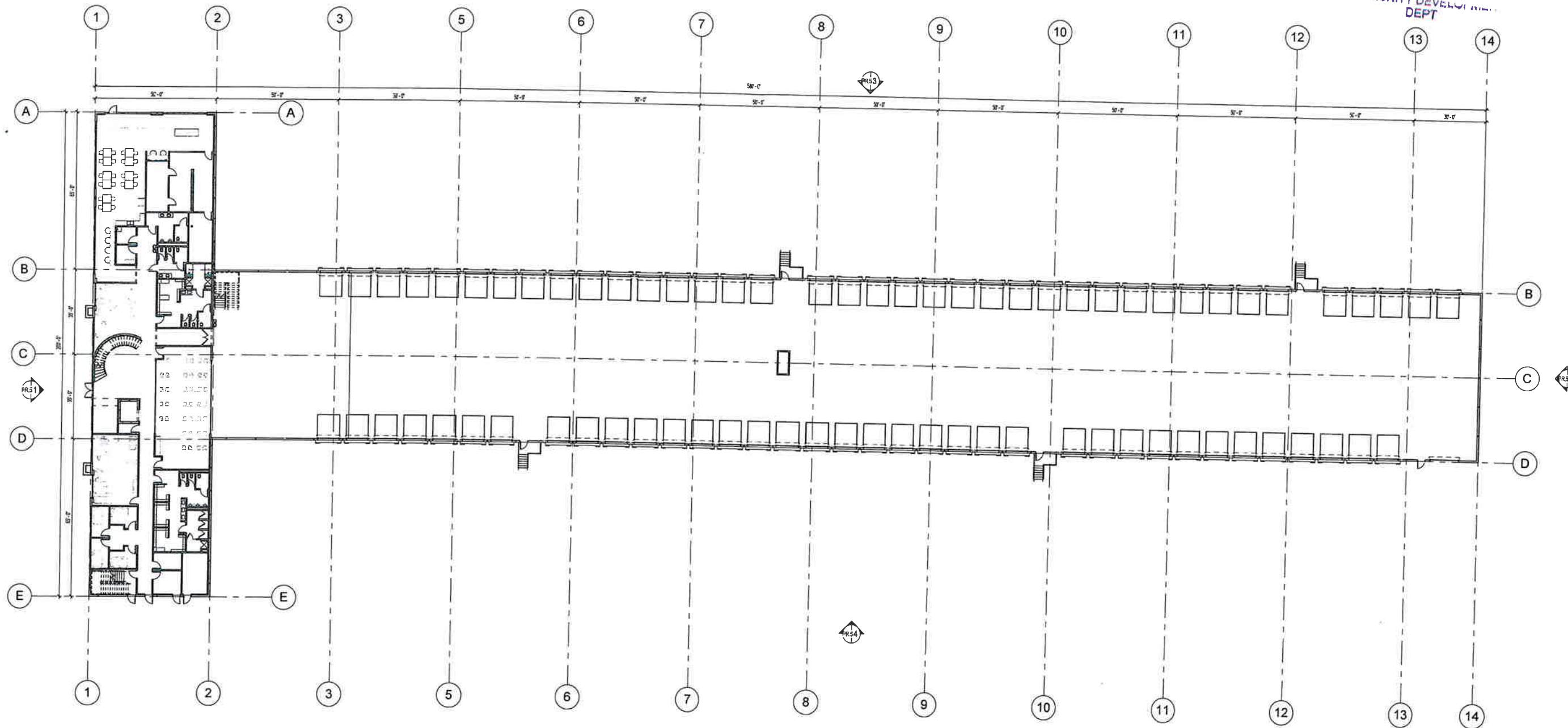


EXHIBIT G-1



MAINFREIGHT TERMINAL
NORTH AVE. & MORTON RD.
Carol Stream, IL 60185

SCALE - 1" = 20'-0"

OVERALL FLOOR PLAN - LEVEL 1



(21436) GA1271 08-21-2023

PR.1



RECEIVED
AUG 21 2023
COMMUNITY DEVELOPMENT
DEPT

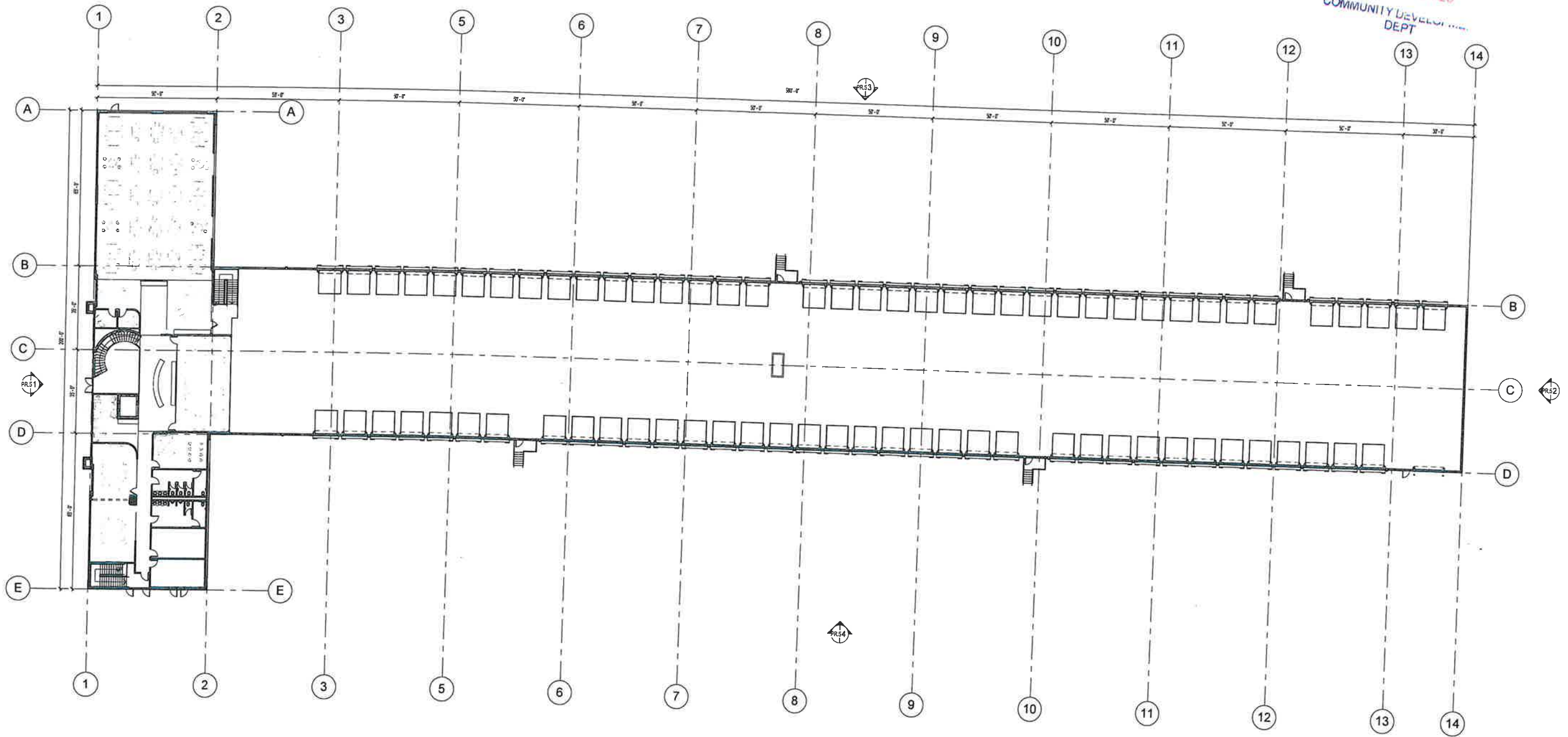


EXHIBIT G-2



MAINFREIGHT TERMINAL
NORTH AVE. & MORTON RD.
Carol Stream, IL 60188

SCALE = 1" = 20'-0"

OVERALL FLOOR PLAN - LEVEL 2



(24x36) GA1271 08-21-2023

PR.2

ARCO
MURRAY

GMA
ARCHITECTS

RECEIVED
 AUG 21 2023
 COMMUNITY DEVELOPMENT
 DEPT

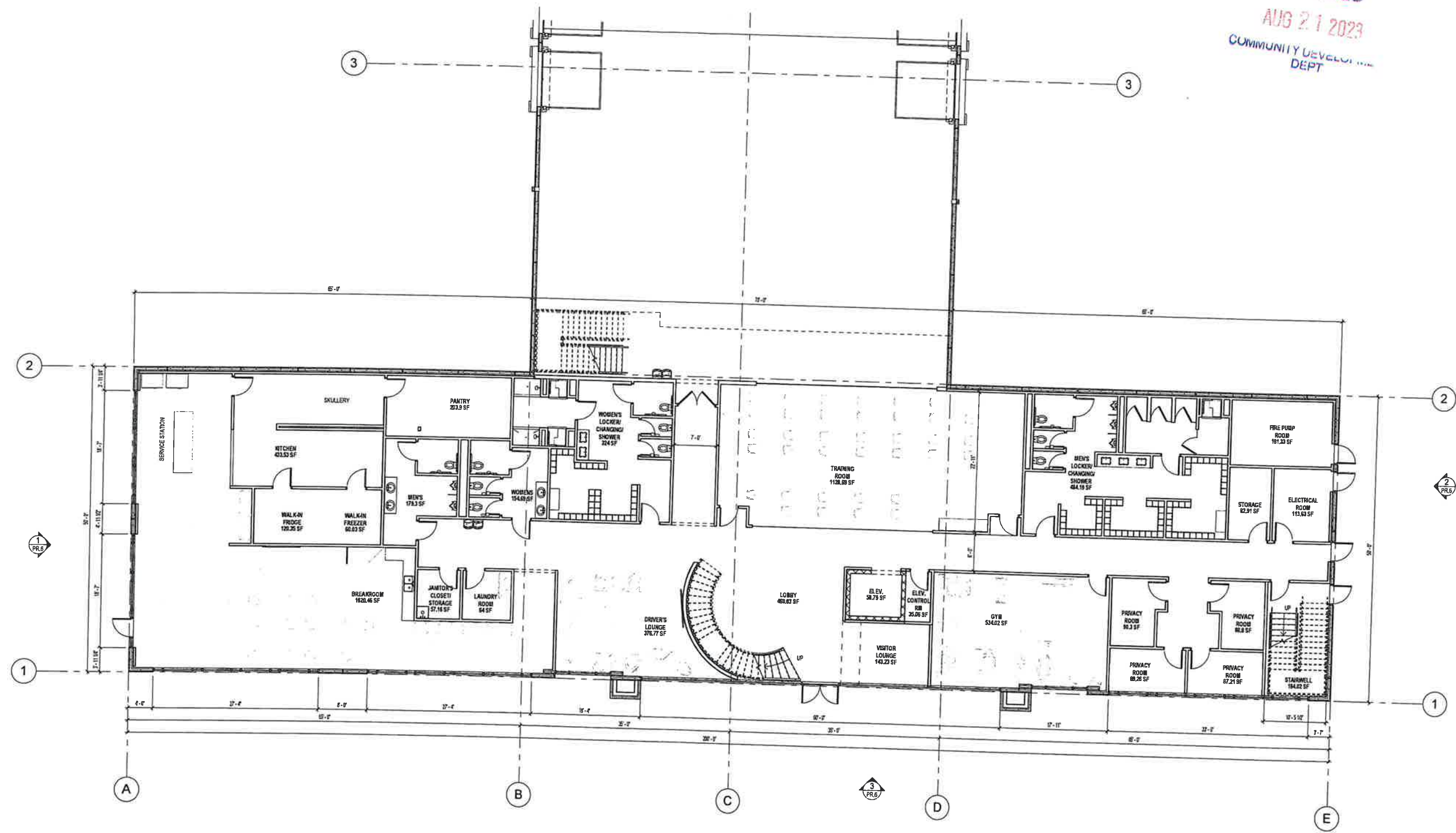


EXHIBIT G-3



MAINFREIGHT TERMINAL
 NORTH AVE. & MORTON RD.
 Carol Stream, IL 60185

SCALE = 1/8" = 1'-0"

ENLARGED OFFICE FLOOR PLAN - LEVEL 1



[24436] CA1271 08-21-2023

PR.3

ARCO
 MURRAY

GMA
 ARCHITECTS

RECEIVED
 AUG 21 2023
 COMMUNITY DEVELOPMENT
 DEPT

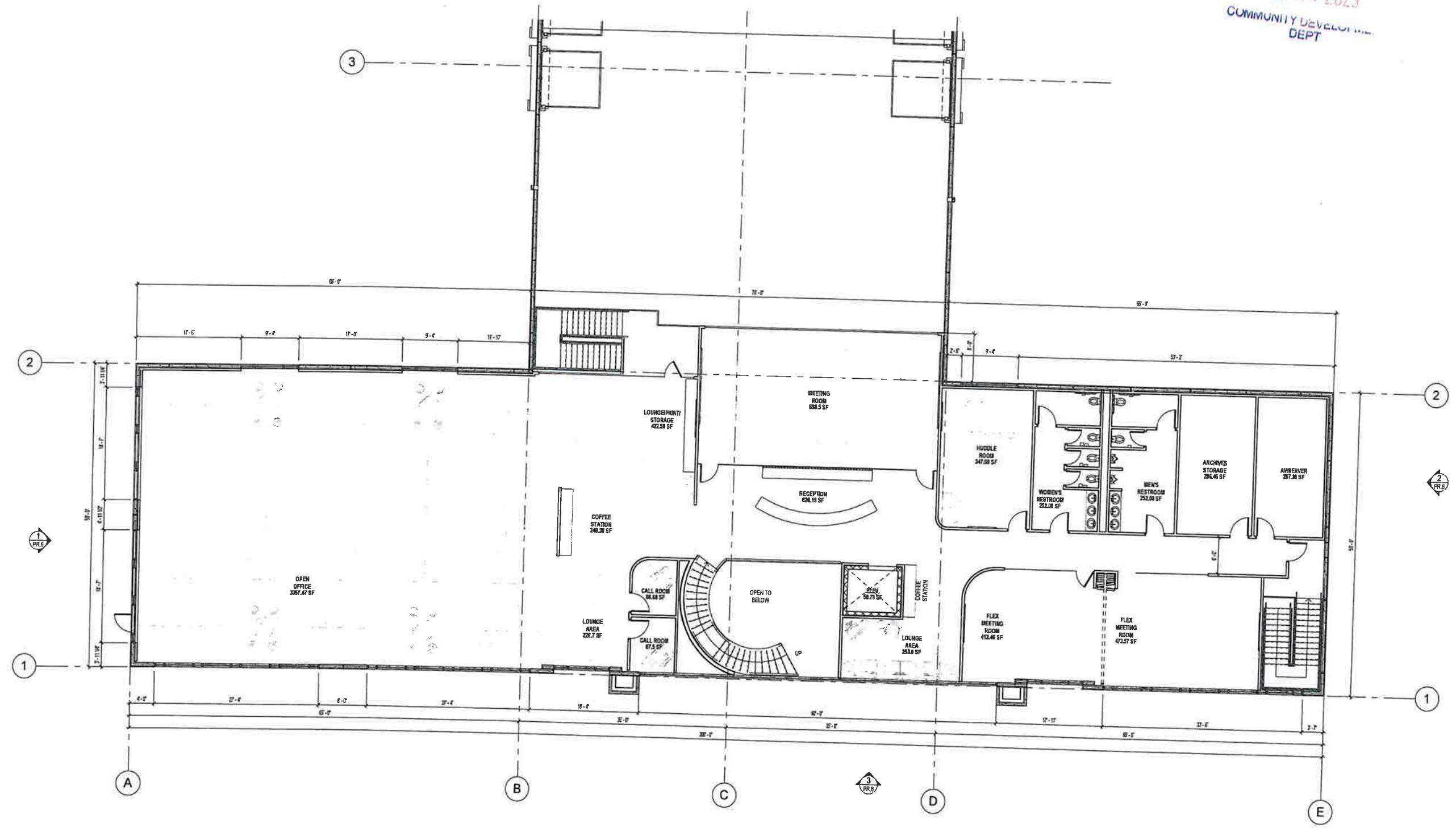


EXHIBIT G-4



MAINFREIGHT TERMINAL
 NORTH AVE & MORTON RD.
 Carol Stream, IL 60185

SCALE - 1/8" = 1'-0"

ENLARGED OFFICE FLOOR PLAN - LEVEL 2



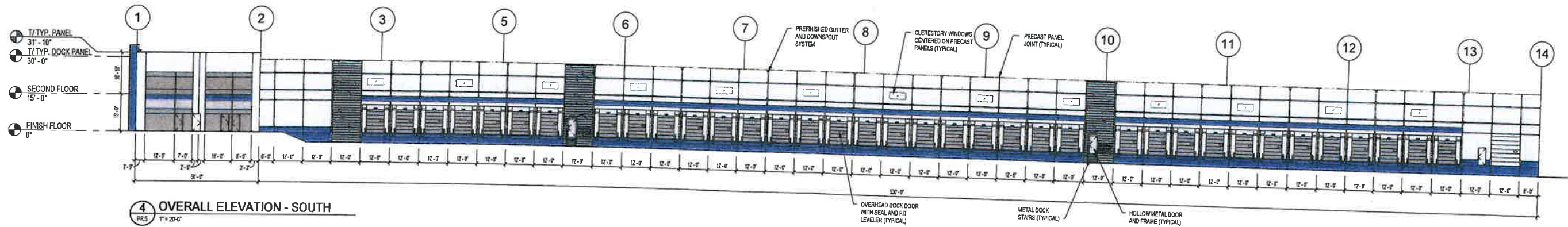
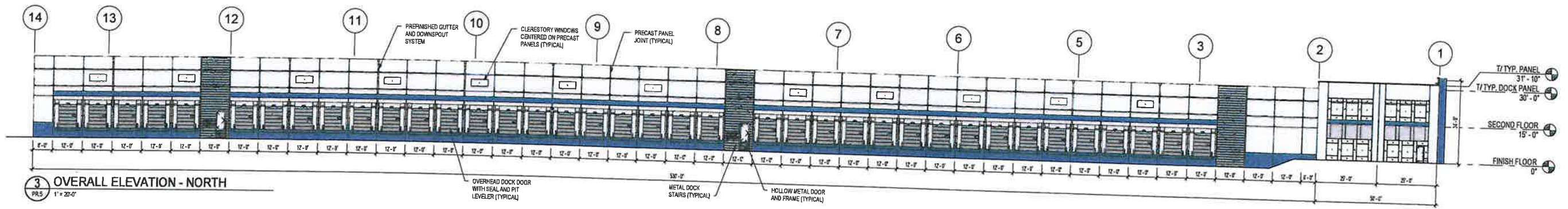
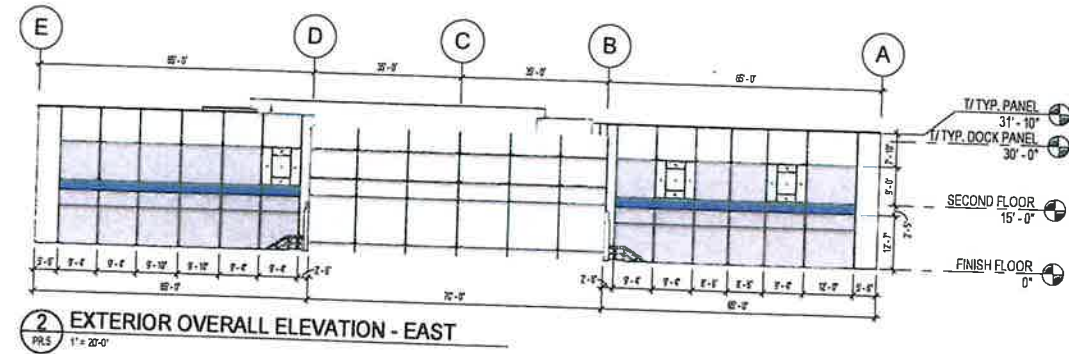
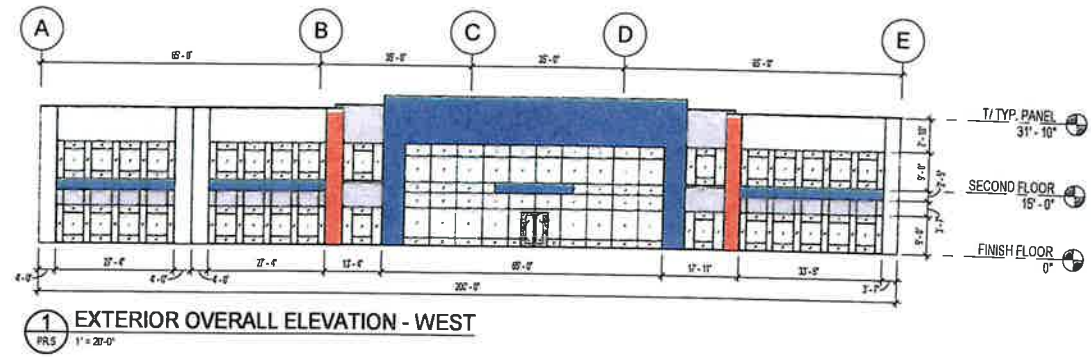
(24x36) GA1271 08-21-2023

PR.4



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 AUG 21 2021
 COMMUNITY DEVELOPMENT
 DEPT

EXHIBIT H-1



MAINFREIGHT TERMINAL
 NORTH AVE. & MORTON RD.
 Carol Stream, IL 60185

SCALE - 1" = 20'-0"

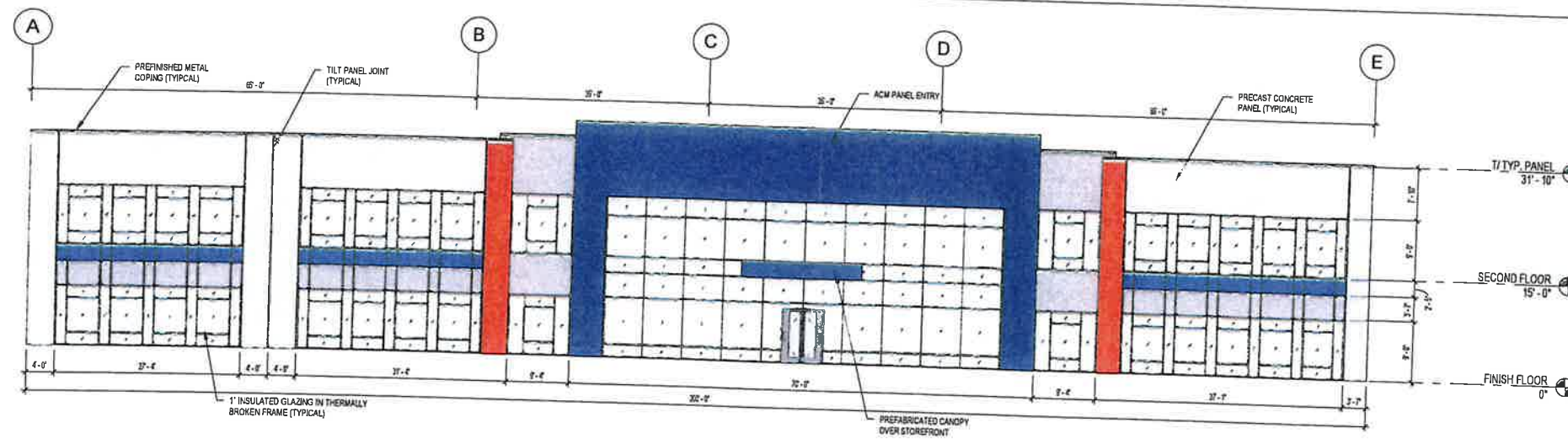
OVERALL EXTERIOR ELEVATIONS

[26x36] GA1271 06-21-2023

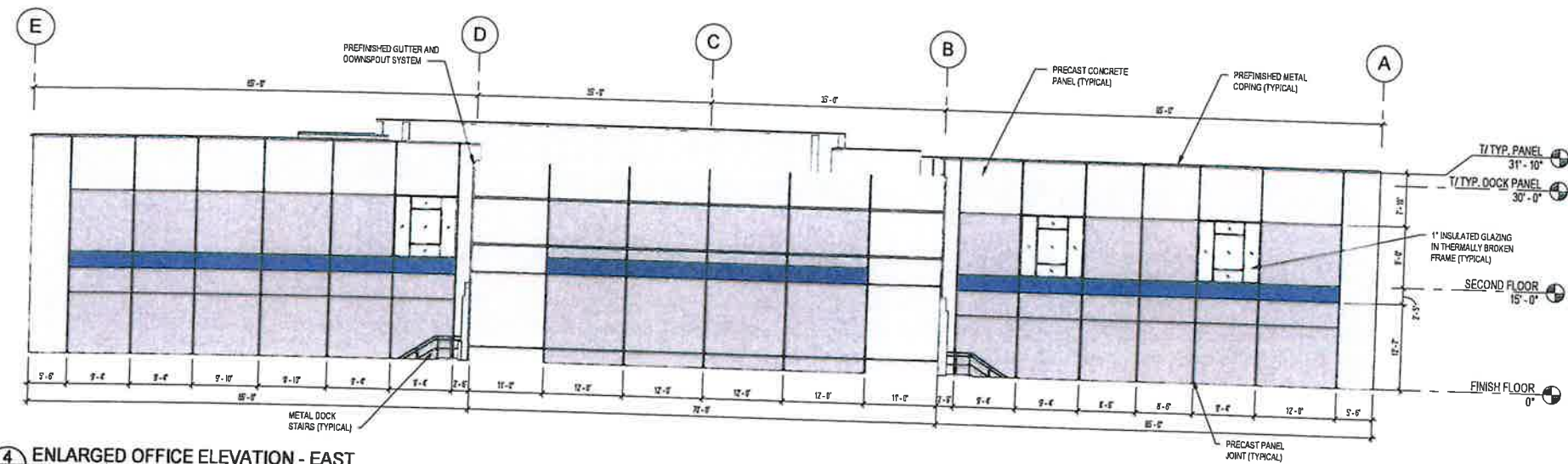
PR.5



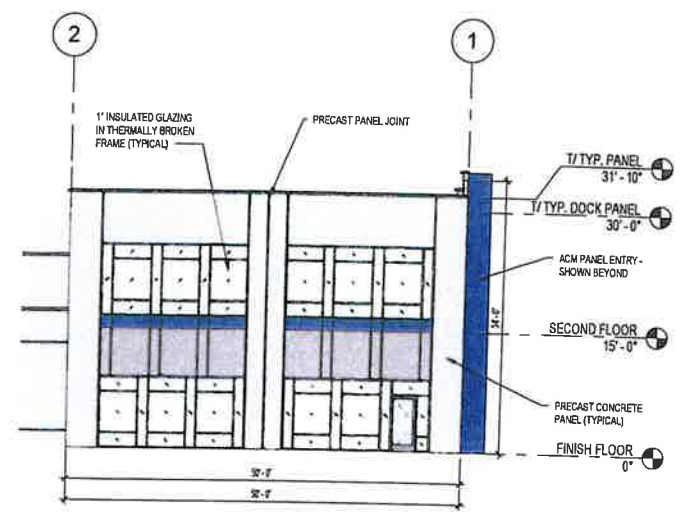
RECEIVED
 AUG 21 2023
 COMMUNITY DEVELOPMENT
 DEPT



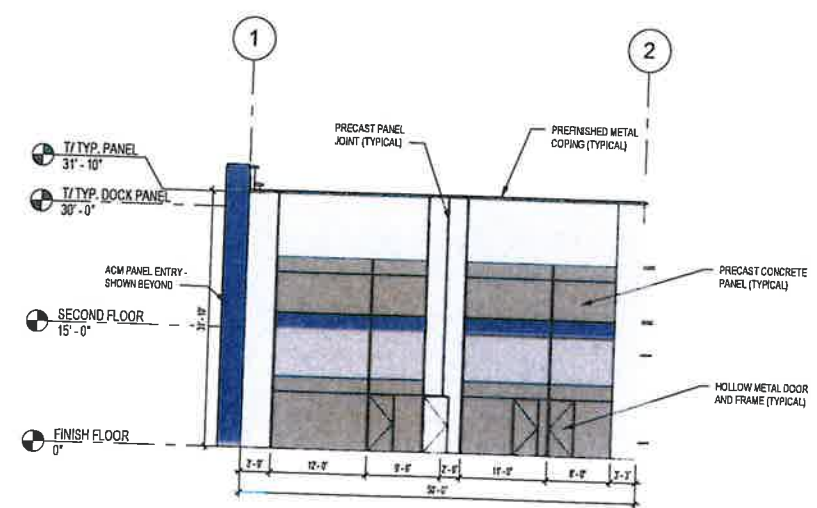
3 ENLARGED OFFICE ELEVATION - WEST
 PR.6 3/32" = 1'-0"



4 ENLARGED OFFICE ELEVATION - EAST
 PR.6 3/32" = 1'-0"



1 OFFICE ELEVATION - NORTH
 PR.6 3/32" = 1'-0"



2 OFFICE ELEVATION - SOUTH
 PR.6 3/32" = 1'-0"

EXHIBIT H-2



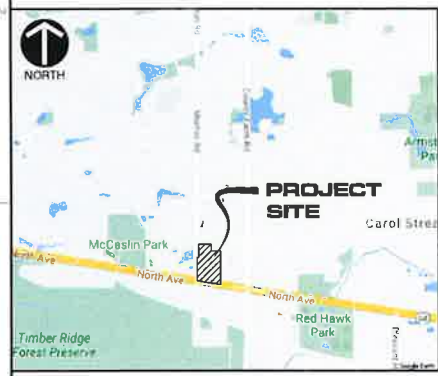
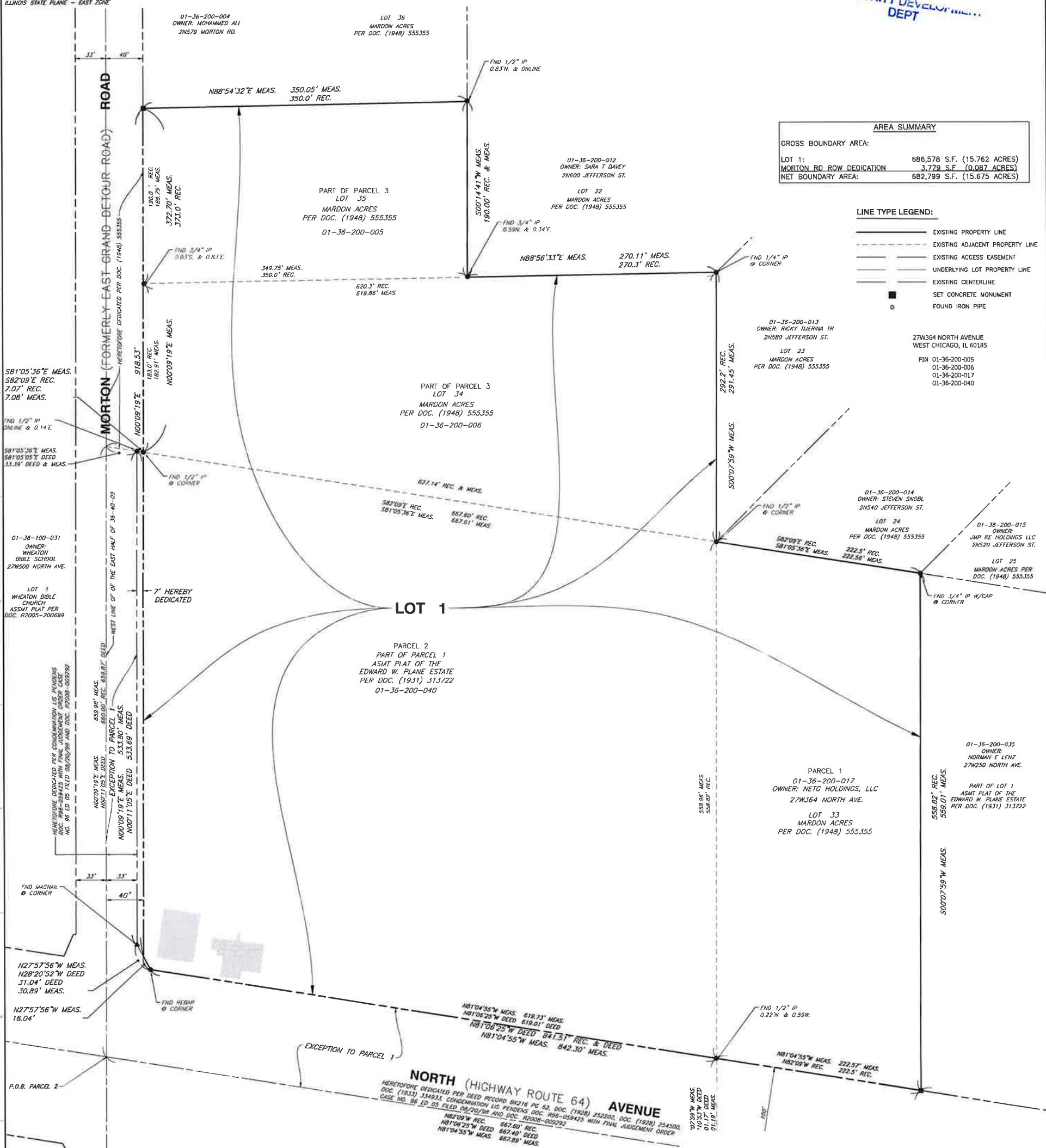
NORTH

BASIS OF BEARINGS:
ILLINOIS STATE PLANE - EAST ZONE

NORTH AVE TH CONSOLIDATION PLAT

OF
PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 09, EAST OF
THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS.

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SEP 14 2023
COMMUNITY DEVELOPMENT
DEPT



SURVEYOR'S NOTES:

- ALL MEASUREMENTS AND DISTANCES ARE SHOWN IN FEET AND DECIMAL PARTS THEREOF.
- IRON PIPES AT ALL LOT CORNERS (UNLESS NOTED OTHERWISE).
- DENOTES CONCRETE MONUMENT SET.
- 3/4" I.D. X 24" LONG IRON PIPE PLACED AT THE POSITIONS NOTED IN #2 TO CONFORM TO ILLINOIS COMPILED STATUTES CHAPTER 765 ILCS 205/1 REGARDING PLACEMENT OF MONUMENTS.
- SEE SURVEYOR'S CERTIFICATE FOR FEMA FLOOD MAP INFORMATION.
- THE SURVEYED PROPERTY IS LOCATED WITHIN THE CORPORATE LIMITS OF THE CITY OF WHEATON.
- EASEMENTS TO BE GRANTED HAVE BEEN INTENTIONALLY BEEN OMITTED. A PLAT OF EASEMENT FOR THE SUBJECT SITE WILL BE PREPARED GRANTING REQUIRED EASEMENTS IF DEEMED NECESSARY.
- TAXING BODIES ARE AS FOLLOWS:
CAROL STREAM FIRE
GRADE SCHOOL DISTRICT 25
HIGH SCHOOL DISTRICT 94
COLLEGE OF DUPAGE 502
COUNTY OF DUPAGE
DUPAGE AIRPORT AUTHORITY
FOREST PRESERVE DISTRICT
WAYNE TOWNSHIP
WAYNE TOWNSHIP ROAD
WAYNE TOWNSHIP SPC POLICE

EXHIBIT I-1

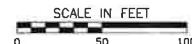


Civil & Environmental Consultants, Inc.
1230 East Diehl Road, Suite 200 - Naperville, IL 60563
Ph: 630.963.6026 - 877.963.6026 - Fax: 630.963.6027
www.ccecinc.com

PLAT OF SUBDIVISION

Situate in
27W364 NORTH AVENUE
WEST CHICAGO, ILLINOIS 60185
Made For
TIMBER HILL
8770 W BRYN MAWR
CHICAGO, IL 60631

NO	DATE	DESCRIPTION
1	08/10/2023	REVISION FOR VILLAGE COMMENTS



DATE:	08/10/2023	SCALE:	1" = 50'	DRAWING NO.:	SV01
DRAWN BY:	BTR	CHECKED BY:	DRAFT	SHEET	1 OF 2
PROJECT NO.:	334-443	APPROVED BY:	DRAFT		

RECEIVED

SEP 14 2023

REVISION RECORD		
NO.	DATE	DESCRIPTION
1	09/13/2023	REVISED PER VILLAGE COMMENTS

OWNER'S CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

THIS IS TO CERTIFY THAT THE UNDERSIGNED IS/ARE THE OWNER/OWNERS OF THE LAND DESCRIBED IN THE ANNEXED PLAT, AND HAS/HAVE CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED, AS INDICATED THEREON, FOR THE USES AND PURPOSES THEREIN SET FORTH, AND DOES/DO HEREBY ACKNOWLEDGE AND ADOPT THE SAME UNDER THE STYLE AND TITLE THEREON INDICATED. THE UNDERSIGNED HEREBY DEDICATES/DEDICATE FOR PUBLIC USE THE LAND SHOWN ON THIS PLAT FOR THOROUGHFARES, STREETS, ALLEYS, EASEMENTS, DRAINAGE AND PUBLIC SERVICES; AND HEREBY ALSO RESERVES/RESERVE AND GRANTS/GRANT TO THE VILLAGE OF CAROL STREAM, AND THE UTILITY COMPANIES OPERATING NOW OR IN THE FUTURE UNDER A FRANCHISE FROM THE VILLAGE, THE UTILITY EASEMENTS, WHICH ARE SHOWN ON THE PLAT OR STATED ON THEIR STANDARD FORM WHICH IS ATTACHED HERETO.

NAME/TITLE

ADDRESS

DATED THIS ___ DAY OF _____, 20__.

NOTARY CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

I, _____ A NOTARY PUBLIC IN AND FOR THE SAID COUNTY IN THE STATE AFORESAID, DO HEREBY CERTIFY THAT

PRINT NAME _____ AND _____ OF SAID
PRINT NAME _____ TITLE _____
PRINT NAME _____ TITLE _____

OWNER, WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS, WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT, AS SUCH

TITLE _____ AND _____ TITLE _____

RESPECTFULLY APPEARED BEFORE ME THIS DAY IN PERSON AND JOINTLY AND SEVERALLY ACKNOWLEDGE THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT AND AS THE FREE AND VOLUNTARY ACT OF SAID OWNER FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL

THIS ___ DAY OF _____, 20__.

NOTARY PUBLIC SIGNATURE

PRINT NAME

MY COMMISSION EXPIRES ON _____, 20__.

VILLAGE CLERK CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

I, _____, VILLAGE CLERK OF THE VILLAGE OF CAROL STREAM, ILLINOIS, HEREBY

CERTIFY THAT THIS PLAT WAS PRESENTED TO AND BY RESOLUTION DULY APPROVED BY THE BOARD OF TRUSTEES OF THE VILLAGE AT ITS MEETING HELD ON _____, 20__, AND THAT THE REQUIRED BOND OR OTHER GUARANTEE HAS EITHER BEEN POSTED OR ADEQUATE PROVISION HAS BEEN MADE FOR SUCH BOND TO BE POSTED, FOR THE COMPLETION OF THE IMPROVEMENTS REQUIRED BY THE REGULATIONS OF THE VILLAGE.

IN WITNESS WHEREOF, I HAVE HEREUNTO SET MY HAND AND SEAL OF THE VILLAGE OF CAROL STREAM, ILLINOIS THIS ___ DAY OF _____, 20__.

MAYOR

VILLAGE CLERK

VILLAGE ENGINEER CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

I, _____, VILLAGE ENGINEER OF THE VILLAGE OF CAROL STREAM, ILLINOIS, HEREBY CERTIFY THAT THE LAND IMPROVEMENTS DESCRIBED IN THE ANNEXED PLAT AND THE PLANS AND SPECIFICATIONS THEREOF MEET THE MINIMUM REQUIREMENTS OF THE VILLAGE AND HAVE BEEN APPROVED BY ALL PUBLIC AUTHORITIES HAVING HAVING JURISDICTION THEREOF.

I DO FURTHER CERTIFY THAT THERE HAS BEEN FILED WITH ME AND I HAVE REVIEWED TOPOGRAPHICAL AND PROFILE STUDIES FILED WITH THIS PLAT.

DATED AT CAROL STREAM, DUPAGE COUNTY, ILLINOIS

THE ___ DATE OF _____, 20__.

VILLAGE ENGINEER

FINANCE DIRECTOR CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

I, _____, FINANCE DIRECTOR OF CAROL STREAM, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE BEEN APPORTIONED AGAINST THE TRACT OF LAND INCLUDED IN THE PLAT.

DATED AT CAROL STREAM, DUPAGE COUNTY, ILLINOIS THIS ___ DAY OF _____, 20__.

FINANCE DIRECTOR

PLAN COMMISSION CERTIFICATE

COMMUNITY DEVELOPMENT DEPT

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

APPROVED THIS ___ DAY OF _____, 20__.

VILLAGE OF CAROL STREAM PLAN COMMISSION

CHAIRPERSON

CERTIFICATE CONCERNING DRAINAGE

STATE OF ILLINOIS }SS
COUNTY OF DU PAGE }

THE UNDERSIGNED HEREBY CERTIFY THAT, TO THE BEST OF OUR KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATERS WILL NOT BE CHANGED BY THE CONSTRUCTION OF SUCH SUBDIVISION OR ANY PART THEREOF, OR THAT SUCH SURFACE WATER DRAINAGE WILL NOT BE CHANGED WITHOUT ADEQUATE PROVISION BEING MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS INTO PUBLIC AREAS OR DRAINS WHICH THE SUBDIVIDER HAS A RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL NOT BE DEPOSITED ON THE PROPERTY OF ADJOINING LAND OWNERS IN SUCH CONCENTRATIONS AS MAY CAUSE DAMAGE TO THE ADJOINING PROPERTY BECAUSE OF THE CONSTRUCTION OF THE SUBDIVISION. THE UNDERSIGNED OWNER OR DULY AUTHORIZED AGENT FURTHER ACKNOWLEDGES THE EXISTENCE OF THE ORDINANCES OF THE VILLAGE OF CAROL STREAM AND RESTRICTS THE FUTURE USE OF THE LAND HEREIN SUBDIVIDED IN THAT NO BUILDING PERMITS SHALL BE SOUGHT BY THE UNDERSIGNED OWNER OR AGENT OR THEIR SUCCESSORS IN INTEREST OR ISSUED BY THE VILLAGE FOR CONSTRUCTION OF SUCH LAND UNTIL AND UNLESS THE CONSTRUCTION AND THE CHANGES IN THE LAND BROUGHT ABOUT BY SUCH CONSTRUCTION AND TOPOGRAPHICAL CHANGE COMPLY WITH THE ORDINANCES OF THE VILLAGE RELATING TO SURFACE WATERS, DRAINAGE, WATER RETENTION AND DETENTION, INCLUDING THOSE ORDINANCES ASSURING THE CONSTRUCTION OF SUCH IMPROVEMENTS THROUGH THE POSTING OF SECURITY.

DATED THIS ___ DAY OF _____, A.D. 20__.

BY:

REGISTERED PROFESSIONAL ENGINEER

MORTGAGEE CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

_____ AS MORTGAGEE UNDER THE PROVISIONS OF A CERTAIN MORTGAGE DATED _____, A.D. 20__, AND RECORDED IN THE RECORDER OF DEEDS OFFICE OF _____ COUNTY, ILLINOIS ON THE ___ DAY OF _____, A.D. 20__ AS DOCUMENT NO. _____ DATE _____ MONTH _____ YEAR HEREBY CONSENTS TO AND APPROVES THE SUBDIVISION OF THE LAND AND THE GRANTING OF THE EASEMENT(S) DEPICTED HEREON.

DATED THIS ___ DAY OF _____, A.D. 20__.

PRINT MORTGAGEE NAME: _____

BY: _____ ATTEST: _____

ITS: _____ ITS: _____

NOTARY'S CERTIFICATE - MORTGAGEE

STATE OF ILLINOIS }SS
COUNTY OF _____ }

THE UNDERSIGNED, A NOTARY PUBLIC IN THE COUNTY AND STATE AFORESAID, DO HEREBY CERTIFY THAT

(NAME) _____ (TITLE) _____
OF _____ AND (NAME) _____

(TITLE) _____ OF _____ WHO ARE PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT AS SUCH

(TITLE) _____ AND (TITLE) _____ RESPECTIVELY, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT OF SAID _____, AS MORTGAGEE, FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND SEAL

THIS ___ DAY OF _____, 20__.

NOTARY PUBLIC

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS }SS
COUNTY OF DUPAGE }

I, DOUGLAS R. MCCINTIC, AN ILLINOIS LICENSED PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT I HAVE SURVEYED AND SUBDIVIDED THE FOLLOWING DESCRIBED PROPERTY AND THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION THEREOF. ALL DIMENSIONS ARE GIVEN IN FEET AND DECIMALS THEREOF.

PARCEL 1:
LOT 33 IN MARDON ACRES, BEING A SUBDIVISION OF PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 31, TOWNSHIP 40 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 4, 1948, AS DOCUMENT 555355, IN DUPAGE COUNTY, ILLINOIS.

PARCEL 2:
THAT PART OF PARCEL 1 IN THE ASSESSMENT PLAT OF THE EDWARD W. PLANE ESTATE, BEING A SUBDIVISION OF PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 31, TOWNSHIP 40 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHWEST CORNER OF SAID PARCEL 1, BEING A CROSS IN THE CENTER LINE OF STATE BOND ISSUE HIGHWAY ROUTE 64 ALSO KNOWN AS NORTH AVENUE, FOR A POINT OF BEGINNING, AND RUNNING THENCE NORTH ALONG THE CENTER LINE OF THE PUBLIC HIGHWAY KNOWN AS MORTON ROAD, FOR A DISTANCE OF 680.0 FEET; THENCE SOUTH 82 DEGREES 09 MINUTES EAST PARALLEL WITH THE CENTER LINE OF SAID NORTH AVENUE FOR A DISTANCE OF 667.60 FEET TO THE NORTHWEST CORNER OF LOT 33 IN MARDON ACRES RECORDED AS DOCUMENT 555355; THENCE SOUTH PARALLEL WITH THE WEST LINE OF THE EAST 1/2 OF SAID SECTION 36 AND BEING ALONG THE WEST LINE OF SAID LOT 33 AND SAID WEST LINE EXTENDED SOUTH, 660 FEET TO THE CENTER LINE OF NORTH AVENUE; THENCE NORTH 82 DEGREES 09 MINUTES WEST ALONG SAID CENTER LINE 667.60 FEET TO THE POINT OF BEGINNING, ACCORDING TO THE ASSESSMENT PLAT OF THE EDWARD W. PLANE ESTATE RECORDED JUNE 15, 1931 AS DOCUMENT 313722, (EXCEPTING THEREFROM THAT PART TAKEN BY CONDEMNATION IN CASE 96D-5), IN DU PAGE COUNTY, ILLINOIS.

PARCEL 3:
LOTS 34 AND 35 IN MARDON ACRES, BEING A SUBDIVISION OF PART OF SECTION 36, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 31, TOWNSHIP 40 NORTH, RANGE 10, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED OCTOBER 4, 1948, AS DOCUMENT 555355, IN DU PAGE COUNTY, ILLINOIS.

SAID DESCRIBED PROPERTY IS LOCATED WITHIN AN AREA HAVING A ZONE DESIGNATION "X", AREA OF MINIMAL FLOOD HAZARD, BY THE SECRETARY OF HOUSING AND URBAN DEVELOPMENT, ON FLOOD INSURANCE RATE MAP NUMBER 17043C0042I, WITH AN EFFECTIVE DATE OF AUGUST 1, 2019, FOR THE COMMUNITY NUMBER 170197, IN DUPAGE COUNTY, STATE OF ILLINOIS, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PROPERTY IS SITUATED. THIS PLAT HAS BEEN PREPARED BY CIVIL & ENVIRONMENTAL CONSULTANTS, INC., ILLINOIS LICENSED PROFESSIONAL DESIGN FIRM NO. 184.004002, LICENSE EXPIRES APRIL 30, 2025, UNDER MY PERSONAL DIRECTION FOR THE EXCLUSIVE USE OF THE CLIENT NOTED HEREON.

GIVEN UNDER MY HAND AND SEAL

THIS ___ DAY OF _____, 20__.

ILLINOIS LICENSED PROFESSIONAL LAND SURVEYOR NO. 035-002992 LICENSED VALID THROUGH NOVEMBER 30, 2024

C&E
Civil & Environmental Consultants, Inc.
1230 East Diehl Road, Suite 200 - Naperville, IL 60563
Ph: 630.963.6026 • 877.963.6026 • Fax: 630.963.6027
www.ccecinc.com

PLAT OF SUBDIVISION
Situates in
27W364 NORTH AVENUE
WEST CHICAGO, ILLINOIS 60185
Made For
TIMBER HILL
8770 BRYN MAWR SUITE 1350
CHICAGO, IL 606

DATE: CHICAGO, IL 60631	SCALE: 1"= 50'	DRAWING NO.
DRAWN BY: BTR	CHECKED BY: DRAFT	SV01
PROJECT NO: 334-443	APPROVED BY: DRAFT	SHEET 2 OF 2

EXHIBIT I-2

RECEIVED

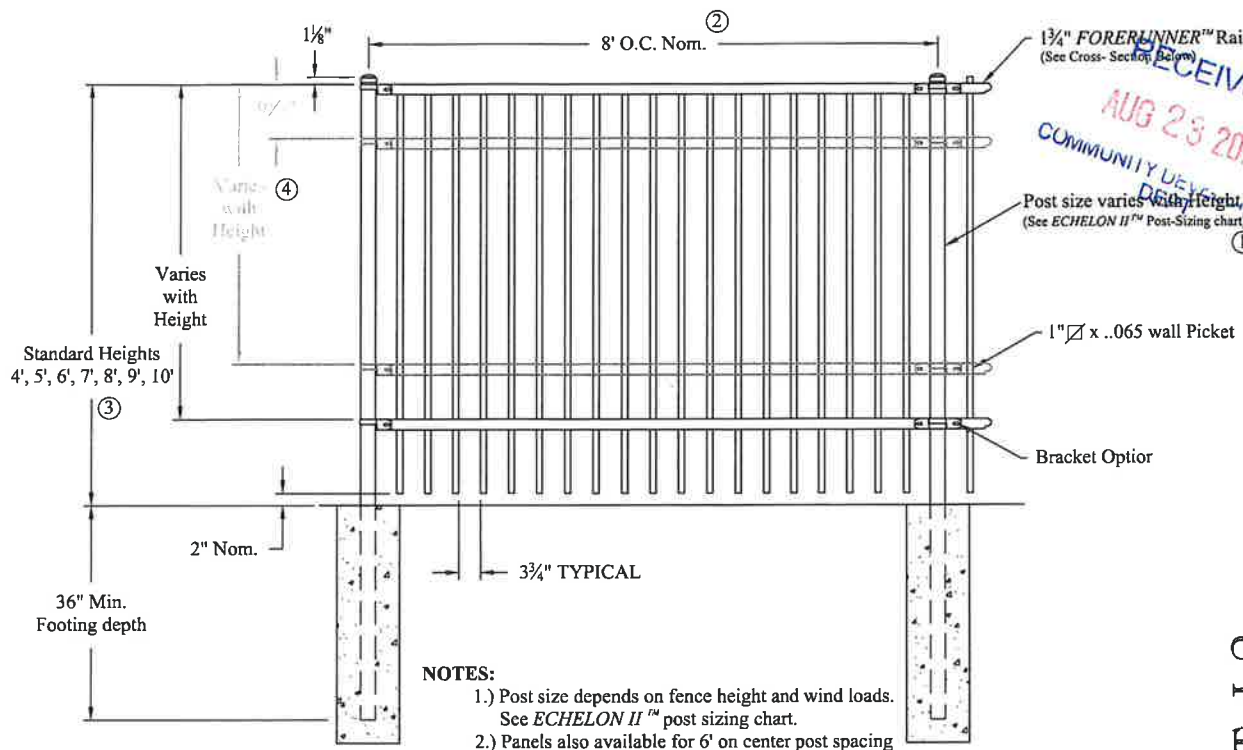
AUG 23 2023

COMMUNITY DEVELOPMENT



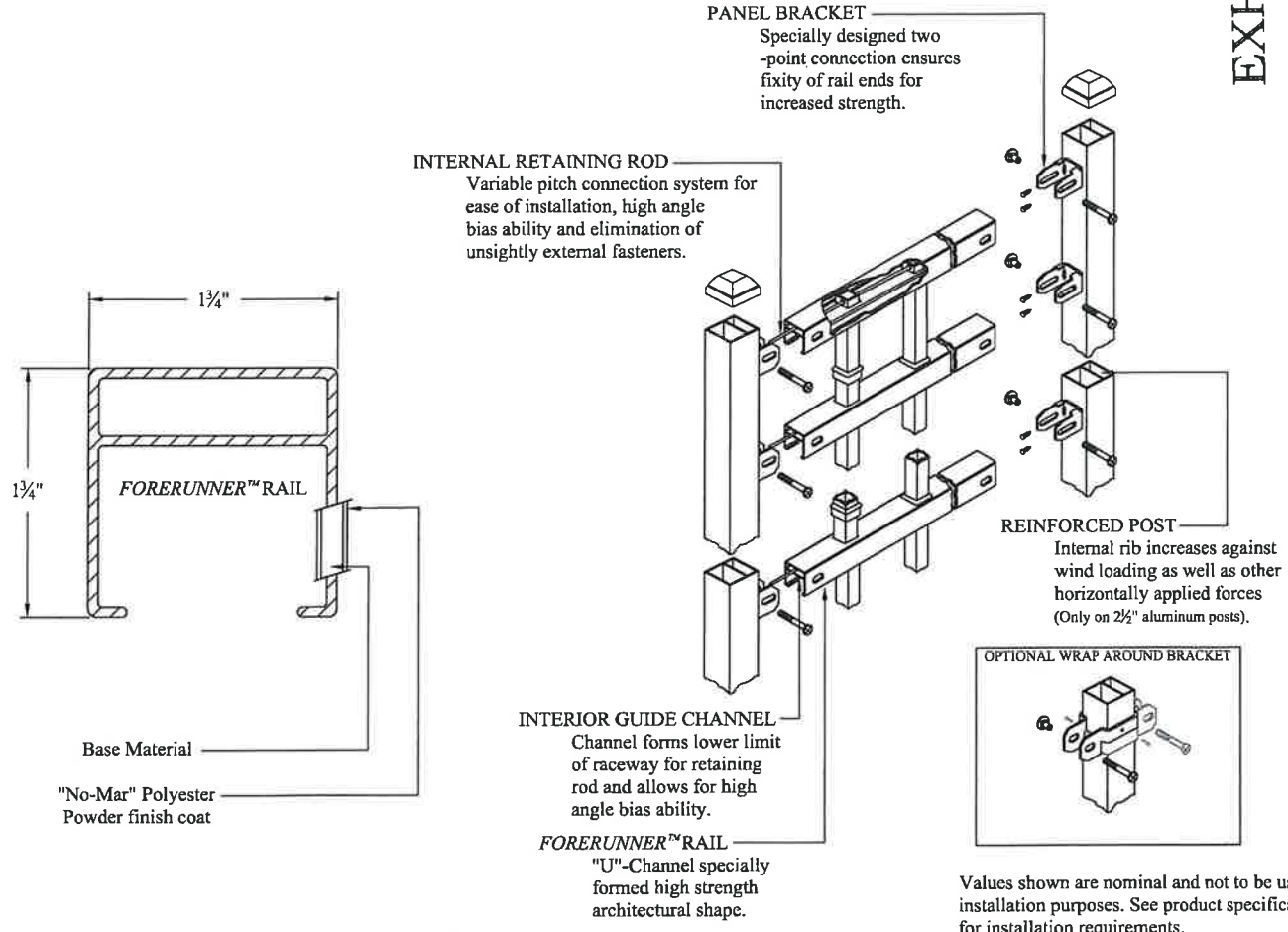
EXHIBIT J-1

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 AUG 23 2023
 COMMUNITY DEVELOPMENT DEPT.



- NOTES:**
- 1.) Post size depends on fence height and wind loads. See *ECHELON II™* post sizing chart.
 - 2.) Panels also available for 6' on center post spacing
 - 3.) Additional heights available on request. Some heights noted require a third and/or fourth rail.
 - 4.) Third and Fourth rail optional.

EXHIBIT J-2



Values shown are nominal and not to be used for installation purposes. See product specification for installation requirements.

INDUSTRIAL STRENGTH ALUMINUM

Title: ECHELON II MAJESTIC 2/3/4-RAIL		
DR: NJB	SH. 1 of 1	SCALE: DO NOT SCALE
CK: BS	Date 2-07-12	REV: e



1555 N. Mingo
 Tulsa, OK 74116
 1-888-333-3422
 www.ameristarfence.com



EXHIBIT K-1

TRANSPORT® TRAVERSE GATES

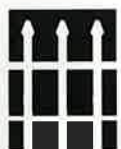
Engineered Cantilever Gate Systems

TransPort is a revolutionary cantilever gate system made up of three components: frame, facade and hardware.

The component design decreases the risk of damage during transit and does not require a team of installers or machinery to install the gate system. The cantilever gate can easily be installed with a standard three-man crew in about two hours.



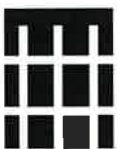
STYLES



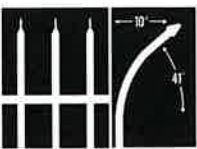
CLASSIC™



MAJESTIC™



GENESIS™



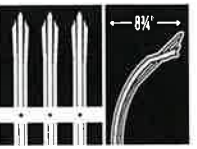
INVINCIBLE™



TRIDENT™



STRONGHOLD™



GAUNTLET™

NOTE:
Styles, colors
and adornments
are based upon
fence system.
Some options
not shown.

RECEIVED
SEP 19 2023
COMMUNITY DEVELOPMENT
DEPT

KNOCKDOWN GATE SYSTEM | ASSEMBLED ON-SITE | EASE OF TRANSPORT



TRANSPORT TRAVERSE II®

1" Ornamental Picket Design

Matches: Montage II | Echelon II | Aegis II



TRANSPORT TRAVERSE IS®

2.75" Security Pale Design

Matches: Impasse II



TRANSPORT ESTATE®

.75" Decorative Arched Picket Design

Matches: Aluminum Estate Entry Gates

*Only available in TransPort.

UPRIGHTS 2.5" x 16GA PRE-GALV STEEL

DIAGONAL SUPPORTS 2" x 16GA PRE-GALV STEEL

BOTTOM TRACK 3.75" x 6.5" ALUMINUM PROFILE

TOP TRACK 3" x 5.5" ALUMINUM PROFILE

HEIGHT 4'-10'

OPENINGS 6'-30'



EXHIBIT K-2

HARDWARE

Unlike other engineered cantilever gates, TransPort gates greater than 24' openings come equipped with four internal trollies that distribute the weight of the gate as it is opened and closed.

TRANSPORT LINK®

Chain Link Design also available. See ameristarperimeter.com for details.

*Only available in TransPort.

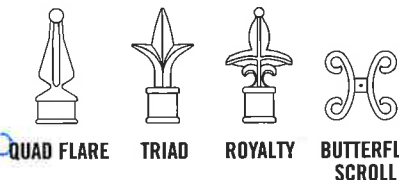
PRIMARY APPLICATIONS

- Water Treatment & Storage
- Municipal Facilities
- Government Facilities
- Airports
- Manufacturing Plants
- Petro & Chemical Facilities
- Power Plants & Substations
- Schools & Universities
- Stadiums & Event Centers

COLORS



ADORNMENTS



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SEP 19 2023

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AUG 15 2023

Traffic Impact Study Proposed Freight Terminal Facility

Unincorporated DuPage County, Illinois



March 15, 2023

1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed freight terminal facility located in the northeast quadrant of the intersection of North Avenue with Morton Road in Unincorporated DuPage County, Illinois. As proposed, the site (which is currently occupied by a truck parking facility) will be redeveloped to provide a freight terminal facility consisting of an approximately 47,100 square-foot building, 92 light vehicle parking spaces, 44 tractor parking spaces, 86 trailer parking spaces, and 81 loading docks. Access to the site will continue to be provided via Morton Road.

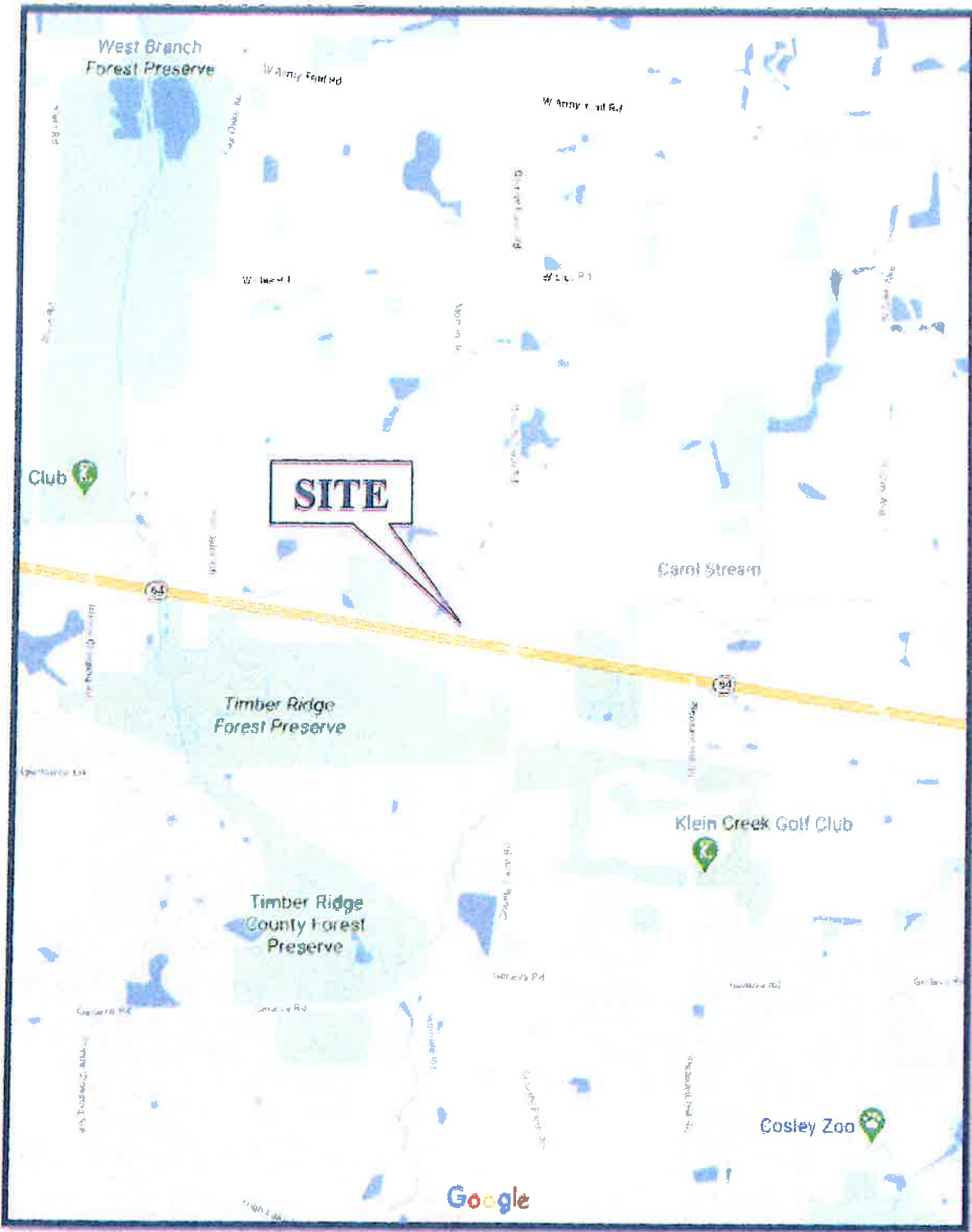
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed facility will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate traffic generated by the proposed facility. **Figure 1** shows the location of the site in relation to the area roadway system, **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed facility
- Directional distribution of the facility traffic
- Vehicle trip generation for the facility
- Future traffic conditions including access to the facility
- Traffic analyses for the weekday morning and weekday evening peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning and weekday evening peak hours for the following conditions:

1. Existing Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area.
2. No-Build Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes increased by an ambient area growth not attributable to any particular facility.
3. Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the existing traffic volumes, ambient area growth not attributable to any particular development, and the net increase in traffic estimated to be generated by the proposed facility.



Site Location

Figure 1



Aerial View of Site

Figure 2

2. Existing Conditions

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future conditions. The following provides a description of the geographical location of the site, physical characteristics of the area roadway system including lane usage and traffic control devices, and existing peak hour traffic volumes.

Site Location

The site is located in the northeast quadrant of the intersection North Avenue with Morton Road, is bounded by commercial uses to the east, North Avenue to the south, and Morton Road to the west, and is currently occupied by a truck parking facility. Land uses in the vicinity of the site include single family residences to the north, DuPage Water Conditions, Smoke Vapor, Dunkin Donuts and a strip retail development to the east, North Avenue Pub & Grill and Affinity Auto Sales to the south, a warehouse-distribution facility to the southwest, and Wheaton Bible Church to the west.

Existing Roadway System Characteristics

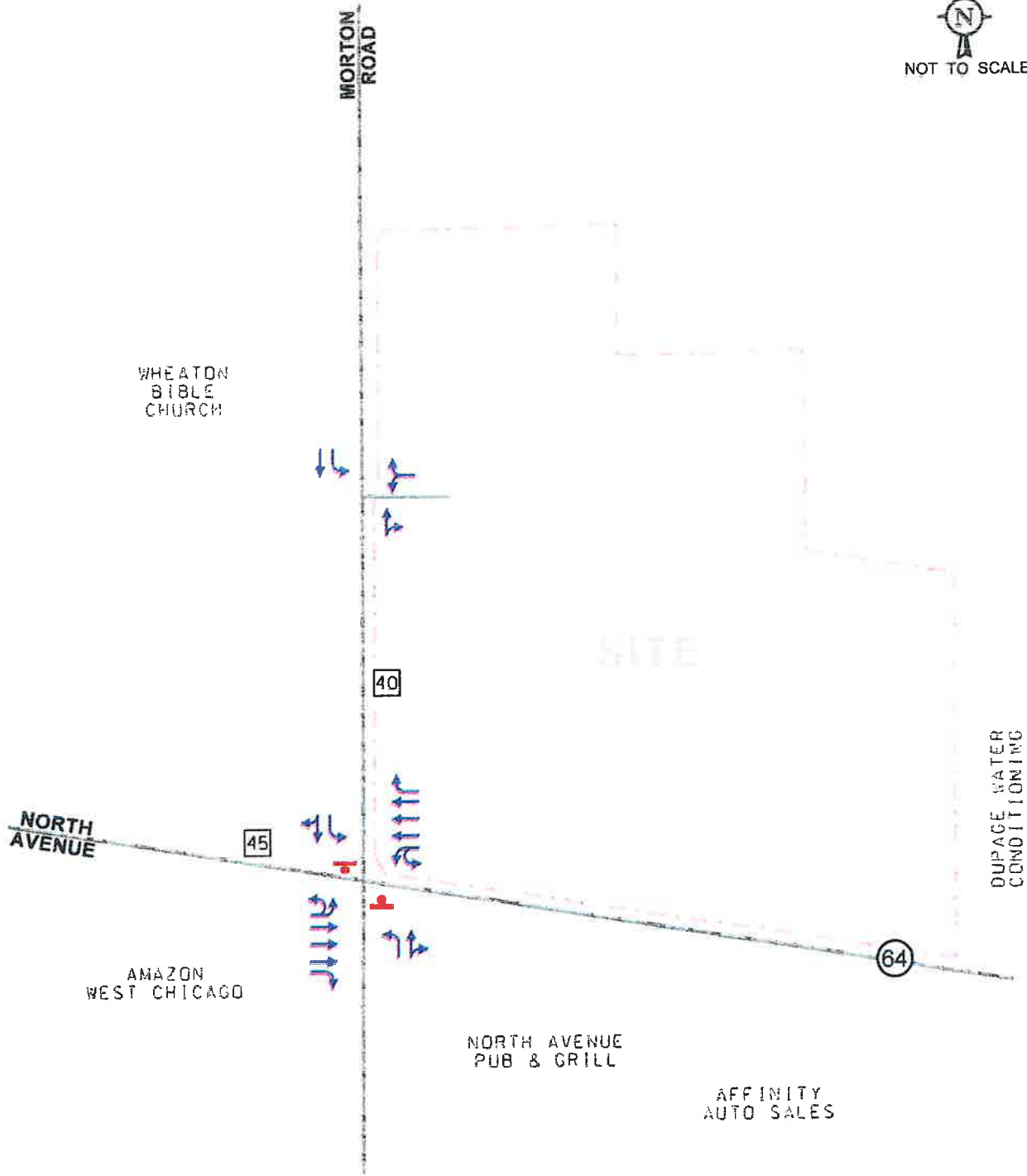
The characteristics of the existing roadways near the site are described below and illustrated in **Figure 3**.

North Avenue (Illinois Route 64) is an east-west arterial roadway that generally provides three lanes in each direction divided by a landscaped median. The median is wide enough to provide refuge for left-turn and U-turn movements as well as northbound and southbound through movements. At its unsignalized intersection with Morton Road, North Avenue provides exclusive left-turn and right-turn lanes on both approaches. North Avenue is under the jurisdiction of the Illinois Department of Transportation (IDOT), carries an Average Annual Daily Traffic (AADT) volume of 35,000 vehicles (IDOT AADT 2021), of which 4,050 vehicles are trucks, and has a posted speed limit of 45 mph. It should be noted that North Avenue is designated as a Strategic Regional Arterial (SRA) route. SRA routes are intended to carry larger volumes of traffic including industrial, commercial and passenger vehicles as a complement to the region's expressway system. Furthermore, North Avenue is the main arterial roadway through the central part of DuPage county that serves the existing industrial warehouse facilities along the roadway, connecting the City of St. Charles to the I-355 expressway.

Morton Road is a north-south collector roadway generally providing one lane in each direction separated by a two-way left-turn lane, that extends between Lies Road to the north and the Glasshagel Fields access drive to the south. Morton Road is under stop sign control at its intersection with North Avenue, where the northbound and southbound approaches provide a striped left-turn lane and a shared through/right-turn lane. Morton Road south of North Avenue is under the jurisdiction of the Village of Carol Stream. North of North Avenue, Morton Road carries an AADT volume of 1,750 vehicles (IDOT 2020) and has a posted speed limit of 40 miles per hour.



NOT TO SCALE



LEGEND

-  - TRAVEL LANE
-  - STOP SIGN
-  - SPEED LIMIT

Existing Traffic Volumes

In order to determine current traffic conditions within the study area, KLOA, Inc. conducted peak period traffic counts at the intersection of North Avenue with Morton Road. The traffic counts were conducted on Tuesday, March 7, 2023 during the weekday morning (6:00 A.M. to 9:00 A.M.) and weekday evening (3:00 P.M. to 6:00 P.M.) peak periods. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:15 A.M. to 8:15 A.M. and the weekday evening peak hour of traffic occurs from 4:30 P.M. to 5:30 P.M.

Traffic counts were also conducted at the existing access drive serving the truck parking facility off Morton Road to determine the trip generation of the facility during the weekday morning and weekday evening peak hours. The results of the traffic counts indicated that the facility generated 11 total trips during the weekday morning peak hour (five inbound and six outbound) and 16 total trips during the weekday evening peak hour (nine inbound and seven outbound). Of the total trips generated by the existing facility, two trips and eight trips during the weekday morning and weekday evening peak hours, respectively, were trucks.

Figure 4 illustrates the existing traffic volumes inclusive of truck traffic. **Figure 5** illustrates the existing semi-trailer truck traffic volumes. Copies of the traffic count summary sheets are included in the Appendix.

Crash Analysis

KLOA, Inc. obtained crash data¹ for the most recent available past five years (2017 to 2021) for the intersection of North Avenue with Morton Road. The crash data for the intersection is summarized in **Table 1**. A review of the crash data indicated that no fatalities were reported at the intersection between 2014 and 2018.

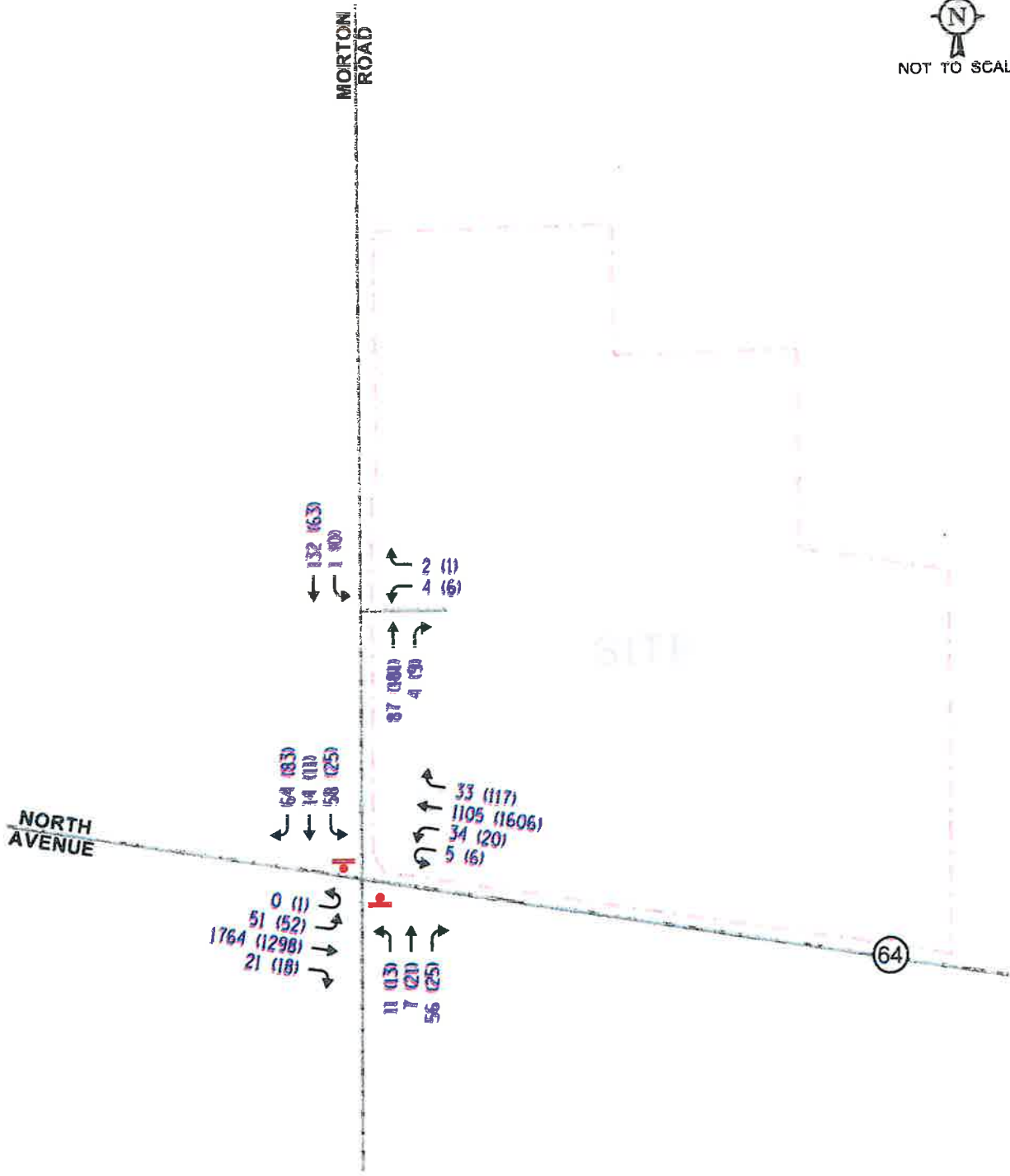
Table 1
NORTH AVENUE WITH MORTON ROAD – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2017	0	0	0	2	0	2	0	4
2018	1	0	1	0	0	2	0	4
2019	0	0	0	1	0	2	0	3
2020	0	0	0	1	1	1	0	3
2021	<u>1</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3</u>	<u>0</u>	<u>4</u>
Total	2	0	1	4	1	10	0	18
Average	< 1	0	< 1	< 1	< 1	2	0	3.6

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s).



NOT TO SCALE



LEGEND

- 00 - AM PEAK HOUR (7:15-8:15 AM)
- (00) - PM PEAK HOUR (4:30-5:30 PM)

Freight Terminal Facility
Unincorporated DuPage
County, Illinois

Existing Traffic Volumes

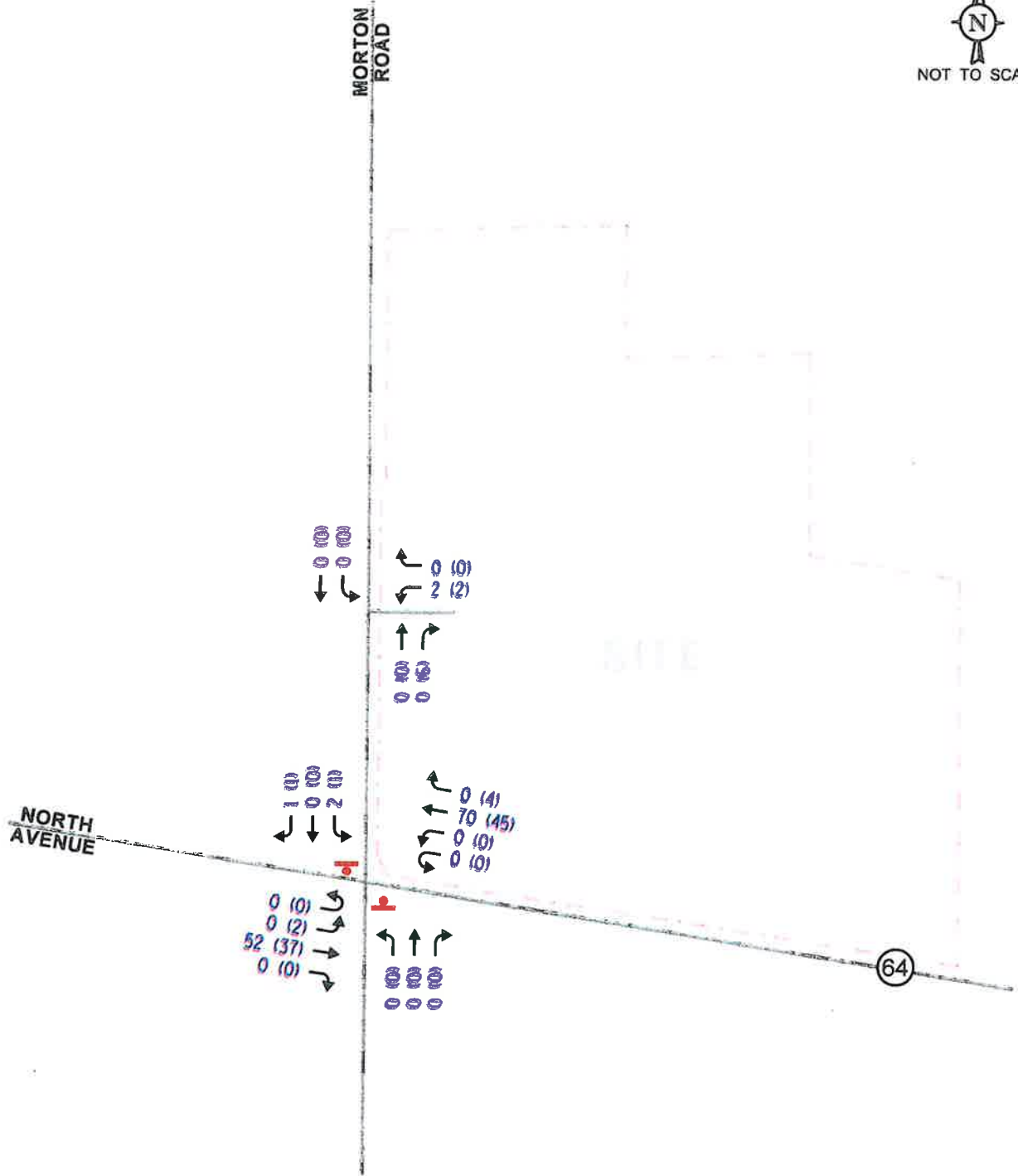


Job No: 23-068

Figure: 4



NOT TO SCALE



LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:30-5:30 PM)

3. Traffic Characteristics of the Proposed Facility

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed facility, including the directional distribution and volumes of traffic that it will generate.

Proposed Site and Facility Plan

As proposed, the site will be redeveloped to provide a freight terminal consisting of an approximately 47,100 square-foot building and 81 loading docks of which 40 docks will be located on the south side of the building and 41 docks will be located on the north side of the building. A total of 92 parking spaces will be provided for light/passenger vehicles and these parking spaces will be located on the west side of the building. The northerly portion of the site will provide 44 parking spaces for tractors and 86 parking spaces for trailers.

Access to the facility will be provided via a proposed full movement access drive on Morton Road located approximately 635 feet north of North Avenue (centerline-to-centerline). This access drive will provide one inbound lane and one outbound lane and outbound movements should be under stop-sign control. It should be noted that this access drive will be the relocation of the existing full movement access drive serving the site approximately 170 feet to the north.

Additional access to the facility will be provided via a proposed access drive on Morton Road located approximately 180 feet north of North Avenue (centerline-to-centerline). Overall, it is recommended that this access drive be restricted via channelization to northbound right-turn in movements only based on the following:

- Due to the proximity of the drive to North Avenue and the operations of the North Avenue with Morton Road intersection (as discussed later), there will be times when a westbound left-turn from the access drive onto Morton Avenue will be difficult, particularly by tractor-trailer vehicles.
- As discussed later, only a small percentage traffic is projected to arrive to and depart the site from the north on Morton Road and it is recommended that this traffic be restricted to light/passenger vehicles only. With the provision of a full access further to the north on Morton Road, accommodating southbound left-turns in is not necessary at this location.
- Given that this access drive will be utilized by tractor-trailer vehicles, any channelization with an island will need to be mountable and as such, the left-turn out restriction for passenger vehicles will be difficult to enforce.

However, the provision of a right turn in access drive at this location will be beneficial as it will allow for site traffic to access the site just north of North Avenue, minimizing the volume of traffic on Morton Road. Additionally, it will allow trucks to access the southerly loading docks directly, minimizing the volume of truck activity on the north side of the proposed building.

To help enforce the right-in only access restriction, “Do Not Enter” signs should be placed at the connection of the access drive to the passenger vehicle parking lot and where the access drive connects to the paved area serving the loading docks.

Due to the residential nature of Morton Road to the north of the site, all truck traffic should be restricted to/from North Avenue. This restriction should be enforced via signage as well as by designing the northeast curb radii at the full movement access drive to physically prohibit trucks vehicles from making a westbound to northbound right-turn movement.

A copy of the proposed site plan illustrating the site and proposed access locations is included in the Appendix.

Proposed Facility Operations

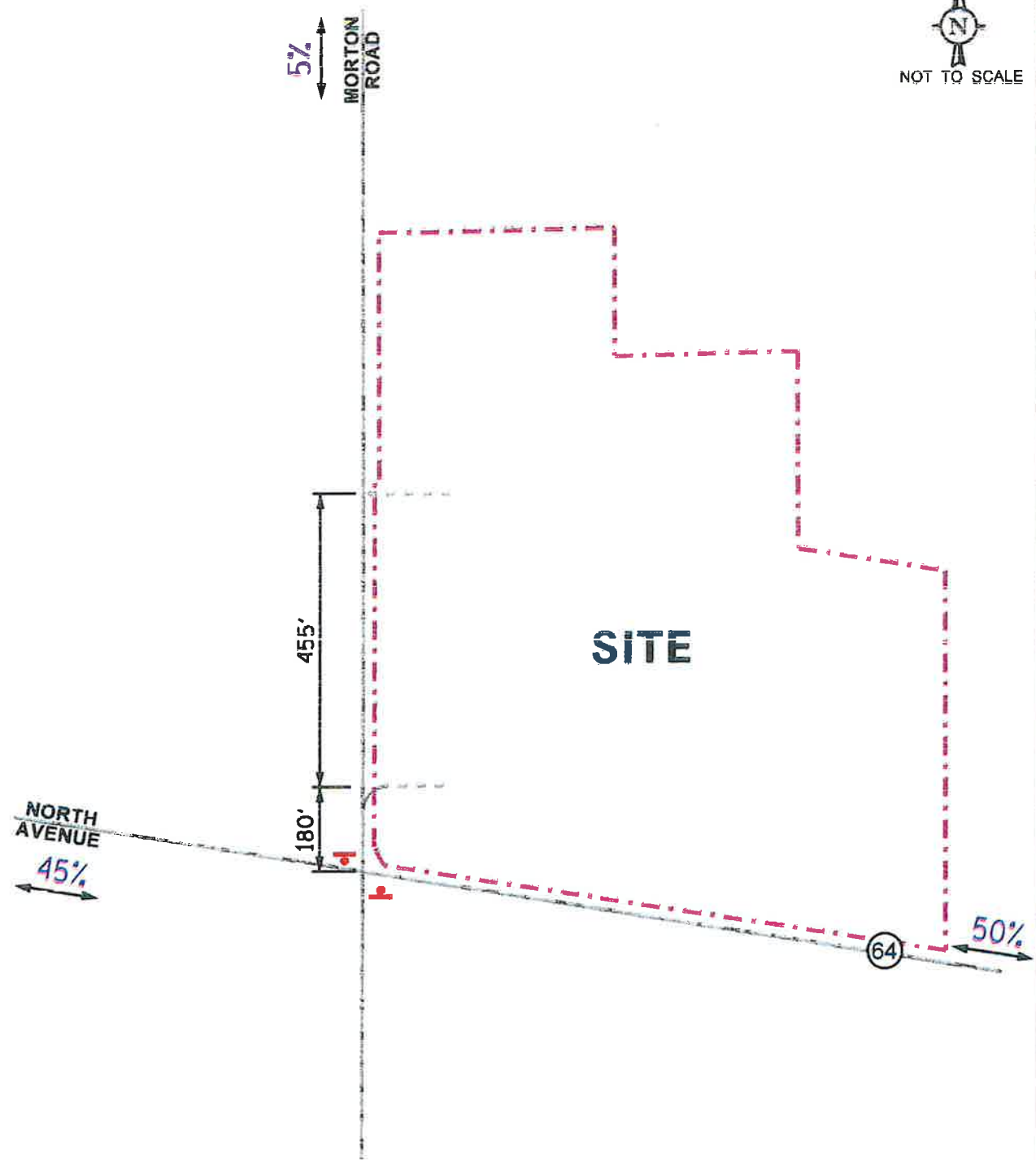
As proposed, the facility will operate seven days a week, with 24-hour operation Monday through Friday. During a typical peak weekday, it is anticipated that the facility will see approximately 20 trucks daily that make local pick-up and deliveries to the Chicagoland Area. Trucks typically departing in the morning between 5:00 A.M. and 8:30 A.M. and returning at various times throughout the day. On-site employees arrive between 6:00 A.M. and 9:00 A.M. and depart between 5:00 P.M. and 10:00 P.M. There are approximately 50 to 60 employees anticipated to be on site at any given time. During off peak times, the facility is estimated to have between 70 and 80 inbound/outbound line-haul trucks per week. The outbound trucks generally depart between the hours of 10:00 P.M. and 3:00 A.M. with the inbound trucks arriving between 4:00 A.M. and 11:00 A.M.

Directional Distribution

The directions from which traffic generated by the proposed facility will approach and depart the site were estimated based on existing travel patterns, as determined from the traffic counts. **Figure 6** illustrates the directional distribution of the facility-generated traffic. As indicated earlier, trucks will not be permitted to travel to and from the north on Morton Road



NOT TO SCALE



LEGEND

- 00% - PERCENT DISTRIBUTION
- 00' - DISTANCE IN FEET

Facility Traffic Generation

The total number of vehicle trips to be generated by the proposed facility was estimated based on information provided by the operator of the proposed facility, as previously described. It was conservatively assumed that 50 percent of the truck and employee passenger vehicle activity occurs during the weekday morning and weekday evening peak hour. However, as previously indicated, these activities are dispersed over a three or more-hour period and thus result in a more evenly distribution of the arrival and departure of traffic. **Tables 2** summarizes the estimated trip generation for the facility. It should be noted that not all of the vehicle trips generated by the proposed facility will be new to the area, as the proposed facility will be replacing an existing truck parking facility that is currently generating traffic.

Table 2
ESTIMATED PEAK HOUR AND DAILY TRIP GENERATION – OPERATOR DATA

Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour			Daily Trips	
	In	Out	Total	In	Out	Total	In	Out
Truck Terminal	35	10	45	10	35	45	80	80
Truck Trips	5	5	10	5	5	10	20	20
Passenger Vehicle Trips	30	5	35	5	30	35	60	60

4. Projected Traffic Conditions

The total projected traffic volumes include the existing traffic volumes, increase in background traffic due to growth, and the traffic estimated to be generated by the proposed subject facility.

Facility Traffic Assignment

The estimated weekday morning and weekday evening peak hour traffic volumes that will be generated by the proposed facility were assigned to the roadway system in accordance with the previously described directional distribution (Figure 6). **Figure 7** illustrates the traffic assignment of the expansion generated trips. As previously recommended, all traffic to/from the north on Morton Road is representative a passenger vehicle with all trucks traveling to/from North Avenue.

Background Traffic Conditions

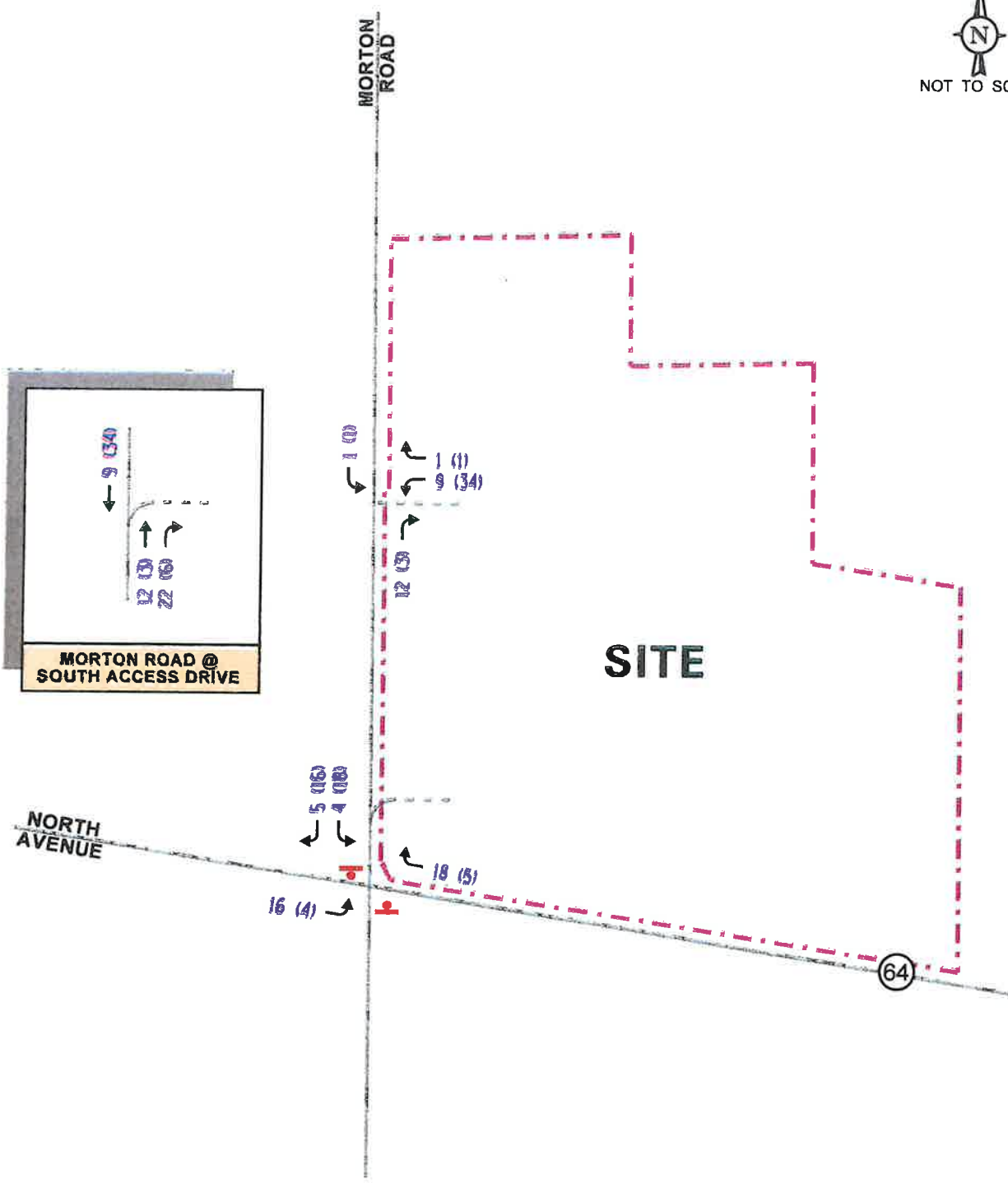
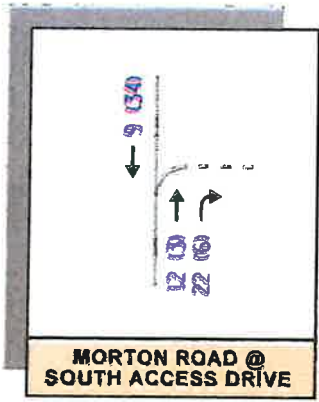
The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on ADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP) in a letter, the existing traffic volumes are projected to increase by a compound annual growth rate of 0.22 percent per year. As such, traffic volumes were increased by approximately 1.3 percent total to represent Year 2029 conditions (one-year buildout plus five years). A copy of the CMAP projections letter is included in the Appendix. The Year 2029 no-build traffic volumes are illustrated in **Figure 8**.

Total Projected Traffic Volumes

The facility-generated traffic (Figure 7) was added to the year 2029 no-build volumes (Figure 8) to determine the Year 2029 total projected traffic volumes, as shown in **Figure 9**. It should be noted that the results of the capacity analyses are conservative as the traffic generated by the existing truck parking facility was not removed from the area intersections.



NOT TO SCALE



LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:30-5:30 PM)

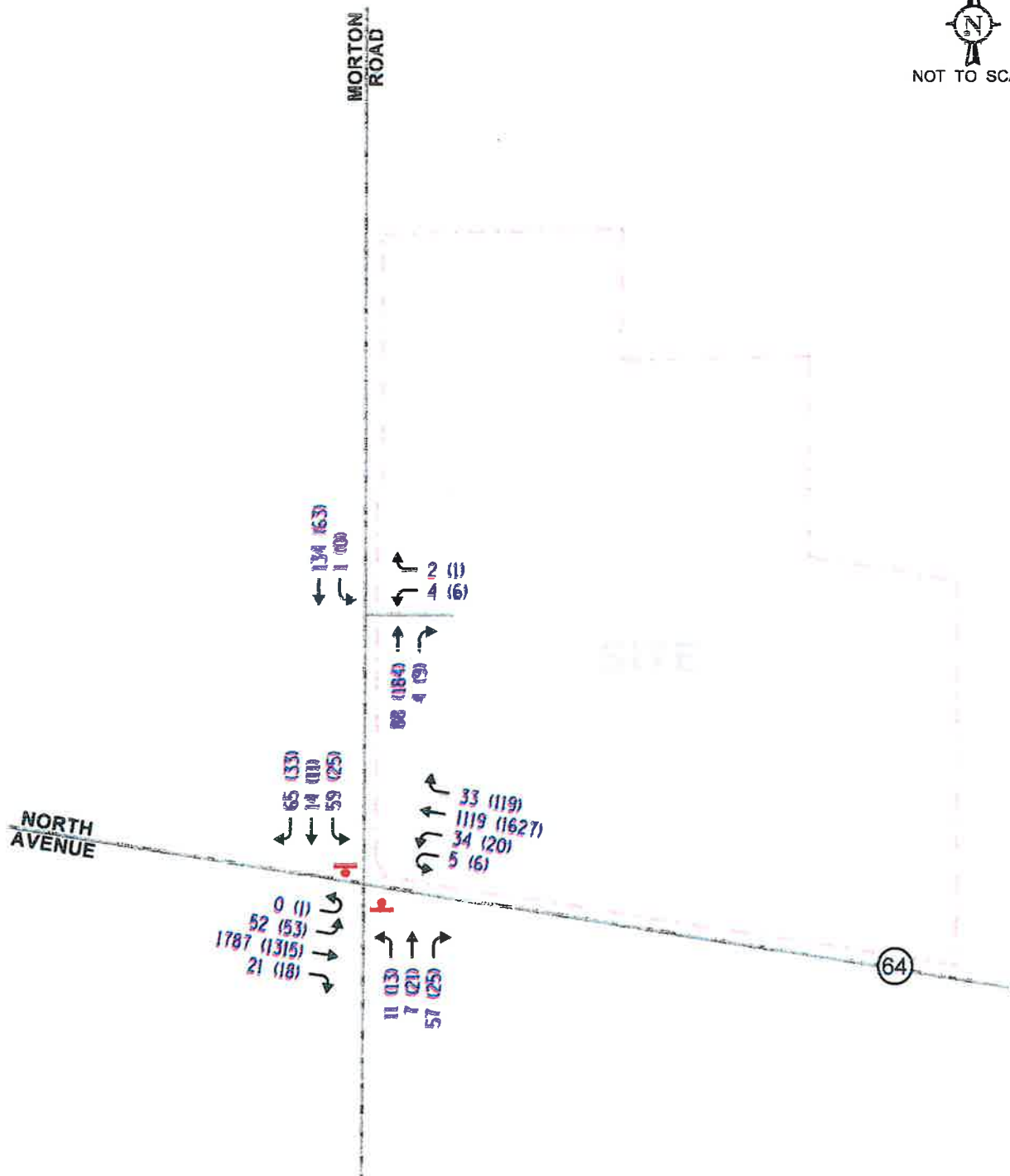
Freight Terminal Facility
 Unincorporated DuPage
 County, Illinois

Site-Generated Traffic Volumes

Job No: 23-068 Figure: 7



NOT TO SCALE

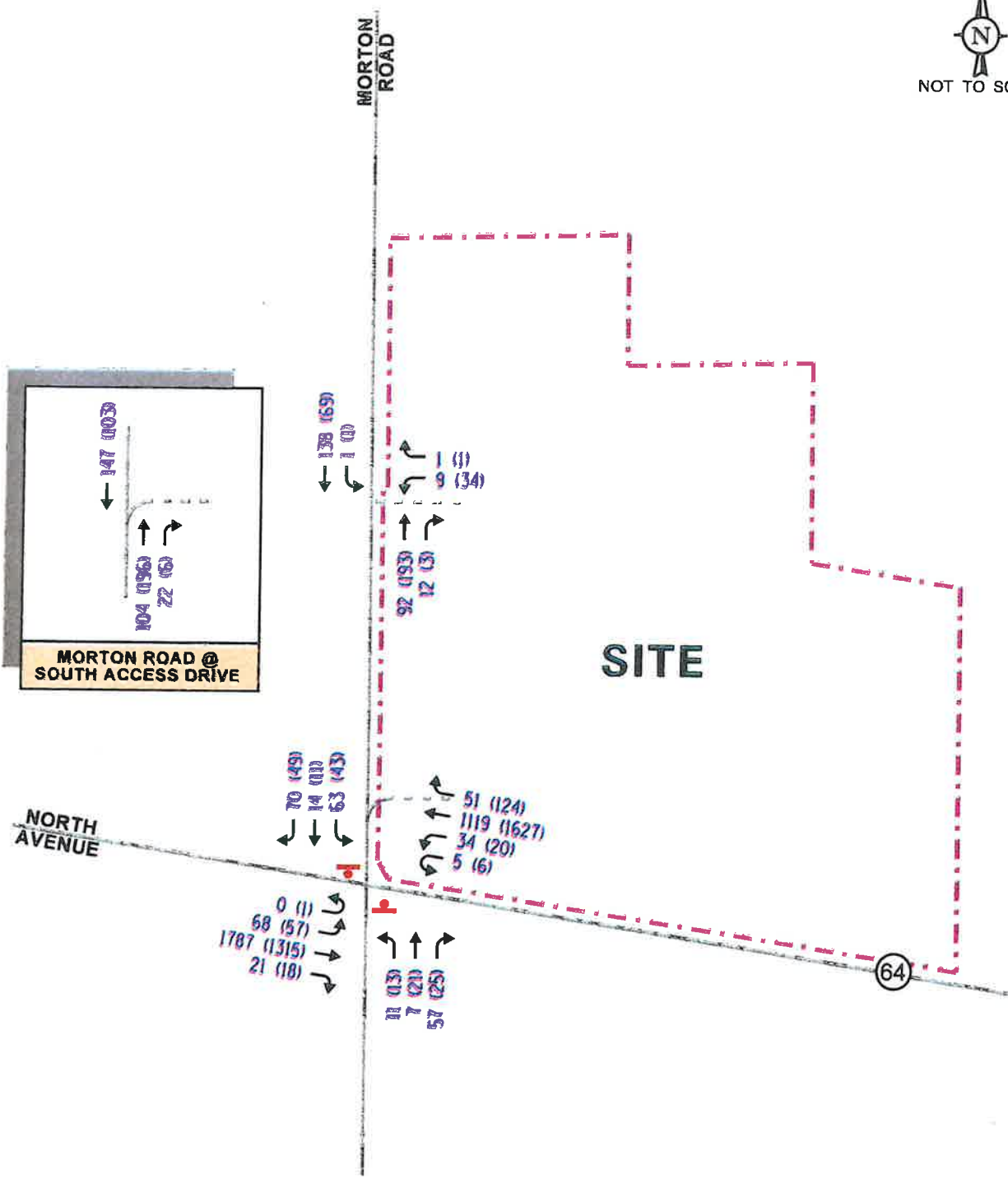
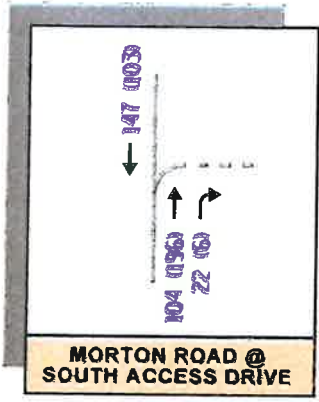


LEGEND

- 00 - AM PEAK HOUR (7:15-8:15 AM)
- (00) - PM PEAK HOUR (4:30-5:30 PM)



NOT TO SCALE



LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:30-5:30 PM)

5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning and weekday evening peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any roadway improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning and weekday evening peak hours for the existing, no-build, and Year 2029 total projected traffic volumes.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6th Edition and analyzed using Synchro/SimTraffic 11 software.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the existing, no-build and Year 2029 total projected conditions are presented in **Tables 3, 4, and 5**, respectively. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 3
CAPACITY ANALYSIS RESULTS – EXISTING CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
North Avenue with Morton Road				
• Northbound Approach	F	50.6	E	39.5
• Southbound Approach	E	40.6	C	23.3
• Eastbound Left Turn	A	9.7	B	12.0
• Westbound Left Turn	E	37.9	C	20.9
LOS = Level of Service Delay is measured in seconds.				

Table 4
CAPACITY ANALYSIS RESULTS – NO-BUILD CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
North Avenue with Morton Road				
• Northbound Approach	F	52.0	E	41.9
• Southbound Approach	E	43.5	C	24.1
• Eastbound Left Turn	A	9.8	B	12.4
• Westbound Left Turn	E	39.2	C	21.3
LOS = Level of Service Delay is measured in seconds.				

Table 5
CAPACITY ANALYSIS RESULTS – YEAR 2029 TOTAL PROJECTED CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
North Avenue with Morton Road				
• Northbound Approach	F	54.8	E	42.5
• Southbound Approach	F	52.4	D	25.5
• Eastbound Left Turn	B	10.1	B	12.6
• Westbound Left Turn	E	39.2	C	21.3
Morton Road with Full Access Drive				
• Westbound Approach	B	10.7	B	11.4
• Southbound Left Turn	A	7.4	A	7.7
LOS = Level of Service Delay is measured in seconds.				

Discussion and Recommendations

The following summarizes how the intersections are projected to operate and identifies any roadway and traffic control improvements necessary to accommodate the facility-generated traffic.

North Avenue with Morton Road

The results of the capacity analyses indicate that the southbound approach currently operates at level of service (LOS) E during the weekday morning peak hour and at LOS C during the weekday evening peak hour. The northbound approach currently operates at LOS F during the weekday morning peak hour and at LOS E during the weekday evening peak hour. Furthermore, eastbound left-turning movements from North Avenue onto Morton Road currently operate at LOS A during the weekday morning peak hour and LOS B during the weekday evening peak hour. Westbound left-turning movements from North Avenue onto Morton Road currently operate at LOS E during the weekday morning peak hour and at LOS C during the weekday evening peak hour.

It should be noted that the existing levels of service for the northbound and southbound approaches and the westbound left-turn movements are common for stop sign-controlled intersections along higher volume arterial roadway similar to North Avenue. Typical of other unsignalized intersections along North Avenue, the minor approaches rely on the gaps in traffic created by the platooning on North Avenue as well as the wide landscaped median, which provides refuge for turning and through movements on Morton Road. It should be noted that the analyses did take into consideration the traffic signal located at the intersection of North Avenue with County Farm Road which creates gaps in the westbound North Avenue traffic stream.

Under Year 2029 no build traffic volumes, all of the approaches and movements are projected to continue operating at existing levels of service with increases in delay of approximately three seconds or less over existing conditions.

Under Year 2029 total projected traffic volumes, all of the approaches and movements are projected to continue operating at no-build levels of service with increases in delay of approximately three seconds or less, except for the eastbound left-turn movement during the weekday morning peak hour which is projected to operate at LOS B and the southbound approach during the weekday evening peak hour which is projected to operate at LOS D. The volume to capacity ratios for the northbound and southbound approaches are projected to be less than one, indicating that both approaches have sufficient reserve capacity to accommodate the traffic estimated to be generated by the proposed facility. Furthermore, the 95th percentile queues for the northbound and southbound approaches are projected to be two to three vehicles which is an increase of one vehicle over existing conditions. Overall, the proposed development is only projected to increase the volume of traffic traversing this intersection by approximately one percent during the peak hours.

As such, the traffic estimated to be generated by the proposed development will have a limited impact on the operations of this.

It should be noted that since North Avenue is an SRA route, a traffic signal is only warranted based on an 8-hour traffic signal warrant with the minor approach criteria modified to be a minimum of 150 vehicles for a two-lane minor approach. The weekday morning and weekday evening traffic volumes do not exceed 150 vehicles and, as such, it is anticipated that this intersection will not meet the 8-hour traffic signal warrant criteria. This intersection should be monitored in the future to determine the need for a signal.

Morton Road with Proposed Access Drives

The results of the capacity analyses indicate that outbound movements from the full-movement access drive onto Morton Road are projected to operate at LOS B during both peak hours. Furthermore, southbound left-turning movements are projected to operate at LOS A during the peak hours and these movements will continue to be accommodated via the existing two-way left-turn lane on Morton Road. As such, this access drive will be adequate in accommodating the traffic estimated to be generated by the proposed facility and no roadway or traffic control improvements will be required.

As previously indicated, it is recommended that the southerly access drive be restricted to right-turn movements in, given its proximity to North Avenue and limited volume of traffic accessing the site to/from the north on Morton Road. All inbound traffic will be able to operate under free flow conditions and vehicles will be able to access the employee parking lot or loading dock area without impacting the operations of Morton Road. Furthermore, the provision of this access drive will allow trucks to access the southerly loading docks directly, minimizing the truck activity along the north side of the proposed building. In order to accommodate inbound trucks efficiently, it is recommended that a 50-foot tapered right turn lane be provided on Morton Road at the right-in only access drive.

Overall, the proposed access system with the recommended configuration will be adequate in accommodating the traffic estimated to be generated by the proposed facility and will provide efficient and flexible access.

6. Conclusion

Based on the preceding analyses and recommendations, the following conclusions have been made:

- Unsignalized intersections along North Avenue rely heavily on the use of the wide landscaped medians and platooning of North Avenue traffic in order to complete minor movements.
- The results of the capacity analyses show that the existing roadway system generally has sufficient reserve capacity to accommodate the traffic to be generated by the proposed facility.
- Access to the site will continue to be provided via a full movement access drive on Morton Road with will be improved to better accommodate truck turning maneuvers. The supplemental right-in only access drive will increase access flexibility and minimize the truck activity on the north side of the site.
- Both access drives will be designed to accommodate truck turning maneuvers efficiently.
- In order to accommodate inbound trucks efficiently, it is recommended that a 50-foot tapered right turn lane be provided on Morton Road at the right-in only access drive.
- The proposed access system will be adequate in accommodating traffic estimated to be generated by the truck parking facility and will ensure efficient and flexible access will be provided.



Village of Carol Stream

FRANK SAVERINO, SR., MAYOR • JULIA SCHWARZE, CLERK • ROBERT MELLOR, MANAGER
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(630) 665-7050 • FAX (630) 665-1064
www.carolstream.org

MEMORANDUM

TO: Chairman and Commissioners, Plan Commission

FROM: Tom Farace, Planning & Economic Development Manager

THROUGH: Donald T. Bastian, Community Development Director

DATE: September 21, 2023

RE: PC/ZBA Case 23-0036, Village of Carol Stream / Text Amendment – Video Gaming Cafes – Withdrawn Case

A case was scheduled for the September 25, 2023 PC/ZBA meeting to review a proposed UDO text amendment. The amendment pertained to regulations for video gaming cafes, and specifically the distance requirements between video gaming cafes. After further review, staff has determined that Village-initiated revisions to the distance requirements will not be considered at this time. Public notice was completed for the text amendment to be reviewed at the September 25, 2023 PC/ZBA meeting, but since staff has reconsidered the proposed amendment, the case no longer needs to be heard by the PC/ZBA.

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